



Southeastern Pennsylvania Transportation Authority

Board

Chairman
John W. MacMurray

Vice Chairman
Hillel S. Levinson, Esq.

G. Roger Bowers, Esq.
Frank W. Jenkins, Esq.
James C. McConnon, Esq.
James C. McHugh
Joseph L. Pyle, Jr.
Dennis J. Rochford
Lawrence R. Stoltz
Robert J. Thompson
James B. Wilson

Chief Counsel
Lewis H. Van Dusen, Jr., Esq.

Executive Office

2028 PSFS Building • 12 South 12th Street
Philadelphia, Pa. 19107
(215) 574-7300

RECEIVED

JAN 12 1979

SECRETARY'S OFFICE
Public Utility Commission

January 10, 1979

Mr. C. J. McElwee, Secretary
Pennsylvania Public Utility Commission
P. O. Box 3265
Harrisburg, Pennsylvania 17120

Re: C. 20235
Borough of Folcroft

Dear Mr. McElwee:

This will acknowledge receipt of your letter dated January 4, 1979, enclosing a copy of an Order issued by the Commission on December 21, 1978, regarding the above captioned matter.

Yours very truly

E. Huss
Edward H. Huss
Staff Counsel

EHH/sg
Enc.



This 9th day of January, 1979

the undersigned hereby acknowledges receipt of

COPY OF ORDER

rendered by the Commission in Docket Number C. 20235

under date of December 21, 1978 and accepts service in behalf

of SOUTHEASTERN PENNSYLVANIA TRANSPORTATION AUTHORITY

E. H. H. H.

DOCUMENT
FOLDER

RECEIVED

JAN 16 1979

SECRETARY'S OFFICE
Public Utility Commission

This 8th day of JANUARY 1979

the undersigned hereby acknowledges receipt of

COPY OF ORDER

rendered by the Commission in Docket Number C. 20235

under date of December 21, 1978 and accepts service in behalf

of PHILADELPHIA SUBURBAN WATER COMPANY

Robert A. Luksa

PLEASE NOTE: Due to the unfortunate death of Mr. John G. McKay, Jr., all future communications should be forwarded to the attention of Mr. Robert A. Luksa.

DOCUMENT
FOLDER

PHILADELPHIA ELECTRIC COMPANY

2301 MARKET STREET

P.O. BOX 8699

PHILADELPHIA, PA. 19101

(215) 841-4000

EDWARD G. BAUER, JR.
VICE PRESIDENT
AND GENERAL COUNSEL

EUGENE J. BRADLEY
ASSOCIATE GENERAL COUNSEL

DONALD BLANKEN
RUDOLPH A. CHILLEMI
E. C. KIRK HALL
T. H. MAHER CORNELL
PAUL AUERBACH
ASSISTANT GENERAL COUNSEL

EDWARD J. CULLEN, JR.
JOHN F. KENNEDY, JR.
ASSISTANT COUNSEL

RECEIVED

JAN 10 1979

SECRETARY'S OFFICE
Public Utility Commission

January 8, 1979

Mr. C. J. McElwee, Secretary
Pennsylvania Public Utility Commission
P. O. Box 3265
Harrisburg, PA 17120

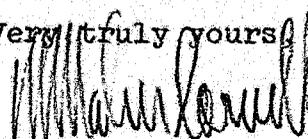
RE: C. 20235 - Borough of Folcroft v. Penn Central
Transportation Company, Department of Trans-
portation of the Commonwealth of Pennsylvania,
County of Delaware, Southeastern Pennsylvania
Transportation Authority, Consolidated Rail
Corporation and National Railroad Passenger
Corporation

Dear Mr. McElwee:

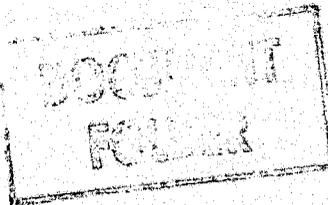
This will acknowledge receipt of your letter
of January 4, 1979, enclosing the copy of the order
issued by the Commission on December 21, 1978.

Please find enclosed the receipt of service
which I have executed.

Very truly yours,


T. H. Maher Cornell

THMC/kmc
Enclosure



This 8th day of January

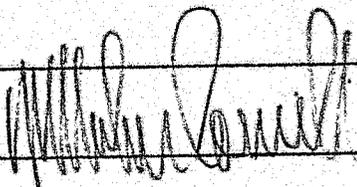
the undersigned hereby acknowledges receipt of

COPY OF ORDER

rendered by the Commission in Docket Number G. 20235

under date of December 21, 1978 and accepts service in behalf

of PHILADELPHIA ELECTRIC COMPANY



DOCUMENT
FOLDER

RECEIVED

JAN 11 1979

SECRETARY'S OFFICE
Public Utility Commission

This 9th day of January 1978

the undersigned hereby acknowledges receipt of

COPY OF ORDER

rendered by the Commission in Docket Number C. 20235

under date of December 21, 1978 and accepts service in behalf

of CONSOLIDATED RAIL CORPORATION

Joe A. Green

DOCUMENT
FOLDER

FILE

RECEIVED

JAN 11 1979

W. Preston Granbery
Attorney

SECRETARIES OFFICE
Public Utility Commission

**The Bell Telephone Company
of Pennsylvania**

Law Department

One Parkway
Philadelphia, Pennsylvania 19102
Phone: (215) 466-4717

January 9, 1979

C. J. McElwee, Secretary
Commonwealth of Pennsylvania
Pennsylvania Public Utility Commission
P.O. Box 3265
Harrisburg, PA 17120

Re: Borough of Folcroft v. Penn Central Transportation Company,
Department of Transportation of The Commonwealth of Penn-
sylvania, County of Delaware, Southeastern Pennsylvania
Transportation Authority, Consolidated Rail Corporation
and National Railroad Passenger Corporation - C. 20235

Dear Mr. McElwee:

Enclosed herewith please find the executed acceptance of service
regarding the above-captioned proceeding.

Very truly yours,

W. Preston Granbery

W. Preston Granbery
Attorney

WPG/cjb
enc.

DOCUMENT
FOLDER

FILE

RECEIVED

JAN 9 1979

SECRETARY'S OFFICE
Public Utility Commission

This 8th day of January, 1979,

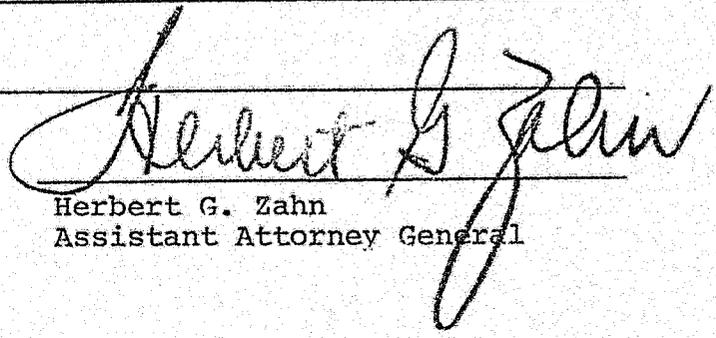
the undersigned hereby acknowledges receipt of

ONE(1) EXECUTED and THREE(3) COPIES OF ORDER

rendered by the Commission in Docket Number C. 20235

under date of December 21, 1978 and accepts service in behalf

of PENNSYLVANIA DEPARTMENT OF TRANSPORTATION


Herbert G. Zahn
Assistant Attorney General

DOCUMENT
FOLDER FILE

This 9th day of January

the undersigned hereby acknowledges receipt of

COPY OF ORDER

rendered by the Commission in Docket Number C. 20235

under date of December 21, 1978 and accepts service in behalf

of THE BELL TELEPHONE COMPANY OF PENNSYLVANIA

W. Preston Grubey

DOCUMENT
FOLDER

FILE

RECEIVED

JAN 12 1979

SECRETARY'S OFFICE
Public Utility Commission

This 10TH day of January

the undersigned hereby acknowledges receipt of

COPY OF ORDER

rendered by the Commission in Docket Number C. 20235

under date of December 21, 1978 and accepts service in behalf

of BOROUGH OF FOLCROFT

[Handwritten Signature]

DOCUMENT
FOLDER

FILE

RECEIVED

JAN 22 1979

SECRETARY'S OFFICE
Public Utility Commission

This 11th day of January, 1979

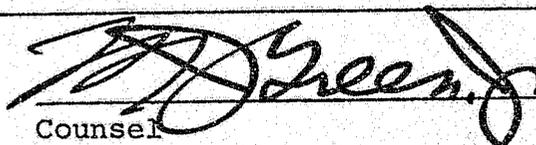
the undersigned hereby acknowledges receipt of

COPY OF ORDER

rendered by the Commission in Docket Number C. 20235

under date of December 21, 1978 and accepts service in behalf

of NATIONAL RAILROAD PASSENGER CORPORATION



Counsel
National Railroad Passenger Corporation
400 N. Capitol Street, N.W.
Washington, D.C. 20001

(202) 383-3989

DOCUMENT
FOLDER

FILE

RECEIVED

JAN 17 1979

SECRETARY'S OFFICE
Public Utility Commission

This 11th day of January

the undersigned hereby acknowledges receipt of

COPY OF ORDER

rendered by the Commission in Docket Number C. 20235

under date of December 21, 1978 and accepts service in behalf

of TRUSTEES OF PENN CENTRAL TRANSPORTATION COMPANY

Robert Swajko

DOCUMENT
FOLDER

16

RECEIVED

JAN 12 1979

LAW OFFICES

NOLAN AND SEMERARO

SECRETARY'S OFFICE
Public Utility Commission

LAWYERS & COUNSELORS BUILDING
SECOND AND OLIVE STREETS
MEDIA, PENNSYLVANIA 19063

C 20235

PETER J. NOLAN
ANTHONY R. SEMERARO
AUGUST T. GROOVER
RICHARD C. TINUCCI
ALFRED J. MATTEI
DONALD E. JOHNSON
RICHARD S. CLARKSON, JR. *
THEODORE S. MELNYCHUK

January 11, 1979

TELEPHONE
(215) 565-3400

FILE NO.:

REPLY TO: R. C. T.

* ALSO MEMBER OF NEW JERSEY BAR

Commonwealth of Pennsylvania
Pennsylvania Public Utility Commission
P. O. Box 3265
Harrisburg, Pennsylvania 17120

Attention: C. J. McElwee, Secretary

RE: Borough of Folcroft vs. Penn Central Transportation Co., et al

Dear Mr. McElwee:

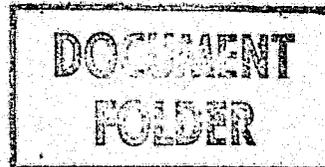
Enclosed herein you will find a receipt for a copy of an Order indicating that I have accepted service on behalf of my client, the Borough of Sharon Hill.

Very truly yours,

Richard C. Tinucci

Richard C. Tinucci

RCT/bb
Enclosure



This 11th day of January, 1979

the undersigned hereby acknowledges receipt of

COPY OF ORDER

rendered by the Commission in Docket Number C. 20235

under date of December 21, 1978 and accepts service in behalf

of BOROUGH OF SHARON HILL

Richard E. Turner

DOCUMENT
FOLDER



COMMONWEALTH OF PENNSYLVANIA
PENNSYLVANIA PUBLIC UTILITY COMMISSION
P. O. BOX 3265, HARRISBURG, Pa. 17120

February 5, 1979

IN REPLY PLEASE
REFER TO OUR FILE
C. 20235

The Honorable Edward Biester
Attorney General
Commonwealth of Pennsylvania
Capitol Annex Building
Harrisburg, PA 17120

Re: Borough of Folcroft

v.

Penn Central Transportation Company,
Department of Transportation of the
Commonwealth of Pennsylvania, County of
Delaware, Southeastern Pennsylvania
Transportation Authority, Consolidated
Rail Corporation and National Railroad
Passenger Corporation.

Dear General Biester:

This is to advise you that the Public Utility Commission seeks enforcement of its order of October 6, 1977, in the above captioned matter, against the Department of Transportation.

On January 16, 1974, the Borough of Folcroft (Delaware County) filed a complaint with the Commission, in which it alleged that hazardous conditions existed at the crossings where Folcroft Avenue (a borough street) partly in the Borough of Folcroft and partly in the Borough of Sharon Hill, and where Primos Avenue (State Highway Route 420), partly in the Borough of Folcroft and partly in the Borough of Glenolden, all in Delaware County, cross over and above the grade of electrified tracks of Penn Central Transportation Company (now owned by National Railroad Passenger Corporation, "AMTRAK"). An order was issued on June 25, 1974, sustaining the Borough's complaint and directing that the Primos Avenue bridge be posted for a maximum load limit of seven (7) tons and that a new modern structure be built by the Department to replace the existing structure. The Department was directed to submit preliminary plans for and cost estimates of the new structure within ninety (90) days. After a hearing on the plans, the Commission directed, by order dated May 13, 1975, that the Department submit detailed construction and right-of-way plans within fifteen (15) months.

RECEIVED
FEB 13 1979
P. O. BOX 3265
HARRISBURG, PA 17120

COMMONWEALTH OF PENNSYLVANIA
PENNSYLVANIA PUBLIC UTILITY COMMISSION

- 2 -

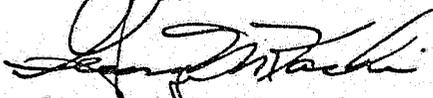
On September 23, 1977 the Department petitioned the Commission for a two (2) year time extension and a hearing on the matter was held on May 10, 1977. In the interim, the Borough notified the Commission that Primos Avenue bridge had deteriorated to the point that the structure had to be closed to vehicular traffic.

On October 6, 1977, the Commission issued an order approving an extension of time, for the Department until September 30, 1978, for submittal of detailed plans. Additionally, the subject structure was reopened, but by Commission order of August 29, 1978, the posting was reduced to a two and a half (2 1/2) ton maximum load limit.

On October 6, 1978, the Department again petitioned for a time extension (until September 30, 1978), but the Commission denied this petition. The Commission, in its order of December 21, 1978 (entered: January 4, 1979), concluded that further delay in the implementation of an improvement at this location would impose an unjustifiable burden on the traveling public. The Department has neither appealed nor sought administrative review of the Commission's order of December 21, 1978.

Since the Department has failed to comply with the provisions of the Commission's order of October 6, 1977, directing it to submit detailed construction and right-of-way plans, the Commission, pursuant to Section 503 of the Public Utility Code, 66 Pa.C.S. §503, respectfully requests that you take appropriate action against the Department to assure compliance. (A copy of this order is attached)

Sincerely,



George M. Kashi
Chief Counsel

PENNSYLVANIA
PUBLIC UTILITY COMMISSION
Harrisburg, PA 17120

Public Meeting held December 21, 1978

Commissioners Present:

W. Wilson Goode, Chairman
Robert K. Bloom
Louis J. Carter
Helen B. O'Bannon
Michael Johnson

Borough of Folcroft

v.

Penn Central Transportation Company,
Department of Transportation of the
Commonwealth of Pennsylvania, County
of Delaware, Southeastern Pennsylvania
Transportation Authority, Consolidated
Rail Corporation and National Railroad
Passenger Corporation

C. 20235 *(Hm)*

O R D E R

BY THE COMMISSION:

On January 16, 1974, Borough of Folcroft filed the subject complaint alleging that hazardous and inadequate conditions exist at the crossings where Folcroft Avenue (a borough street), partly in the borough of Folcroft and partly in the borough of Sharon Hill, and where Primos Avenue (State Highway Route 420), partly in the borough of Folcroft and partly in the borough of Glenolden, all in Delaware County, cross above the grade of four electrified tracks of Penn Central Transportation Company (now National Railroad Passenger Corporation), and requesting that respondent railroad undertake immediate repairs to the structures, to determine and post load limits, and take steps necessary to replace the inadequate bridges with modern structures.

Primos Avenue, in the vicinity of the crossing, is graded for an approximate width of 24 feet, with the center portion being paved with bituminous material for a width of 22 feet, with both approaches to the crossing transitioning to meet the bridge cartway width of 16 feet 8 inches. The bridge, constructed in 1891 by The Pennsylvania Railroad Company, is a 5-span structure, totalling 116 feet 9 inches in length, consisting of two nominal 15-foot timber spans on each end of the bridge with the center span over the tracks being a through plate girder span 58 feet in length. The bridge deck is composed of timber planks overlaid with bituminous material between timber curbs. A sidewalk, 5 feet in width, is located outside the girder on the north side of the bridge. The highway approaches, on tangent alignment, have grades that are steep ascending toward the bridge, with the change in grade being accomplished within the length of the bridge.

The Folcroft Avenue bridge, constructed in 1891 by The Pennsylvania Railroad Company, is a single span 62-foot long through plate girder bridge with a timber plank deck overlaid with bituminous material between timber curbs. The bridge cartway width is 15 feet. The highway approaches, paved for a width of 30 feet, are on tangent alignment and have grades that are steep ascending toward the bridge with the change in grade being accomplished within the short length of the bridge.

Following hearing held April 24, 1974, this Commission issued an order on June 25, 1974 sustaining the borough's complaint and determining that the existing Primos Avenue bridge, posted for a maximum load limit of 7 tons, should be replaced with a new, modern structure. The order directed Pennsylvania Department of Transportation, within 90 days from

date of service of the order to prepare and submit preliminary plans and cost estimates for the reconstruction of the Primos Avenue crossing and the approaches thereto.

Upon notification from Penn Central Transportation Company that an analysis of the load-carrying capability of the Folcroft Avenue bridge, located approximately 900 feet northwardly of the Primos Avenue crossing, revealed that certain structural members of the bridge were insufficient in strength to safely accommodate a vehicle weighing in excess of 7-1/2 tons, the Commission issued a telegram order on November 22, 1974, directing the borough of Folcroft to post the bridge for a maximum load limit of 7 tons, and further directing Pennsylvania Department of Transportation to post and maintain suitable detour routes for vehicles weighing in excess of 7 tons.

Following receipt of the preliminary plans and estimate of costs on October 28, 1974, from Pennsylvania Department of Transportation, for the reconstruction of the Primos Avenue crossing, further hearing was held February 25, 1975, after which this Commission issued an order on May 13, 1975, and entered May 22, 1975, directing that Pennsylvania Department of Transportation, having agreed so to do, within 15 months from date of service of the order, to prepare and submit detailed construction and right-of-way plans providing for the reconstruction of said bridge. At the February 25, 1975 hearing, a witness for PennDOT testified that the department had been unable to post a detour for vehicles weighing in excess of 7 tons, due to the weight restrictions on neighboring bridges.

On September 23, 1976, Pennsylvania Department of Transportation filed a petition seeking an extension of time of 2 years in which to prepare and submit the detailed construction and right-of-way plans for the reconstruction of the Primos Avenue crossing. In its petition the department averred that, since its preliminary plans contained numerous inaccuracies and, since it had updated its design standards and procedure, the department must resurvey the entire project area and possibly make a line change and, as such, an additional 2 years would be required to complete the detailed construction and right-of-way plans.

At Public Meeting held November 26, 1976, this Commission directed that hearing be set on the aforesaid petition of PennDOT. Hearing was held May 10, 1977. Included in PennDOT's testimony at the hearing was a statement that the department had the funding for the design of the Primos Avenue improvement, a part of which would come from the Federal Government, but it did not have the funds for the construction of the improvement.

Upon being notified by Borough of Folcroft, on July 27, 1977, that the Primos Avenue bridge was in a seriously hazardous condition due to the existence of large pot-holes in the bridge deck's bituminous wearing surface, this Commission issued a certified letter order on August 2, 1977 directing, inter alia, that the bridge be closed to all vehicular traffic; that PennDOT immediately repair the bridge deck wearing surface; and that upon completion of the repair work the bridge be opened to passenger vehicles only.

By order issued October 6, 1977, and entered October 20, 1977, this Commission stated that it was not convinced that PennDOT had made a determined effort to expedite the much needed Primos Avenue crossing improvement; however, the Commission granted the department an extension of time until September 30, 1978 in which to prepare and submit the detailed construction and right-of-way plans for the reconstruction of said crossing. The order further directed National Railroad Passenger Corporation to perform a detailed inspection of the Primos Avenue bridge and to submit a report of its recommendations regarding the weight restrictions that should be placed on the bridge and any repairs necessary to maintain said restrictions.

Upon being notified by National Railroad Passenger Corporation that its analysis of the structural members of the Primos Avenue bridge, several of which were severely deteriorated, revealed that the bridge should be rated for an H-2.3 (2.3 ton) loading, the Commission issued an emergency order on August 29, 1978 directing Pennsylvania Department of Transportation to immediately post the bridge for a maximum load limit of 2-1/2 tons, and directing National Railroad Passenger Corporation to perform monthly inspections of the bridge.

Other crossings, all above grade, of the involved line of railroad in the subject vicinity, together with their weight restrictions are as follows:

<u>Highway</u>	<u>Location</u>	<u>Weight Restriction</u>
Ashland Avenue	1500 feet south of Primos Avenue crossing	7 tons
Sharon Avenue (State Highway Route 23072)	3200 feet north of Primos Avenue crossing	15 tons
Calcon Hook Road (State Highway Route 23073)	5000 feet north of Primos Avenue crossing	7 tons

Due to the weight restrictions of the aforementioned bridges, PennDOT has been unable to provide a suitable detour for vehicles weighing in excess of 15 tons. The detour established by the department follows a long, circuitous route, a portion of which lies within the city of Philadelphia.

This matter is presently before us upon petition, filed October 2, 1978, by Pennsylvania Department of Transportation seeking modification of our order issued October 6, 1977 in this proceeding, by granting the department an additional extension of time of 2 years in which to prepare and submit the detailed construction and right-of-way plans for the reconstruction of the Primos Avenue bridge and the highway approaches thereto.

In its petition the department avers that, because of its dire financial position; because many of its employees have been furloughed, including its bridge design engineers who had been working on the subject ordered plans; because it is financially unable to replace or recall the aforesaid bridge design engineers or to hire independent consulting engineers for said work, PennDOT will not be able to comply with the provisions of the October 20, 1977 order requiring plan completion on or before September 30, 1978. PennDOT further avers that it hopes it will be able to complete the required plans and cost estimates on or before September 30, 1980; said plans currently being in the preliminary stage of design.

On November 1, 1978, the Commission received from the borough of Folcroft, an Answer to the petition filed by the department. In its Answer, the borough avers that the reason the project has not proceeded

is solely that the department chooses not to proceed and, that if there is a legitimate excuse, the department should have brought this to the Commission's attention earlier.

The borough also states that the department should have taken the necessary steps for financing the project after the Commission's order was issued on June 25, 1974, and it denies that the department will complete plans for the improvement by September 30, 1980. On the contrary, the borough claims that the department will continue to delay the project as long as possible. Therefore, it requests that the Commission deny the department's request.

Upon full consideration of the petition submitted by the department, the Answer by the borough of Folcroft, and the existing record, we determine that the instant petition, seeking a time extension for submission of plans and cost estimates until September 30, 1980, should be denied.

Initially, we find that the department's time extension petition is premised on its lack of funds. However, the department on September 23, 1976 sought a two year time extension on the preparation of plans and cost estimates, and on October 6, 1977, the Commission approved the department's request. Nonetheless, the Commission stated that it was not convinced that the department had made a determined effort to expedite the completion of the project. The department's instant petition does not dispel the Commission's earlier finding.

We further find that the petition sets forth no newly discovered evidence or material change in conditions or circumstances, since issuance of the Commission's order of October 6, 1977.

Additionally, the record shows that there are currently restrictive weight limitations placed on neighboring bridges and that the department is unable to provide a reasonable detour for vehicles weighing in excess of fifteen (15) tons, which would not be overly circuitous and long.

Accordingly, we conclude that a further delay in the implementation of the improvement will impose an unjustifiable burden on the traveling public.

We further conclude that the department has failed to allege such new evidence or material change in circumstances which would warrant the Commission's modification of its October 6, 1977 order. Furthermore, the Commission's statutory duty to effectuate the prevention of accidents and the promotion of the safety of the public, will be compromised by permitting further delay in the completion of this project; THEREFORE,

IT IS ORDERED:

1. That the prayer of petition of Department of Transportation of the Commonwealth of Pennsylvania, filed October 2, 1978, seeking modification of our order issued October 6, 1977 at C. 20235, be and is hereby denied.

2. That in all respects our prior orders in this proceeding remain in full force and effect.

BY THE COMMISSION,

C. J. McElwee
Secretary

(SEAL)

ORDER ADOPTED: December 21, 1978

ORDER ENTERED:

JAN 4 1979

Huff

February 6, 1979
E-54-7.78

RECEIVED

1979

SECRETARY'S OFFICE
Public Utility Commission



Mr. C. J. McElwee, Secretary
Pennsylvania Public Utility Commission
P.O. Box 3265
Harrisburg, PA 17120

Re: O.H. 7.78 Primos Ave.
P.U.C. C-20235

BUREAU OF
FEB 9 1979
RAIL
TRANSPORTATION

Dear Mr. McElwee:

In accordance with the P.U.C. order in the subject action, Amtrak inspected the Primos Ave. Bridge in the Borough of Folcroft on Thursday, Dec. 7, 1978 and again on Wednesday, Jan. 31, 1979.

The inspections did not reveal any significant changes in the structure that would affect its load carrying capacity of 2½ tons.

By copy of this letter, we are notifying PennDOT and the Borough of Folcroft of the results of our inspection.

Very truly yours,

A. B. Smythe

A. B. Smythe, P.E.
Asst. Chief Engineer, Structures

cc: H. G. Zahn
D. E. Auerbach
I. Oncu

RHC/ec

DOCKETED
COMPLAINT DOCKET
FEB 13 1979

A.
RECEIVED



COMMONWEALTH OF PENNSYLVANIA
PENNSYLVANIA PUBLIC UTILITY COMMISSION
P. O. BOX 3265, HARRISBURG, Pa. 17120

February 7, 1979

IN REPLY PLEASE
REFER TO OUR FILE

C. 20235

The Honorable Edward Biester
Attorney General
Commonwealth of Pennsylvania
Capitol Annex Building
Harrisburg, Pennsylvania 17120

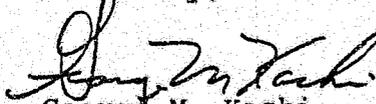
Re: Borough of Folcroft
v.

Penn Central Transportation Company,
Department of Transportation of the
Commonwealth of Pennsylvania, County
of Delaware, Southeastern Pennsyl-
vania Transportation Authority,
Consolidated Rail Corporation and
National Railroad Passenger Corpor-
ation.

Dear General Biester:

With reference to the letter of February 5, 1979
in the above captioned matter, please be advised that a
typographical error appears in the third paragraph on
page 2. The first sentence of that paragraph should read
that the Department petitioned for a time extension until
September 30, 1980 not September 30, 1978.

Sincerely,


George M. Kashi
Chief Counsel

GMK:HSM:sls



Huff

February 23, 1979
E-54-7.75



Mr. C. J. McElwee, Secretary
Commonwealth of Pennsylvania
Public Utility Commission
P.O. Box 3265
Harrisburg, PA 17120

RECEIVED

FEB 27 1979

SECRETARY'S OFFICE
Public Utility Commission

Re: OH 7.75 Primos Ave.
PA P.U.C. Docket # C-20235
Detailed Inspection Report

Dear Mr. McElwee:

In accordance with the latest order issued by the Commission in the subject action, the National Railroad Passenger Corporation has performed a detailed inspection of the bridge carrying Primos Ave. over our facilities, and prepared a report of our findings. Four (4) copies of the report are submitted herewith for the Commission's review.

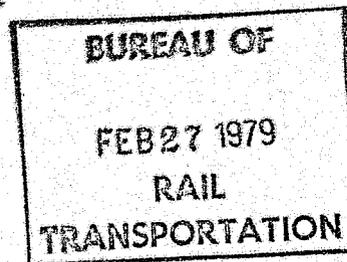
By copy of this letter, we are sending copies of the report by First Class U.S. Postage Paid Mail to all parties of record in this action per the attached list.

Very truly yours,

A. B. Smythe, P.E.
Asst. Chief Engineer, Structures

- cc: W. S. Armentrout
- D. F. Sullivan
- C. Helmetag, Jr.
- E. H. Huss
- D. E. Auerbach
- J. A. Damico, Jr.
- F. P. Connors
- J. T. Sullivan

RHC/ec



DETAILED INSPECTION REPORT
FOR THE BRIDGE CARRYING FOLCROFT AVENUE
OVER THE TRACKS OF
NATIONAL RAILROAD PASSENGER CORPORATION
AT BOROUGH OF FOLCROFT
DELAWARE COUNTY, PENNSYLVANIA

PENNSYLVANIA P.U.C. DOCKET # C-20235

OFFICE OF DEPUTY CHIEF ENGINEER
NATIONAL RAILROAD PASSENGER CORPORATION
PHILADELPHIA, PA

DOCUMENT
FOLDER

TABLE OF CONTENTS

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National Railroad Passenger Corporation at Folcroft, Pa. is a single span thru girder 62' long and 15' wide between timber curbs; steel floor beams and timber stringers support a bituminous covered wood plank deck.

The bridge is in fair to poor condition due primarily to deterioration of the floor beams and the wood plank deck. The bituminous surface is badly deteriorated with the wood deck exposed at some places. The seat angles under a few stringers are badly deteriorated and should be replaced.

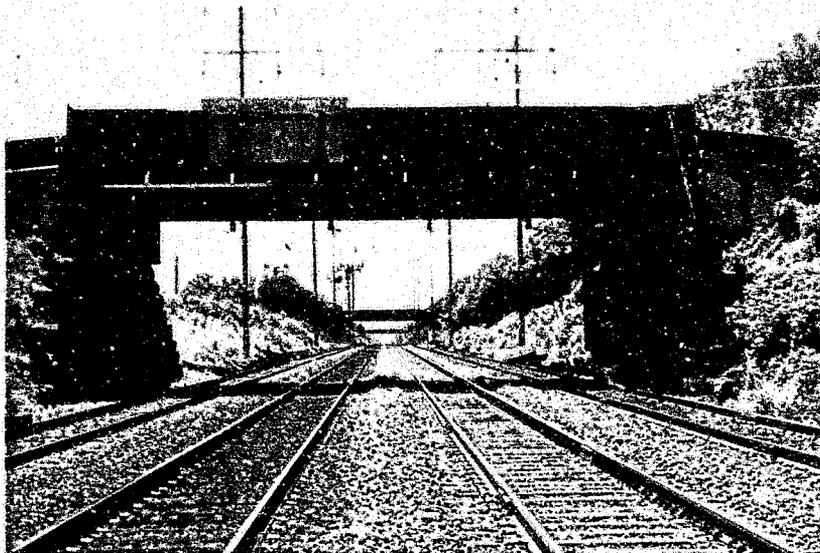
Amtrak rated the structure based on a detailed inspection which it performed. The weakest members were found to be the floor beams which are adequate for the present posted load limit.

Giving due consideration to the age of the girders and the narrow roadway, it is Amtrak's recommendation to replace the structure with a complete bridge built to current highway standards.

DESCRIPTION

The bridge carrying Folcroft Ave. over 4 tracks of the National Railroad Passenger Corporation in the Borough's of Folcroft and Sharon Hill, Pennsylvania is a single span thru girder bridge, 62 ft. long with a 15' clear roadway. The deck is transverse laminated 2x4 inch timber with a bituminous wearing surface; 6x14 inch timber stringers and steel floor beams support the timber deck. The girders of iron were fabricated in 1883 for use at another location, and were installed at this location in 1918. The sidewalks on both sides of the bridge were installed in 1925.

Picture F-1 shows the bridge looking to the south.



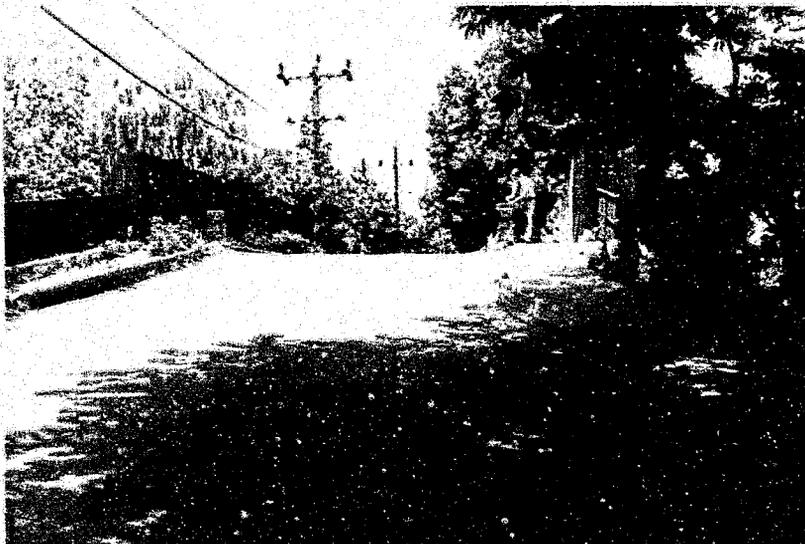
F1

The roadway approach from the west is about 18' wide, while the east approach is 30' wide narrowing to 15' across the structure. The approaches ascend to the bridge on relatively steep grades. Pictures F-2 and F-3 show the west and east approach respectively; the sidewalks measuring about 4' wide are seen in these pictures.

The bridge is currently posted for a weight limit of 7 tons.



F2



F3

DETAILED CLOSE-UP INSPECTION

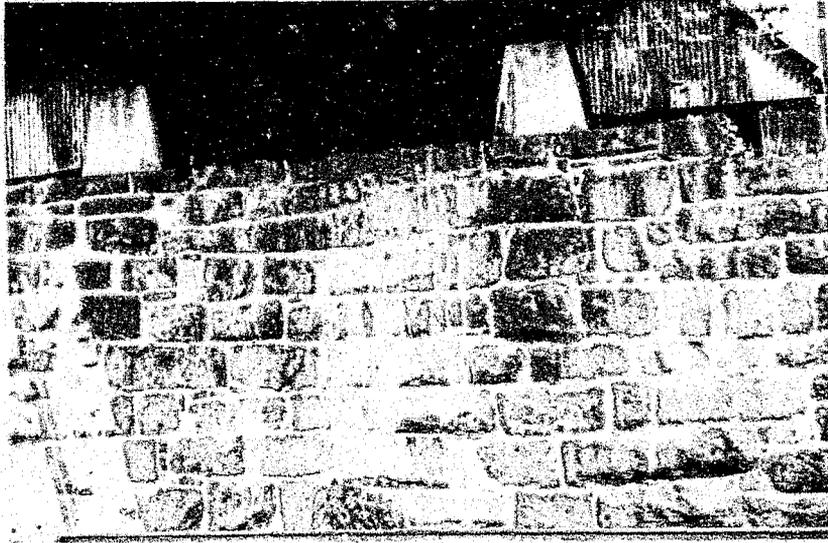
In July of 1978, Amtrak performed a detailed close-up inspection of the subject bridge. The inspection was performed using Amtrak's wire train, a three car work train normally used to maintain the electric catenary system. The following is the report of our observations and conclusions and recommendations based on the inspection.

ABUTMENTS

The high stone masonry abutments are in good condition. The front of the abutment under the bridge shows there is water running down from the joints at the end of the bridge. Pictures F-4 and F-5 show the east and west abutment respectively.



F4



F5

BEARINGS

The plate girders bear on concrete pedestals. The pictures of the abutments show the pedestals which are in good condition.

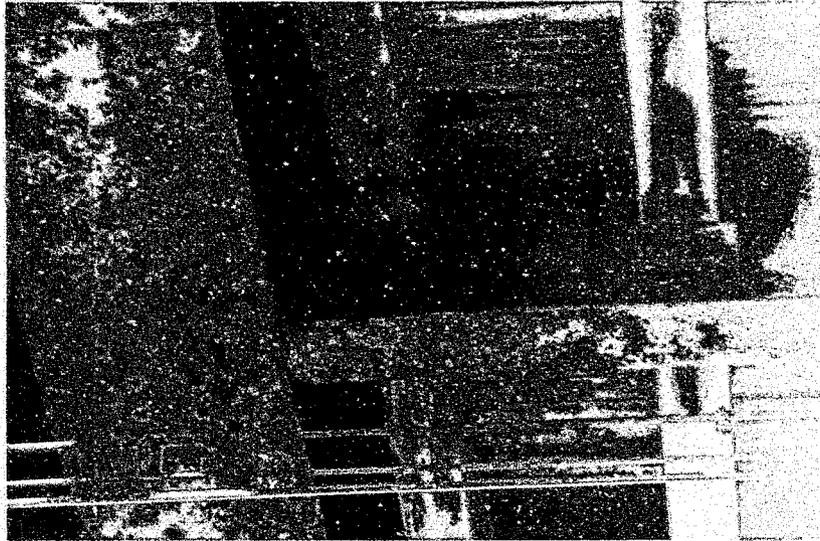
The timber stringers in the end panels bear directly on a piece of timber on top of the backwalls of the abutments. Picture F-6 shows this, and also, that the backwall is in good condition.



F6

FLOOR BEAMS

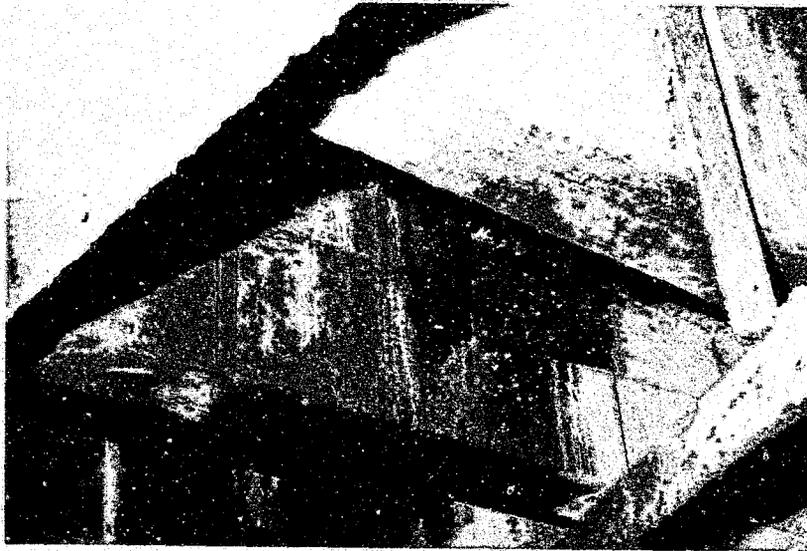
The floor beams of the subject bridge are in fair condition. There is general loss of section along the top and bottom flange; the web also shows some deterioration. A typical floor beam is seen in Picture F-7. The deterioration is due in large part to the application of de-icing chemicals to the surface of the bridge deck. In turn, the chemical laden water finds its way thru the deck onto the beams, where it lays or is absorbed by dirt that has accumulated on the flanges.



F7

STRINGERS

The timber stringers which are 6x14, appear to be in fair to good condition. They are 8'-4" long and spaced 22 1/2" apart. The stringers bear on seat angles attached to the web of the floor beams. Picture F-8 shows a typical stringer that is stained from water leaking thru the deck.



F8

Exhibit 1 in the Appendix, titled "No 16829, PB&WRR - Maryland Div., P.R.R., Tribbitt Public Road Overhead Bridge, North of Folcroft", dated 1918, shows the floor beam details with the seat angles and clips. Picture F-9 shows the condition of one of those connections. The most deteriorated ones are under the curb line of the bridge, and therefore not subjected to direct wheel loads. The cause of the

deterioration is again attributed to de-icing chemicals applied to the surface of the bridge, which then eventually are carried thru the deck and onto the beams.



F9

GIRDERS

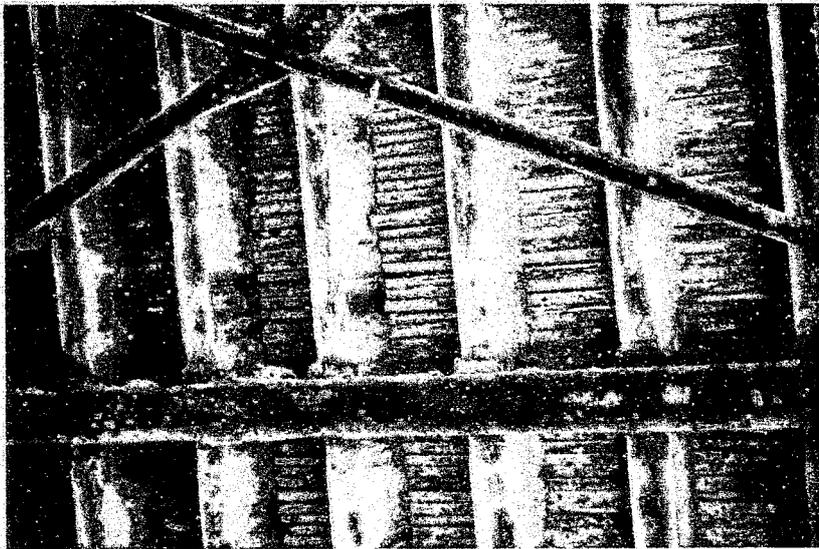
The girders are in fair to good condition. There is some reduction of the web along a line even with the deck surface where debris tends to accumulate against the girder. The end plates and angles of the girder have been damaged by vehicles, but it has not structurally affected the girder. Picture F-10 shows the end of one girder as well as the accumulation of debris and growth along the girder behind the timber curb.



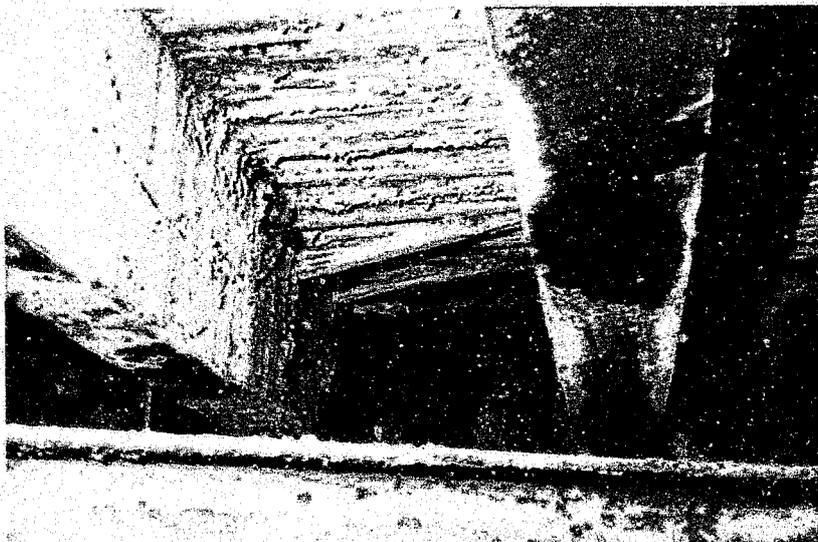
F10

BRIDGE DECK

The bridge deck is in poor condition. The 2"x4" planks are deteriorated and in some locations are broken. Pictures F-11 and F-12 show the underside of the deck where the planks have broken. Accumulation of debris thru the deck is also noticeable in the pictures.

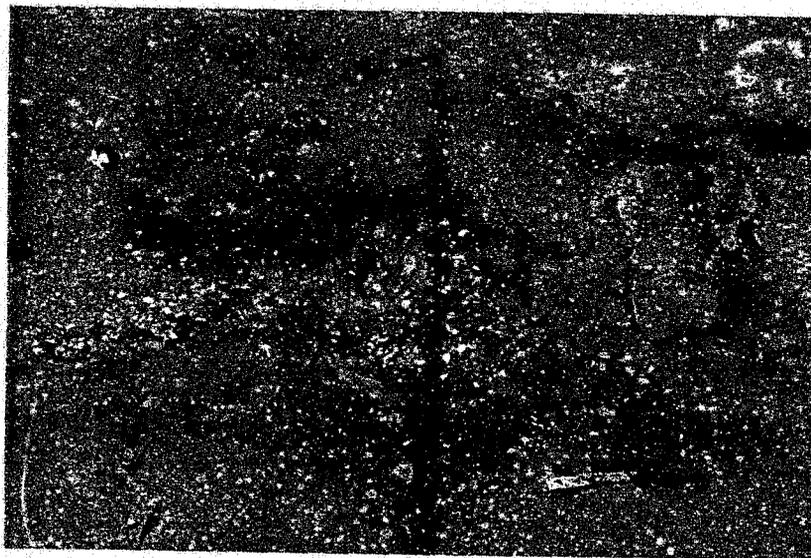


F11



F12

On the surface, the bituminous material is breaking up due to the flexing of the deck under wheel loads. Picture F-13 shows the broken-up surface with exposed wood planks. Picture F-14 shows the entire surface that is badly deteriorated.



F13



F14

Although we have no records of any maintenance of this structure, the bituminous material against the curb leads Amtrak to believe the deck was resurfaced a few times. The timber curb obviously is in poor condition.

SIDEWALK

The sidewalks on the bridge are covered with bituminous material and are in fair to good condition, in that there are no holes exposing the electric catenary system or anything else that would present a hazard to pedestrians.

Picture F-15 shows one of the two sidewalks.



F 15

RATING

Based on our observations and measurements made during the detailed close-up inspection, Amtrak rated the bridge to determine its load carrying capacity.

All structural members were rated using a single lane load; a result of the narrow roadway. The sidewalk live load and fatigue were not considered in the computations of the rating.

The ratings indicated the weakest members were the floor beams and substantiates the current posting of 7.5 tons.

The bridge deck has failed at certain locations, however, overall deterioration would not lead us to recommend a reduction in the posted load limit.

CONCLUSIONS AND RECOMMENDATIONS

The thru girder bridge carrying Folcroft Ave. over 4 tracks of the National Railroad Passenger Corporation in the Boroughs of Folcroft and Sharon Hill, Delaware Vounty, Pa. is in fair to poor condition.

The bridge is a single span thru girder bridge, 62' long with a bituminous covered wood plank deck, timber stringers and steel floor beams. The girder and floor beams are in fair condition with deterioration occurring along the flanges of the floor beams and the web of the girders; the deterioration is largely due to the application of de-icing chemicals to the bridge deck. The timber stringers appear to be in fair to good condition. Under certain stringers, however, the angles fastened to the web of the floor beams to support the stringers are badly deteriorated, and should be replaced. The wood plank deck is in poor condition, and has even failed at certain locations. On the top, the bituminous surface is breaking up, exposing the wood decking. The sidewalks across the bridge are in fair condition.

Based on Amtrak's inspection and analysis of the structure, it could be rehabilitated by installing a new floor system and deck. This would provide a structure capable of carrying all legal loads with the only restriction being the 15' wide roadway between girders. However, because of the relatively short span and the fact the main

girders are old, a complete new structure would be justified. Therefore, Amtrak's recommendation is to replace the existing bridge carrying Folcroft Ave. with a complete new structure built to current highway standards for the volume and type of traffic using the highway. We estimate the cost to be approximately \$160,000, exclusive of any right-of-way damages or significant roadway work.

APPENDIX

OVERSIZE DOCUMENT(S)

Sheet 1

DETAILED INSPECTION REPORT
FOR THE BRIDGE CARRYING PRIMOS AVENUE
OVER THE TRACKS OF
NATIONAL RAILROAD PASSENGER CORPORATION
AT BOROUGH OF FOLCROFT
DELAWARE COUNTY, PENNSYLVANIA

PENNSYLVANIA P.U.C. DOCKET # C-20235

OFFICE OF DEPUTY CHIEF ENGINEER
NATIONAL RAILROAD PASSENGER CORPORATION
PHILADELPHIA, PA.

**DOCUMENT
FOLDER**

TABLE OF CONTENTS

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SUMMARY

The bridge carrying Primos Avenue over 4 tracks of the National Railroad Passenger Corporation at Folcroft, Pa. is a 5 span bridge, 116'-9" long. There are 4 short stringer spans and a center thru girder span, all with a wood plank deck and a bituminous surface. The cartway width between timber curbs is 16'-8".

The bridge is in fair condition except for the main span which is in poor condition. The web of the east girder is severely corroded. The wood plank deck is deteriorating and at certain locations, flexes under wheel loads, causing the bituminous surface to break up.

Amtrak rated the structure, giving consideration to our findings of the inspection. The critical member is the east girder which was rated at H2.3; the analysis assumed the bottom flange to be simply supported between stiffeners due to the loss of web at that location.

Amtrak's recommendation is to replace the existing structure with a new bridge designed and built to the latest standards for highway bridges.

INTRODUCTION

The bridge which is the subject of this detailed close-up inspection, carries Primos Avenue over the tracks of the National Railroad Passenger Corporation at the Borough of Folcroft, Delaware County, Pennsylvania. Primos Avenue, also State Highway Route 420, begins at its connection with the southwesterly boundary line of the City of Philadelphia, extending westerly through Darby Township and Borough of Folcroft, then northerly through the Boroughs of Glenolden, Collingdale, Aldan and Primos to its connection with Springfield Ave., State Highway Route 23065, in Clifton Heights, PA.

The investigation by the P.U.C. is the result of allegations by the Borough of Folcroft, that the subject bridge is hazardous, and that inadequate conditions exist at the crossing.

DESCRIPTION

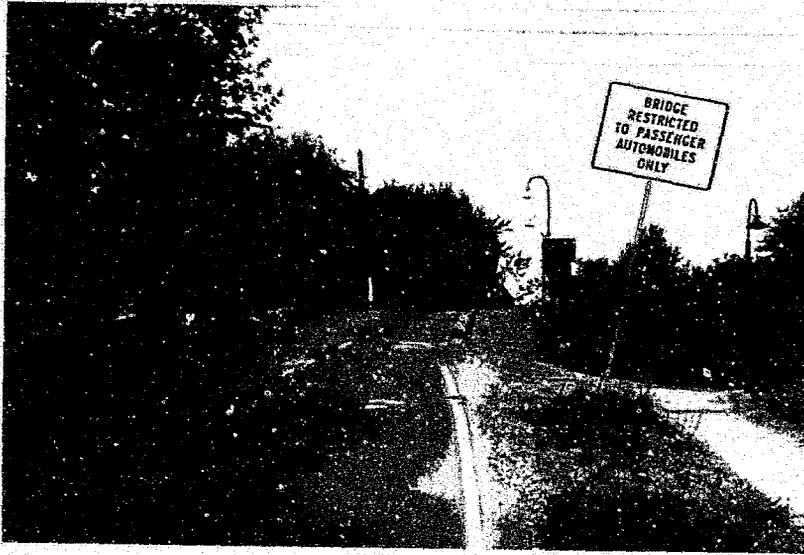
The bridge carrying Primos Avenue over four (4) tracks of Amtrak at Folcroft, Pa. is a five (5) span bridge built in 1891 by the Maryland Division of the Philadelphia, Baltimore and Washington Railroad. The main span which measures 57'-6" long over the railroad, is a thru girder span with timber floor beams, a wood plank floor, and a bituminous wearing surface. The end spans, which measure 14'-6", 15'-4", 15'-11", and 13'-6", are made up of timber stringers with a plank deck and bituminous wearing surface. The substructure is comprised of steel beam bents for the main girder span and timber bents for the end spans; the abutments are stone masonry construction. The overall length of the structure is 116'-9" from face of the stone abutments and is skewed at 90° to the railroad.

The cartway width across the structure is 16'-8" between the wood curbs. The roadway grades ascending to the bridge are 6% on the south and 4% on the north.

Picture P-1 is a view of the structure looking south toward Washington.

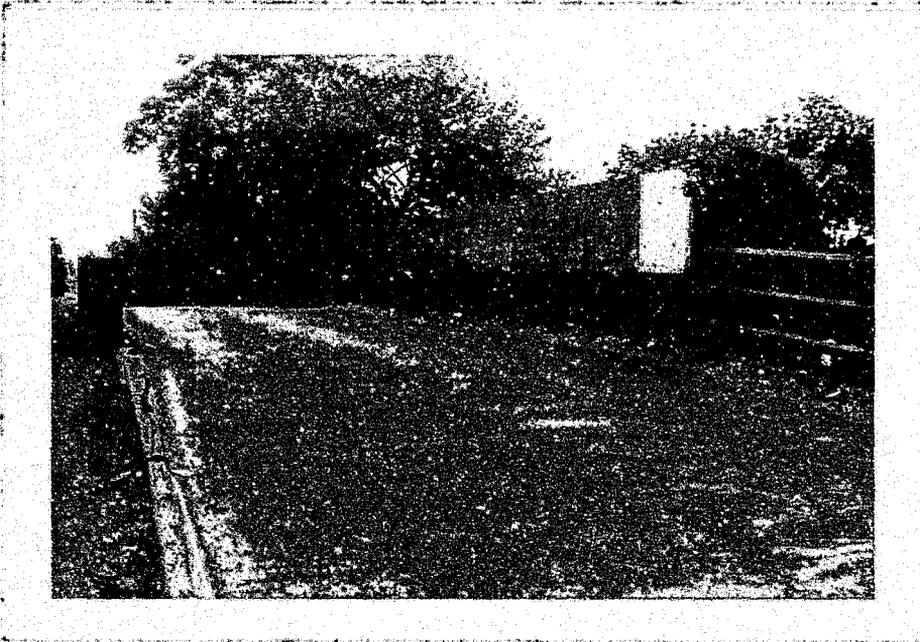


Exhibit A in the appendix is a plan titled Bridge No. 7.75,
#60376, P.B. & W.R.R., Maryland Div. Folcroft, Proposed Reconstruction
of Bridge at Primos Ave., Approved February 1, 1912.



_P2

P-2 is a picture of the approach from the south. The posting sign, which limits the bridge to "Passenger Automobiles Only" was changed by a Public Utility Commission order dated August 29, 1978, limiting the maximum load to two and one half ($2\frac{1}{2}$) tons to enable better police enforcement of the restriction. A sidewalk about 5' wide can be seen on the east side of the bridge. The sidewalk on the outside of the girder, offers pedestrians good protection from vehicles using the bridge.



P3

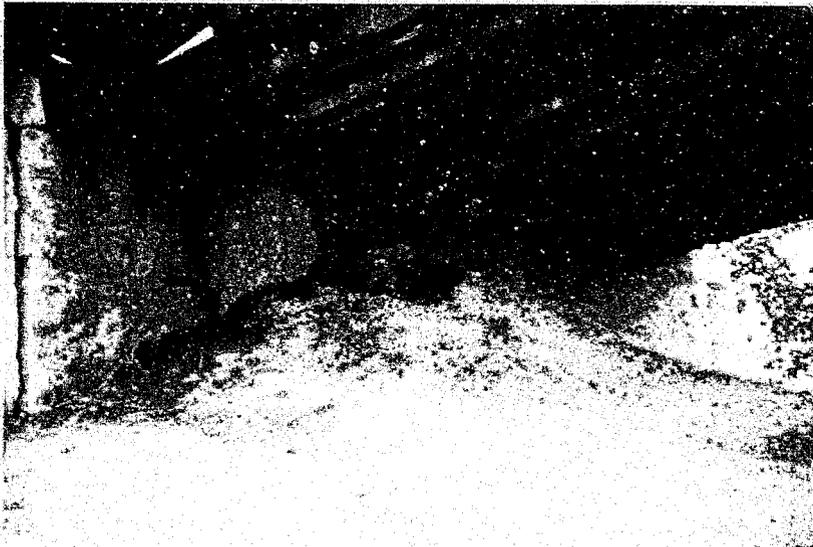
P-3 is a picture of the north approach. The posting signs are further to the north to enable restricted vehicles to detour.

DETAILED CLOSE-UP INSPECTION

In June and July of 1978, Amtrak performed a detailed close-up inspection of the subject bridge. The inspection was performed, using Amtrak's "wire" train; a three car work train normally used to maintain the electric catenary system. The following is the report of our observations and conclusions, and recommendations based on the inspection.

ABUTMENTS

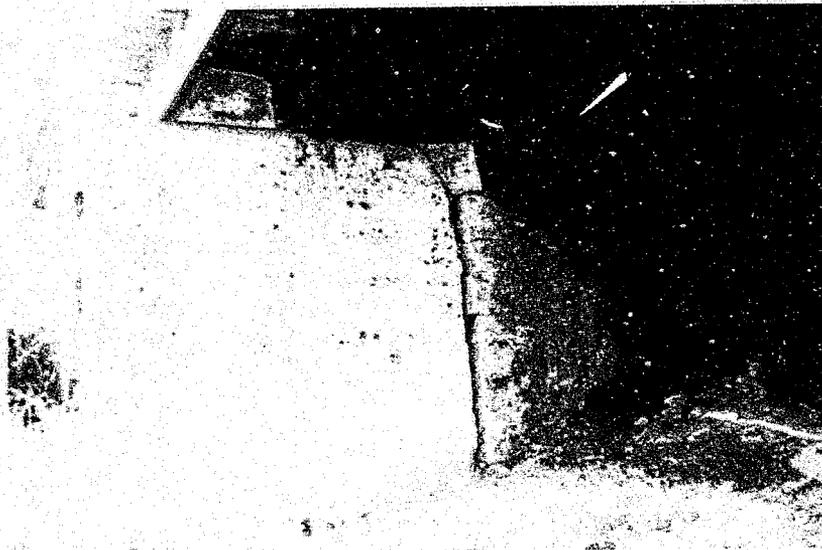
The stone masonry abutments are in fair to good condition. Picture P-4 shows the south abutment with an accumulation of debris in front of it.



P4

Picture P-5 is the wall under the sidewalk portion of the bridge; the wall appears to be pulling away from the abutment. At this time, however, there is nothing to indicate that the wall is continuing to move.

The north abutment is also in fair to good condition, although there is what appears to be a settlement crack under the sidewalk portion of the bridge; picture P-6 shows the north abutment with the crack.



P5



P6

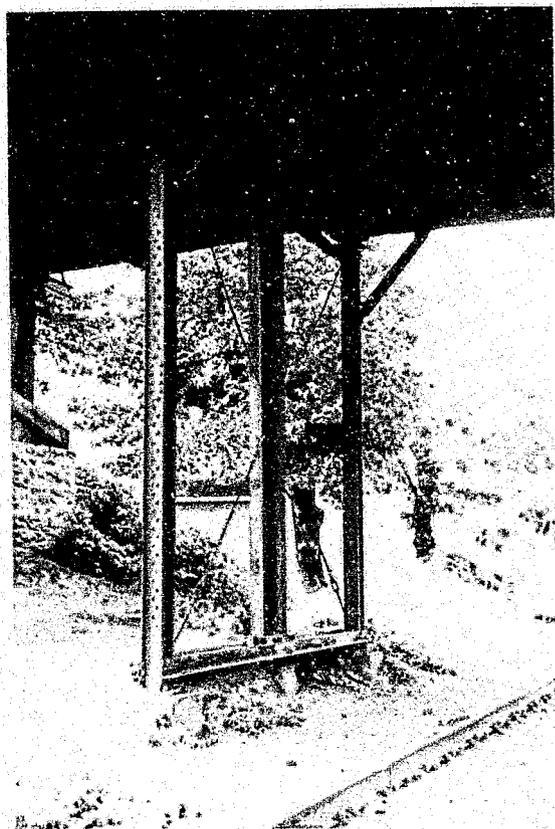
TIMBER AND STEEL PIER BENTS

The timber and steel pier bents are generally in good condition. The foundations for the bents are also in good condition. Pictures P-7 and P-8 show the timber and steel bents respectively at the south end of the bridge.



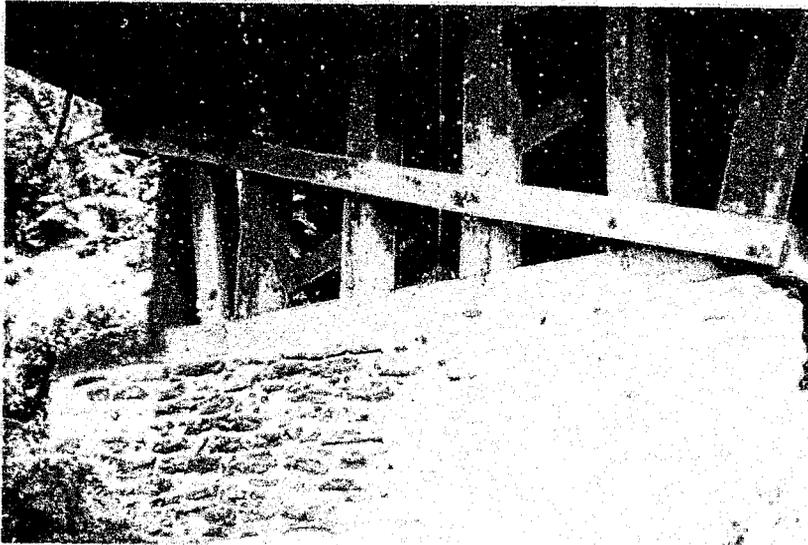
P7

Note in P-8, the middle member is a timber leg. Our records indicate this piece was not part of the original design, but added later.



P8

Pictures P-9 & P-10 show the timber and steel bents at the north end of the bridge. Again, these supports are in good condition.



P9



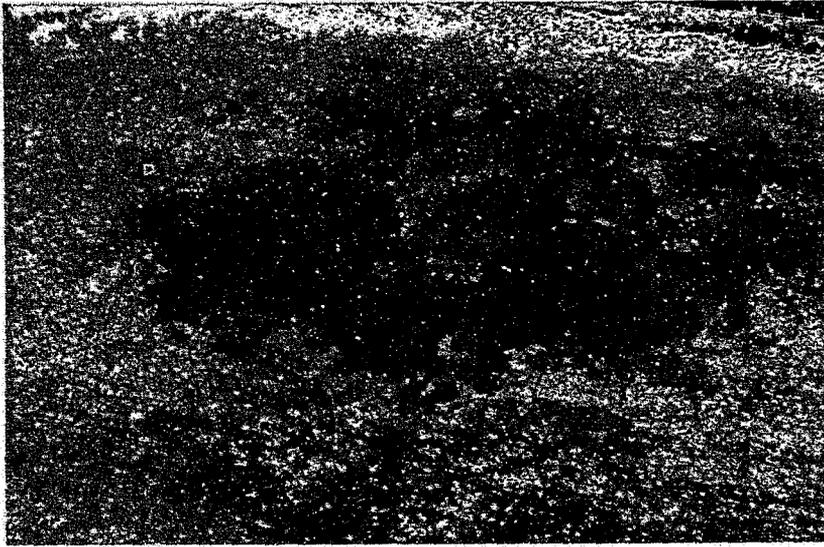
P10

BRIDGE DECK

The bridge deck is in fair condition. The deck is made up of wood planks laid on edge and covered with a bituminous overlay. The deck was overlaid with an additional thickness of bituminous material in the fall of 1978. Picture P-11 shows the wood planks laid on edge as they are exposed behind the timber curb along the bridge. The planks are deteriorating and at certain locations on the bridge, are flexing under wheel loads causing the bituminous surface to break up. Picture P-12 shows a large area on the north approach where this condition exists.



P11



P12

FLOOR BEAMS

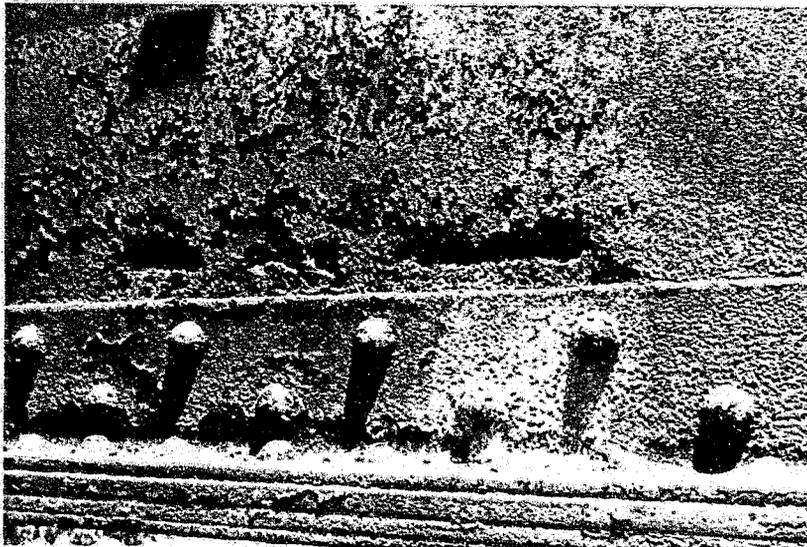
The timber floor beams of the main span over the railroad are in fair to good condition. The beams measure roughly 10" x 12" x 19' and are spaced at 20" center to center. The beams bear directly on the lower flange of the main girders.

STRINGERS

The stringers on the approach spans are timber stringers that measure roughly 8" x 12" and are spaced about 19" center to center. The stringers are in fair to good condition.

GIRDERS

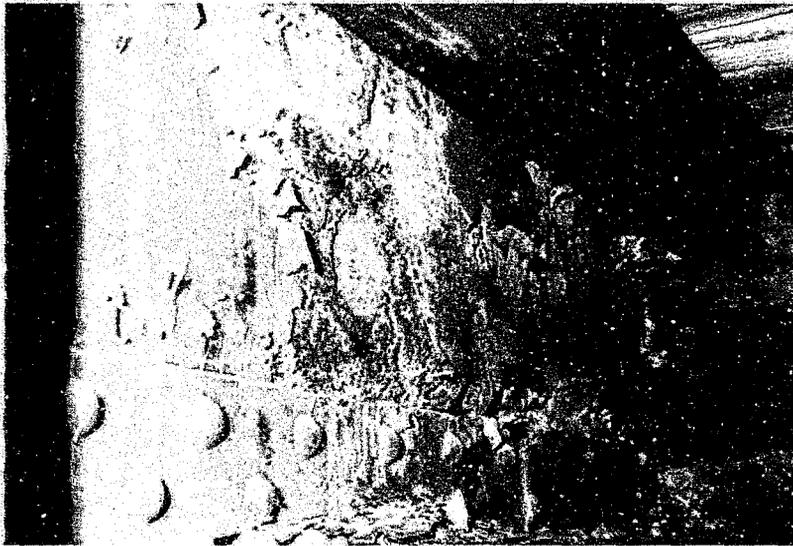
The two girders of the main span over the railroad are steel girders that measure approximately 5'-8" deep. The girder on the west side is in fair condition due to a significant amount of rust and delamination occurring along the girder. The girder on the east side is in poor condition due to the web being corroded and rusted thru. This deterioration is occurring toward the center of the span. Pictures P-13 and P-14 showed sections that are completely rusted through. P-15 shows an area of heavy corrosion which will, when struck with a hammer, break thru.



P13



P14



P15

Another area of significant delamination is along some of the stiffeners - Picture P-16 shows this condition. Note the condition of the rivet heads which in many cases, no longer exist.



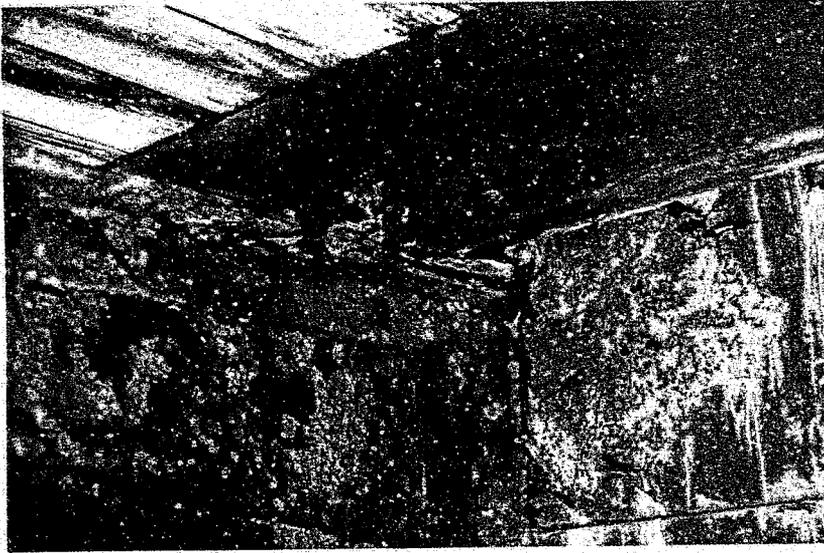
P16

SIDEWALK

The sidewalk on the east side is cantilevered on brackets riveted to the main girder. Picture P-17 shows the sidewalk from above; it is in fair to good condition. From below, picture P-18, the brackets are corroded but not to the extent that there would be concern for the safety of pedestrians. The wood stringers under the sidewalk and wood planking, which is overlaid with bituminous material, are in fair to good condition. This can be seen in picture P-18 and P-19



P17



P18



P19

RATING

Based on our observations and measurements made during the detailed close-up inspection, Amtrak rated the bridge to determine its load carrying capacity.

All structural members were rated using a single lane load; a result of the narrow roadway. Neither sidewalk live load, nor fatigue were considered in the computations of the rating.

The following is a summary of our ratings for this structure.

<u>Member</u>	<u>Inventory</u>	<u>Operating</u>
Plate Girder (Main Span)	H2.3	H4.0
Timber Floor Beams (Main Span)	H11.7	-
Timber Floor Stringers (Approach Span)	H16.6	-
Steel Columns	HS12	HS26

Additional comments are necessary with regard to this rating.

First there is a 12" steel I beam which carries the load from the wood stringers of the approach spans to the steel columns under the main span. This beam is of sufficient capacity and in good condition, and is therefore, not a critical member. Secondly, the interior wood column between the steel columns is also in good condition and not a critical member. The steel girder on the east side is the critical member in this structure. The loss of web along the bottom of the girder extends basically between stiffeners. Because of the break in continuity of the web, we analyzed the girder at that section as though the bottom flange was simply supported between the stiffeners; thus the inventory rating of H 2.3.

CONCLUSIONS AND RECOMMENDATIONS

The thru girder bridge carrying Primos Ave. over 4 tracks of the National Railroad Passenger Corporation in the Borough of Folcroft, Pa. is in fair condition except for the plate girder on the east side of the main span which is in poor condition.

The bridge is made up of 5 spans; the approach spans, two on each side, have a wood plank deck on timber stringers, which in turn, are supported by stone abutments and a timber bent between the first and second span and a steel bent between the second and main span. The abutments and piers are generally in good condition. The timber deck beneath the bituminous overlay is deteriorating and in certain locations on the bridge, tends to flex under wheel loads causing the surface to break-up. The floor beams and stringers throughout the bridge are in good condition. The two main girders over the railroad are in fair to poor condition. The east girder is very bad, due to the loss of a significant portion of the web. It is for this reason that the bridge is currently posted for 2.5 tons. The sidewalk is in good condition, although the sidewalk brackets cantilevered from the girder are deteriorating.

Based on Amtrak's inspection and analysis of the structure, we feel that, in order to provide for the safety and accommodation of the public, this structure should be replaced with a bridge designed and built to present-day highway standards. We estimate that exclusive of any right-of-way damages, a new structure would cost approximately \$350,000.00.

APPENDIX

**OVERSIZE
DOCUMENT(S)**

MAP —

March 1, 1979
E-54-7.58

RECEIVED

MAR 7 1979



Mr. C. J. McElwee, Secretary
Pennsylvania Public Utility Commission
P.O. Box 3265
Harrisburg, PA 17120

SECRETARY'S OFFICE
Public Utility Commission

Re: O.H. #7.58 Folcroft Ave.
P.U.C. C-20235

Dear Mr. McElwee:

Submitted herewith for the Commission's review are four (4) copies of a detailed inspection report prepared by Amtrak.

The subject bridge was inspected at the same time three (3) other overhead highway bridges in the immediate vicinity were also inspected in accordance with P.U.C. orders.

We hereby certify that by U.S. Mail First Class Postage Paid, we are sending copies of the report to all parties of record in this action.

Very truly yours,

A. B. Smythe, P.E.
Asst. Chief Engineer, Structures

cc: W. S. Armentrout
D. F. Sullivan
C. Helmetag, Jr.
E. H. Huss
D. E. Auerbach
J. A. Damico, Jr.
F. P. Connors
J. T. Sullivan
I. Oncu

**DOCUMENT
FOLDER**

**BUREAU OF
MAR 7 1979
RAIL
TRANSPORTATION**

RHC/ec

March 29, 1979

3. 20234

A. B. Snythe, P.E. Assistant
Chief Engineer, Structures
National Railroad Passenger Corporation
Northeast Corridor Operations
1617 John F. Kennedy Boulevard
Philadelphia, Pennsylvania 19103

V. P.U.C.
M.I.S.

MAR 29 1979

Borough of Folcroft

v.

Penn Central Transportation Company, Department of
Transportation of the Commonwealth of Pennsylvania;
County of Delaware, Southeastern Pennsylvania
Transportation Authority, Consolidated Rail Corporation
and National Railroad Passenger Corporation

Dear Mr. Snythe:

Receipt is acknowledged of your letter dated
March 22, 1979 advising that Amtrak inspected the
Primos Avenue bridge on March 21, 1979; that the
inspection did not reveal any changes to the structure
since the previous month's inspection and that no
overweight vehicles crossed the bridge during the time
it was being inspected in the above entitled proceeding.

Your letter will receive the Commission's
attention.

Very truly yours,

for William P. Thierfelder
Acting Secretary

cc: Law Bureau - w/copy of letter
Office of Administrative Law Judge - w/copy of letter
Bureau of Rail Transportation - w/copy of letter

CKG:jl



March 22, 1979
E-54-7.75

MAR 26 1 25 PM '79
RECEIVED
SECRETARY'S OFFICE
PUBLIC UTILITY
COMMISSION



Mr. C.J. McElwee, Secretary
Pennsylvania Public Utility Commission
P.O. Box 3265
Harrisburg, PA 17120

PA. P.U.C.
M.I.S.

MAR 23 1979

Re: C-20235 Primos Ave.
Folcroft, PA OH 7.75

Dear Mr. McElwee:

In accordance with the Commission's order in the subject action, Amtrak inspected the Primos Ave. bridge on Wednesday, March 21, 1979.

The inspection did not reveal any changes to the structure since the previous month's inspection. No overweight vehicles crossed the bridge during the time it was being inspected.

Very truly yours,

A.B. Smythe, P.E.
Asst. Chief Engineer, Structures

cc: D.H. Brunner
D.E. Auerbach
H.G. Zahn
I. Oncu

RHC/ec

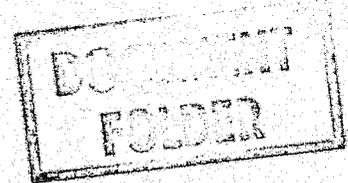
DOCUMENT
FOLDER

DOCKETED
COMPLAINT DOCKET
APR 3 1979
ENTRY No. C

May 18, 1979

C. 20235

A. B. Smythe, P.E.
Assistant Chief Engineer, Structures
National Railroad Passenger Corporation
Northeast Corridor Operations
1617 John F. Kennedy Boulevard
Philadelphia, Pennsylvania 19103



Borough of Folcroft

v.

Penn Central Transportation Company, Department of
Transportation of the Commonwealth of Pennsylvania,
County of Delaware, Southeastern Pennsylvania
Transportation Authority, Consolidated Rail Corporation
and National Railroad Passenger Corporation

Dear Mr. Smythe:

Receipt is acknowledged of your letter dated
May 14, 1979 advising that the bridge was inspected on
May 3, 1979 and the condition of the bridge has not
noticeably changed since the last inspection and that
you are notifying PennDOT and Borough of Folcroft of the
inspection in the above entitled proceeding.

Your letter will receive the Commission's
attention.

Very truly yours,

for William P. Thierfelder
Acting Secretary

cc: Law Bureau - w/copy of letter
Office of ALJ - w/copy of letter
Bureau of Rail Transportation - w/copy of letter

CKG:jl

May 14, 1979
E-54-7.75

MAY 16 1 48 PM '79



RECEIVED
SECRETARY'S OFFICE
PUBLIC UTILITY
COMMISSION

Mr. William Thierfelder, Acting Secretary
Pennsylvania Public Utility Commission
P.O. Box 3265
Harrisburg, PA 17120

Re: PUC C-20235 Primos Avenue
OH 7.75 - Folcroft
Monthly Inspection

Dear Mr. Thierfelder:

We wish to advise the Commission that the subject bridge was inspected on Thursday, May 3, 1979, and that the condition of the bridge has not noticeably changed since the last inspection.

We are notifying PennDOT and the Boro of Folcroft of the inspection in accordance with the order issued in this action.

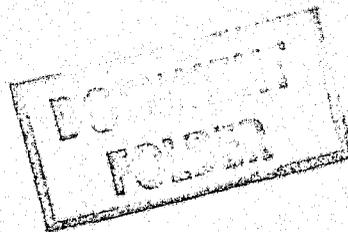
Very truly yours,

A handwritten signature in cursive script, appearing to read "A. B. Smythe".

A. B. Smythe, P.E.
Asst. Chief Engineer, Structures

cc: W. S. Armentrout
H. G. Zahn
D. E. Auerbach
I. Oncu

RHC/ec

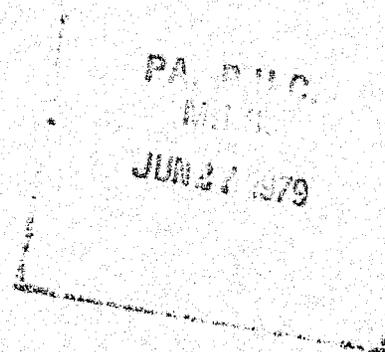


DOCKETED
COMPLAINT DOCKET
MAY 22 1979

June 27, 1979

C. 20235

A. B. Smythe, P.E.
Assistant Chief Engineer, Structures
National Railroad Passenger Corporation
Northeast Corridor Operations
1617 John F. Kennedy Boulevard
Philadelphia, Pennsylvania 19103



Borough of Folcroft

v.

Penn Central Transportation Company, Department of Transportation of the
Commonwealth of Pennsylvania, County of Delaware, Southeastern Pennsylvania
Transportation Authority, Consolidated Rail Corporation and National
Railroad Passenger Corporation

Dear Mr. Smythe:

Receipt is acknowledged of your letter dated June 14, 1979,
advising that Amtrak has replaced certain deteriorated stringer seat
angles in the inspection report submitted to the Commission on March 1,
1979 and that the work was completed by the end of May in the above
entitled proceeding.

Your letter will receive the Commission's attention.

Very truly yours,

for William F. Thierfelder
Acting Secretary



cc: Law Bureau - w/copy of letter
Bureau of Rail Transportation - w/copy of letter
Office of ALJ - w/copy of letter

CKG:j1

June 14, 1979
E-54-7.58

JUN 13 11 05 AM '79



SECRETARY'S OFFICE
PUBLIC UTILITY
COMMISSION

Mr. William Thierfelder
Acting Secretary
Pennsylvania Public Utility Commission
P.O. Box 3265
Harrisburg, PA 17120

PA P.U.C.

JUN 14 1979

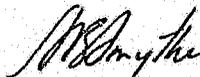
Re: OH 7.58 Folcroft Ave.
Folcroft, PA
P.U.C. C-20235

Dear Mr. Thierfelder:

We wish to advise the Commission that Amtrak has replaced certain deteriorated stringer seat angles noted in the inspection report submitted to the Commission on March 1, 1979. The work was completed by the end of May.

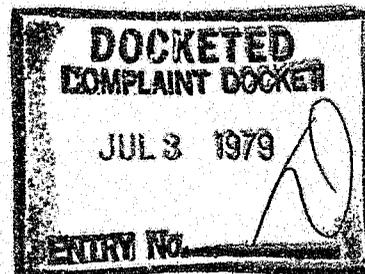
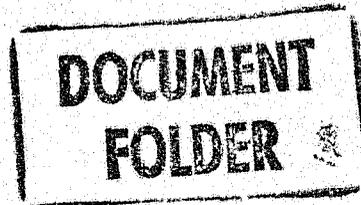
We will continue to keep the Commission advised of the condition of this bridge.

Very truly yours,


A. B. Smythe, P.E.
Asst. Chief Engineer, Structures

cc: W. S. Armentrout
H. G. Campbell
I. Oncu

RHC/ec



August 10, 1979

G. 20235

A. D. Smythe, P.E.
Assistant Chief Engineer, Structures
National Railroad Passenger Corporation
Northeast Corridor Operations
1617 John F. Kennedy Boulevard
Philadelphia, Pennsylvania 19103

PA. P.U.C.
M.I.S. ✓

AUG 10 1979

Borough of Solebort

v.

Penn Central Transportation Company, Department of Trans-
portation of the Commonwealth of Pennsylvania, County of
Delaware, Southeastern Pennsylvania Transportation Authority,
Consolidated Rail Corporation and National Railroad Passenger
Corporation

Dear Mr. Smythe:

Receipt is acknowledged of your letter dated
August 3, 1979 containing relative information in the
above entitled proceeding.

Your letter will receive the Commission's
attention.

Very truly yours,

For William P. Thierfelder
Secretary

cc: Office of ALJ - w/copy of letter
Bureau of Rail Transportation - w/copy of letter
Law Bureau - w/copy of letter

CKG:jl



RECEIVED

August 3, 1979
E-54-7.75
E-54-7.58

AUG 7 1979

SECRETARY'S OFFICE
Public Utility Commission



Mr. William Thierfelder
Acting Secretary
Pennsylvania Public Utility Commission
P.O. Box 3265
Harrisburg, PA 17120

PAT P.U.C.
M.I.S. ✓

AUG 10 1979

Re: OH 7.75 - Primos Ave.
OH 7.58 - Folcroft Ave.
P.U.C. C-20235

Dear Mr. Thierfelder:

We wish to advise the Commission that in accordance with the order in this Complaint, Amtrak on Wednesday, August 1, 1979 inspected the subject bridge. The inspection did not reveal any changes in the condition of either structure with the exception of the new angles beneath certain stringers of the Folcroft Ave. bridge.

Amtrak will continue the inspections in accordance with the Commission's order.

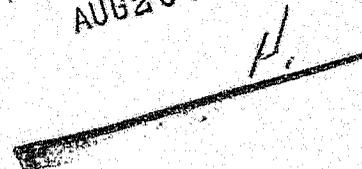
Very truly yours,

A. B. Smythe
A. B. Smythe, P.E.
Asst. Chief Engineer, Structures

cc: W. S. Armentrout
I. Oncu

DOCKETED
COMPLAINT DOCKET
AUG 20 1979

RHG/ec





COMMONWEALTH OF PENNSYLVANIA
PENNSYLVANIA PUBLIC UTILITY COMMISSION
P. O. BOX 3265, HARRISBURG, Pa. 17120

September 6, 1979

IN REPLY PLEASE
REFER TO OUR FILE

RECEIVED

OCT 02 1979

Chairman's Office
Pennsylvania Public
Utility Commission

The Honorable Edward G. Biester, Jr.
Attorney General
Commonwealth of Pennsylvania
Department of Justice
Strawberry Square
Harrisburg, Pennsylvania 17120

Re: Enforcement of Pennsylvania Public Utility
Commission Orders C. 20235, C. 21424,
I.D. 158, I.D. 174, and I.D. 225 Against
The Pennsylvania Department of Transportation

Dear General Biester:

By letters dated March 19, 1979 and April 12, 1979,
(copies enclosed herewith) your office had directed Transpor-
tation Secretary, Thomas D. Larson, to carry out the Pennsyl-
vania Public Utility Commission's orders issued in the matters
listed above. The nature of the Department's obligations were
set forth in prior correspondence to your office.

Please be advised that the Department has not yet
complied with the mandates of any of the five subject orders.
Therefore, in accordance with Section 503 of the Public
Utility Code (66 Pa. C.S. §503) you are requested to take
such additional steps as are necessary to compel the Depart-
ment's adherence to the orders.

Very truly yours,

George M. Kashi
Chief Counsel

GMK:JJK:ss

cc: ✓ W. Wilson Goode, Chairman
Michael Johnson, Commissioner
Louis J. Carter, Commissioner
John B. Wilson, Deputy Chief Counsel
James J. Kutz, Assistant Counsel

DOCUMENT
FOLDER

March 19, 1979

Honorable Thomas D. Larson
Secretary of Transportation
1200 Transportation & Safety Building
Harrisburg, PA.

Re: Borough of Folcroft v. Penn Central Transportation Company, Department of Transportation of the Commonwealth of Pennsylvania, County of Delaware, Southeastern Pennsylvania Transportation Authority, Consolidated Rail Corporation and National Railroad Passenger Corporation.

I.D. 158 - Investigation upon the Commission's own motion for the purpose of determining the safety, structural adequacy and load-carrying capacity of the bridge carrying State Highway Route 726 (Orange Street) over and above the grade of tracks of Consolidated Rail Corporation, in Upper Providence Township, Delaware County.

I.D. 174 - Investigation on the Commission's own motion into the matters and things relevant to the structural adequacy and load-carrying capacity of the bridge carrying State Highway Route 581 over and above the grade of the tracks of Consolidated Rail Corporation, in Cranberry Township, Venango County, as pertain to the proper service, accommodation, convenience and safety of the traveling public using the highway at this location.

Dear Secretary Larson:

Attached for your information is a copy of Section 503 of the Public Utility Code (66 Pa. C.S. § 503) which charges me with the responsibility for enforcing orders of the Public Utility Commission. The Commission has requested me to



COMMONWEALTH OF PENNSYLVANIA
OFFICE OF ATTORNEY GENERAL
HARRISBURG, PA. 17120

EDWARD G. BIESTER, JR.
ATTORNEY GENERAL

April 12, 1979

Honorable Thomas D. Larson
Secretary of Transportation
1200 Transportation & Safety Building
Harrisburg, PA.

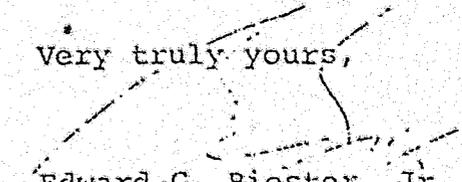
Re: I.D. 225 - Investigation upon the Commission's own motion for the purpose of determining the structural adequacy and load-carrying capacity of the bridges carrying State Highway Route 23072 (Sharon Avenue) and State Highway Route 23073 (Calcon Hook Road) over and above the grade of the tracks of Penn Central Transportation Company, in the Borough of Sharon Hill, Delaware County, as pertains to the proper service, accommodation, convenience and safety of the traveling public utilizing same.

C. 21424 - Borough of Tullytown v. Penn Central Transportation Company, Pennsylvania Department of Transportation, County of Bucks, Consolidated Rail Corporation, and National Rail Passenger Corporation.

Dear Secretary Larson:

In accordance with Section 503 of the Public Utility Code (66 Pa. C.S. § 503), a copy of which I sent you on March 1, 1979, the Public Utility Commission has now requested me to enforce its orders against your Department in the two cases cited above. You are specifically directed, therefore, that the Commission's orders in the above cases be carried out.

Very truly yours,


Edward G. Biester, Jr.
Attorney General

October 18, 1979

C. 20235

A.E. Smythe, P.E.
Assistant Chief Engineer-Structures
National Railroad Passenger Corporation
Northeast Corridor Operations
1617 John F. Kennedy Boulevard
Philadelphia, Pennsylvania 19103

Borough of Folcroft
v.

Penn Central Transportation Company Company, Department of Transportation
of the Commonwealth of Pennsylvania, County of Delaware, Southeastern
Pennsylvania Transportation Authority, Consolidated Rail Corporation and
National Railroad Passenger Corporation

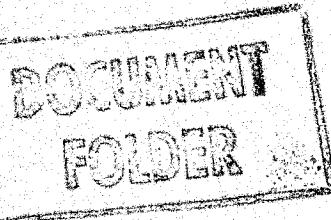
Dear Mr. Smythe:

Receipt is acknowledged of your letter dated September 26,
1979, containing the monthly inspection report in accordance with the
Commission's order issued in the above entitled proceeding.

Your letter will receive the Commission's attention.

Very truly yours,

for William F. Thierfelder
Secretary



cc: Office of Administrative Law Judge - w/copy of letter
Bureau of Rail Transportation - w/copy of letter
Law Bureau - w/copy of letter

CKG:jl

RECEIVED

OCT 9 1979

SECRETARY'S OFFICE
Public Utility Commission



Sept. 26, 1979
File: E-54-7.58, 7.75

Mr. William Thierfelder, Secretary
Pennsylvania Public Utility Commission
P.O. Box 3265
Harrisburg, Pa. 17120

PA. P.U.C.
M.I.S.
OCT 18 1979

Re: Pa. P.U.C. C-20235
OH 7.75 - Primos Ave., OH 7.58 - Folcroft Ave.

Dear Mr. Thierfelder:

We wish to advise the Commission that in accordance with the order issued in the subject complaint, Amtrak performed the monthly inspection of the Primos Avenue and Folcroft Avenue bridges on Tuesday, Sept. 25, 1979.

The bridges appeared to be basically in the same condition as noted at the time of the last inspection. The Primos Avenue bridge, however, has been partially painted; the east girder above roadway level and the pedestrian protection barrier have been painted silver. We do not know who did the painting but perhaps it was done by a local authority to cover some graffiti because the paint is just cosmetic and will afford minimal protection as the surface was not properly prepared for the paint.

Very truly yours,

A.B. Smythe, P.E.
Assistant Chief Engineer-Structures

RHC/da

cc: W.S. Armentrout
K.W. Walker
D.E. Auerbach
I. Oncu

DOCUMENT
FOLDER

DOCKETED
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January 9, 1980
E-54-7.75

JAN 11 10 14 AM '80

FILE
C. K. G.

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SECRETARY'S OFFICE
PUBLIC UTILITY
COMMISSION



William P. Thierfelder, Secretary
PA PUC
P.O. Box 3265
Harrisburg, PA 17120

Re; PA P.U.C. C-20235 - Primos Ave.
Boro of Folcroft
OH #7.75

Dear Mr. Thierfelder:

We wish to advise the Commission that the subject bridge was inspected on Jan. 3, 1980. The condition of the bridge appeared to be no different from the condition noted during previous inspections.

The pedestrian protective fence on the north side needed repairs. This was brought to the attention of our B&B Dept. who proceeded to make the repairs.

Amtrak will continue to inspect this structure according to the Commission's order in this complaint.

Very truly yours,

A. B. Smythe, P.E.
Asst. Chief Engineer, Structures

cc: W. S. Armentrout
I. Oncu

RHC/ec

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FEB 3 1980
TC

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FOLDER

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FEB 14 1980

Amtrak



February 11, 1980
E-54-7.75

SECRETARY'S OFFICE
Public Utility Commission

Mr. William P. Thierfelder, Secretary
Pennsylvania Public Utility Commission
P.O. Box 3265
Harrisburg, PA 17120

Re: PA P.U.C. C-20235
OH 7.75 Primos Avenue
Boro of Folcroft

Dear Mr. Thierfelder:

We wish to advise the Commission that in accordance with the subject order, Amtrak inspected the Primos Ave. bridge on Feb. 8, 1980. The inspection did not reveal any noticeable changes in the condition of the bridge from previous inspections.

We will continue with the monthly inspections and report our findings as ordered.

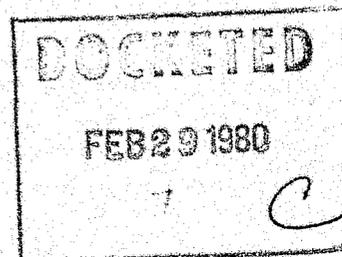
By copy of this letter we are notifying PennDOT and the Boro of our inspection.

Very truly yours,

A. B. Smythe, P.E.
Asst. Chief Engineer, Structures

cc: K. W. Walker
D. E. Auerbach

RHC/ec



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RECEIVED
SECRETARY'S OFFICE
PUBLIC UTILITY
COMMISSION
April 11, 1980

Mr. William P. Thierfelder, Secretary
Pennsylvania Public Utility Commission
P. O. Box 3265
Harrisburg, PA 17120

RE: PaPUC C-20235
OH 7.75 Primos Avenue
Folcroft, PA

Dear Mr. Thierfelder:

In accordance with the Commission's order in the subject proceeding, on Thursday, April 10, 1980, Amtrak inspected the bridge carrying Primos Avenue over the railroad.

The inspection did not reveal any change in the condition of the bridge from that noted during previous inspections.

Amtrak will continue the monthly inspections as ordered.

Very truly yours,

A. B. Smythe, PE.
Assistant Chief Engineer -
Structures

RHC/es

cc: W. S. Armentrout
D. E. Auerbach
K. W. Walker

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APR 21 1980
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Amtrak



July 28, 1980

SECURITY OFFICE
FBI
PHILADELPHIA

File: E-54-7.75

Mr. William P. Thierfelder, Secretary
Pennsylvania Public Utility Commission
Post Office Box 3265
Harrisburg, Pennsylvania 17120

RE: Pa PUC Docket #C-20235
O.H. 7.75 Primos Avenue
Folcroft, Pennsylvania

Dear Mr. Thierfelder:

In accordance with the Commission's order in the subject complaint, Amtrak inspected the Primos Avenue bridge on Friday, July 25, 1980. The inspection did not reveal any changes in the condition of the bridge from that noted during previous inspections except for a hole punched thru the pedestrian protection shield on the east side of the bridge. Amtrak B&B forces will be notified to repair the shield.

Very truly yours,

A. B. Smythe, P.E.
Asst Chief Engr/Structures

cc: W. S. Armentrout
K. E. Walker
D. E. Auerbach

RHC/edd

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AUG 05 1980
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OCT 20 1980



October 14, 1980
File: E-54-7.75

SECRETARY'S OFFICE
Public Utility Commission

Mr. William P. Thierfelder, Secretary
Pennsylvania Public Utility Commission
P.O. Box 3265
Harrisburg, PA 17120

Re: O.H. 7.75 Primos Avenue
Folcroft, PA
Pa. PUC C-20235

Dear Mr. Thierfelder:

We wish to advise the Commission that in accordance with their Order in the subject complaint, Amtrak inspected the Primos Avenue bridge in Folcroft on Monday, October 13, 1980.

The inspection did not reveal any changes in the condition of the bridge from that observed during previous inspections.

Amtrak will continue making these inspections and reporting our findings as required.

Very truly yours,

A. B. Smythe
A. B. Smythe
Asst. Chief Engineer - Structures

RHC/sp

cc: D. E. Auerbach, Solicitor, Borough of Folcroft
K. W. Walker, Chief Utility Engineer, PennDOT

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SECRETARY'S OFFICE
Public Utility Commission

February 11, 1981



DOCUMENT
FOLDER

Mr. William P. Thierfelder, Secretary
Pennsylvania Public Utility Commission
P. O. Box 3265
Harrisburg, Pa. 17120

Re: OH 775 Primos Ave.
Folcroft, Pa.
Pa. PUC C-20236

C-20235

Dear Mr. Thierfelder:

In accordance with the Commission's order in PUC Docket C-20236, Amtrak inspected the Primos Ave. bridge on Monday, February 9, 1981.

The inspection did not reveal any noticeable changes in the condition of the substructure or superstructure; the wearing surface appears in good condition. The posting should remain unchanged.

By copy of this letter, we are advising the Borough and Penn DOT of our inspection.

Very truly yours,

A. B. Smythe, P. E.
Asst. Chief Engineer-Structures

RHC/jt

cc: Messrs. K. W. Walker, Chief Utility Engineer
Penn DOT - T&S Building
Harrisburg, Pa. 17120

D. E. Auerbach, Solicitor
Borough of Folcroft
Legal Arts Building
Media, Pa. 19063

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FEB 18 1981

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Amtrak



July 13, 1981

Mr. William P. Thierfelder
Secretary
Pennsylvania Public Utility Commission
P.O. Box 3265
Harrisburg, Pa. 17120

RECEIVED
SECRETARY'S OFFICE
PUBLIC UTILITY
COMMISSION

JUL 20 9 05 AM '81

SUBJECT: OH 7.75 - Primos Avenue - Folcroft, Pa.
PA PUC Docket #C-20235

Dear Mr. Thierfelder:

We wish to advise the Commission that in accordance with the order on Docket #C-20235, Amtrak inspected the Primos Avenue bridge on Friday, June 26, 1981.

The inspection did not reveal any significant changes in the condition of the bridge from those noted during previous inspections.

Amtrak will continue to inspect the bridge and report to the Commission as advised.

Very truly yours,

A. B. Smythe
Assistant Chief Engineer
Structures

RHC/ks

cc: Messrs. K. W. Walker
Chief Utility Engineer
Penn DOT - T&S Building
Harrisburg, Pa. 17120

D. E. Auerbach, Solicitor
Borough of Folcroft
Legal Arts Building
Media, Pa. 19063

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OCT 5 1981
SECRETARY'S OFFICE
Public Utility Commission

Mr. William P. Thierfelder, Secretary
Pennsylvania Public Utility Commission
P. O. Box 3265
Harrisburg, PA 17120

SUBJECT: OH 7.75 Primus Avenue, Folcroft, PA
PA PUC C-20235

Dear Mr. Thierfelder:

In accordance with PUC order C-20235, Amtrak performed the monthly inspection of the Primus Avenue Bridge on Friday, September 18, 1981.

The inspection revealed the condition of the bridge is the same as that noted during previous inspections.

Amtrak will continue the inspections and reporting our findings to the Boro and Commission as ordered.

Very truly yours,

A. B. Smythe
A. B. Smythe
Assistant Chief Engineer
Structures

cc: K. W. Walker - Chief Utility Engr.
D. E. Auerbach, Solicitor

DOCKETED
OCT 1 1981
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Nov 5 9 03 AM '81



NOV 03 1981
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RECEIVED
SECRETARY'S OFFICE
PUBLIC UTILITY
COMMISSION

Mr. William P. Thierfelder
Secretary
Pennsylvania Public Utility Commission
P.O. Box 3265
Harrisburg, PA 17120

SUBJECT: OH 7.75 Primos Avenue, Folcroft, PA
PA PUC C-20235

Dear Mr. Thierfelder:

In accordance with PUC Order C-20235, Amtrak performed the monthly inspection of the Primos Avenue Bridge on Friday, October 23, 1981.

The inspection revealed a problem at the north end of the bridge. The timber deck is deflecting under wheel loads over the north abutment causing potholes on the surface.

According to the Commissions order, Amtrak is to maintain the entire structure at our initial cost and expense, exclusive of the wearing surface which Penn DOT is ordered to maintain.

Amtrak will arrange to make repairs to the deck. However, because of the narrow roadway and volume of traffic over the bridge, it may be necessary to close the bridge for a short period of time. If closure is required, we will notify Penn DOT to arrange for signing and a detour. The Borough will also be notified so that emergency vehicles will know of the closing.

The bridge, in other respects, remains the same as that noted during previous inspections.

Very truly yours,

A. B. Smythe
Assistant Chief Engineer
Design and Construction

DOCUMENT
FOLDER

DOCKETED
NOV 13 1981
B

Mr. William P. Thierfelder
Page 2

cc: K. W. Walker
Chief Utility Engineer
Penn DOT - T&S Building
Harrisburg, PA 17120

D. E. Auerbach
Borough of Folcroft
Legal Arts Building
Media, PA 19063



DEC 22 1981

RHC/mda

BUREAU OF
DEC 23 1981
RAIL
TRANSPORTATION

Mr. Robert Roland
District Engineer
Pennsylvania Department of Transportation
District 6-0
200 Radnor Chester Road
St. Davids, Pennsylvania

Subject: OH 7.75 Primos Avenue
Folcroft, Pennsylvania
PA PUC-C-20235

RECEIVED
SECRETARY'S OFFICE
PUBLIC UTILITY
COMMISSION

DEC 29 9 07 AM '81

Dear Mr. Roland:

An order from the Pennsylvania Public Utility Commission, dated October 6, 1977, ordered Amtrak to maintain the sub-structure and superstructure of the Primos Avenue Bridge, exclusive of the bituminous wearing service which Pennsylvania Department of Transportation (PennDOT) maintains.

On October 23, 1981, we performed the monthly inspection of the bridge and noted that the timber deck over the north abutment has deteriorated and needs repair. The effort to maintain the bridge will require the closing of Primos Avenue and the establishment of a detour for perhaps two or three days.

Since Primos Avenue is a state highway, we request your cooperation in closing the road and setting the detour as necessary. Furthermore, PennDOT is required to restore the bituminous surface after we repair the deck.

Please contact Mr. Russel Cross at (302)-429-6465 to arrange a schedule for this work.

Very truly yours,

Paul Smythe

A. B. Smythe
Assistant Chief Engineer
Design & Construction

DOCKETED
JAN 5 1982
AB

for

JAN 23 1982

MATTHEW J. RYAN
THE SPEAKER



HOUSE OF REPRESENTATIVES
COMMONWEALTH OF PENNSYLVANIA
HARRISBURG

139 MAIN CAPITOL BUILDING
HARRISBURG, PENNSYLVANIA 17120
717-787-4610

Delaware Co.

C-20235

March 29, 1982

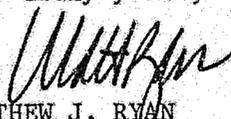
Ray A. Peteritas, Director
Bureau of Rail Transportation
408 Transportation and Safety Building
Harrisburg, Pennsylvania

Dear Mr. Peteritas:

Could you please respond to this inquiry.

Thank you very much.

Very truly yours,

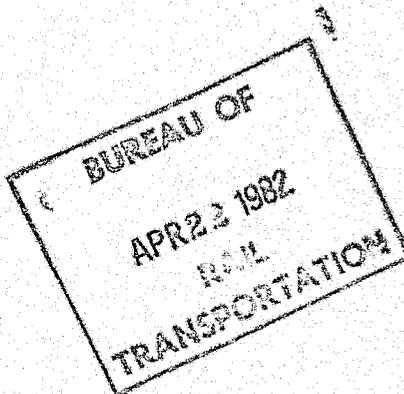
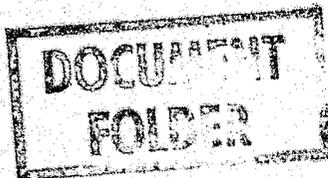

MATTHEW J. RYAN
The Speaker

MJR:cs
Enclosures (2)

RECEIVED

APR 26 1982

SECRETARY'S OFFICE
Public Utility Commission





HOUSE OF REPRESENTATIVES

COMMONWEALTH OF PENNSYLVANIA

HARRISBURG

March 29, 1982

Mr. Joseph Pastuszek
314 West Rose Valley
Wallingford, PA 19086

Dear Mr. Pastuszek:

Thank you for your letter of March 19, 1982.

I have forwarded a copy of your letter to Mr. Ray A. Peteritas,
Director of the Bureau of Rail Transportation, so that he may
answer your inquiry on the Oak Lane Bridge.

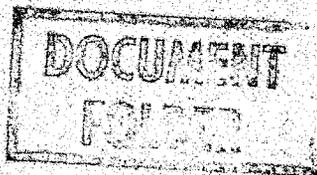
If I can be of further assistance, please do not hesitate to contact
me.

Very truly yours,

MATTHEW J. RYAN
The Speaker

MJR:cs

cc: Mr. Ray A. Peteritas



Pa. Representative

Matt Ryan

Speaker of the House

Dear Matt

3/19/82

I found your Hamburg Report well done & interesting. Please keep up the good work. Also I read where bridges are being renewed, I would like to know why the * Oak Lane Bridge over the R.R. in Glenoldon has never been replaced - I drove a truck over that bridge in 1936 - it was a dangerous bridge then and till today still is - I would like to know why all these years and still the problem exists.

many thanks

Joseph Pastuszak

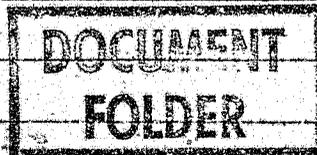
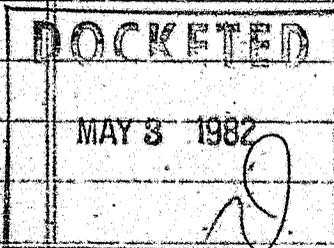
P.S. - I also found the questionnaire on how the voter came out very interesting, which should & should not be of some help in how the public thinks and how the house votes.

Joseph Pastuszak

314 W Rose Valley

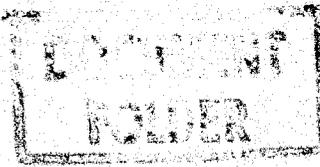
Wallingford, Pa

19086



DOCKETED

MAY 11 1982



C-20235

April 26, 1982

IN REPLY PLEASE
REFER TO OUR FILE

Mr. Joseph Pastuszek
314 West Rose Valley
Wallingford, PA 19086

In re:

Bridge carrying Primos Avenue above the tracks of National Railroad Passenger Corporation, in the Boroughs of Glenolden and Folcroft, Delaware County.

Dear Mr. Pastuszek:

Your letter dated March 19, 1982, to Speaker of the House, Matthew J. Ryan, concerning the condition of the Oak Lane Bridge over the railroad in the Borough of Glenolden, has been referred to the undersigned for response.

A search of our records, including maps and railroad crossing data, leads us to believe that the bridge in question is actually located on Primos Avenue (Legislative Route 420) and crosses over tracks of Amtrak, and that the boundary between the Boroughs of Glenolden and Folcroft is the center line of Primos Avenue. (Attached is a map of the area showing local streets and the railroad).

Assuming that the Primos Avenue Bridge is the one of your concern, please be advised that this crossing structure is presently before the Public Utility Commission upon complaint filed by the Borough of Folcroft. Pennsylvania Department of Transportation has advised us that it presently has an engineering firm engaged to make preliminary studies, including an environmental impact statement, for a new bridge at this location, and as soon as PennDOT chooses its recommended scheme, it will bring the matter to the attention of this Commission for approval.

If we are making an incorrect assumption as to the bridge of your concern, please furnish the undersigned with further details.

Very truly yours,

R. A. Peteritas, P.E., Director
Bureau of Rail Transportation

cc: Honorable Matthew J. Ryan
Speaker of the House
House Post Office
Main Capitol Building
Harrisburg, PA 17120

Chairman Shanaman

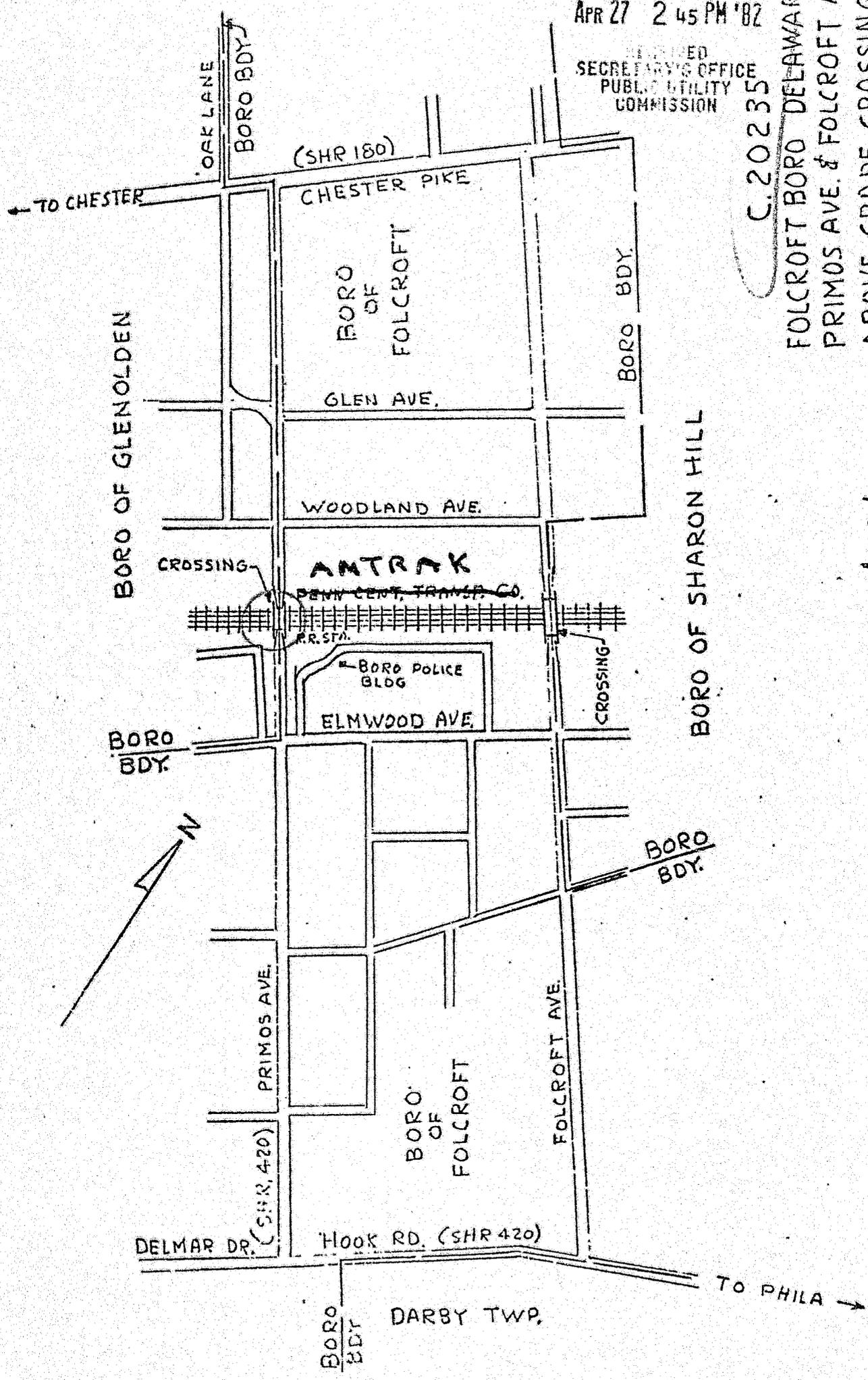
APR 27 2 45 PM '82

RECEIVED
SECRETARY'S OFFICE
PUBLIC UTILITY
COMMISSION

C. 20235

FOLCROFT BORO DELAWARE CO.
PRIMOS AVE. & FOLCROFT AVE.
ABOVE GRADE CROSSINGS
~~PENNCENT. TRANSP. CO.~~
AMTRAK

SCALE: 1"=400'



BORO OF GLENOLDEN

BORO OF SHARON HILL

BORO OF FOLCROFT

DARBY TWP.

BORO BDY.

BORO BDY.

BORO BDY.

OAK LANE

BORO BDY.

(SHR 180)

CHESTER PIKE

BORO OF FOLCROFT

BORO BDY.

GLEN AVE.

WOODLAND AVE.

AMTRAK
PENN. CENT. TRANSP. CO.
P.R. STA.

BORO POLICE BLDG

ELMWOOD AVE.

BORO BDY.

PRIMOS AVE.

(SHR 420)

DELMAR DR.

HOOK RD. (SHR 420)

FOLCROFT AVE.

TO PHILA →

← TO CHESTER

ORIGINAL

RECEIVED

Amtrak



AUG 21 1984

AUG 27 1984

**SECRETARY'S OFFICE
Public Utility Commission**

Mr. Jerry Rich
Secretary
PA Public Utility Commission
P.O. Box 3265
Harrisburg, PA 17120

Subject: Overhead Bridge #7.75
Primos Avenue
Folcroft, PA
P.U.C Docket C-20235

Dear Mr. Rich:

We wish to advise the Commission that in accordance with the order on Docket C-20235 Amtrak inspected the Primos Avenue Bridge on Tuesday, July 3, 1984 and again on Tuesday, July 31, 1984.

The inspection did not reveal significant changes in the condition of the bridge from those noted during previous inspections. The riding surface of the bridge is in satisfactory condition and the posting signs for the 2 1/2 ton load limit are in place.

We have been advised by our bridge inspector that he has observed over weight vehicles using this bridge on occasion. By copy of this letter, we are notifying the Borough of Folcroft of this matter and trust they will continue enforcing the posted load limit as they have in the past.

Amtrak will continue its periodic inspections of this bridge and report to the Commission as ordered.

Very truly yours,

James N. Michel, P.E.
Senior Director
Engineering Design

DOCKETED

AUG 28 1984

RHC/dar

cc: W. Clements
Chief Utility Engineer
PennDOT
Transportation and Safety Building
Harrisburg, PA 17120

**DOCUMENT
FOLDER**

Mr. Jerry Rich
Secretary

Subject: Overhead Bridge #7.75
Primos Avenue
Folcroft, PA
P.U.C Docket C-20235

Page 2

D. E. Auerbach
Solicitor
Borough of Folcroft
Legal Arts Building
Media, PA 19063

COMMONWEALTH OF PENNSYLVANIA

DEPARTMENT OF TRANSPORTATION

DRAWINGS
AUTHORIZING
ACQUISITION OF RIGHT-OF-WAY
FOR

LEG. ROUTE 420 SECTION 02A R/W
IN DELAWARE COUNTY

From Sta. 80+39.00 To Sta. 90+32.00 Length 993.00 Ft. 0.188 Mi.

THE PUBLIC UTILITY COMMISSION WILL, APPROPRIATE PROPERTY IN COMPLAINT DOCKET NO 20233 BETWEEN STATION 85+0700 AND STATION 86+0850 PURSUANT TO THE PROVISIONS OF SECTION 2702 (b) OF THE ACT OF JULY 1, 1978, PL. 598, NO. 16 (66 PS 2702). THIS PLAN PREPARED PURSUANT TO SECTION 2003 (e) OF THE ADMINISTRATIVE CODE (ACT OF APRIL 9, 1929, PL. 177, AS AMENDED BY ACT 1979-100), AND SECTION 402 (D)(1)(3) OF THE EMINENT DOMAIN CODE OF 1964, AS AMENDED

FORM 1000 (1-77) MD W-57 (2-78) OHS

TEL 6-0

DATE	TIME	BY	REMARKS

8

ORIGINAL

RECEIVED

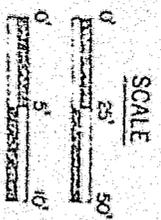
OCT 5 1984

SECRETARY OF TRANSPORTATION
PENNSYLVANIA DEPARTMENT OF TRANSPORTATION

2-20235

DOCUMENTS
FOLDER

RECEIVED
OCT 9 - 1984

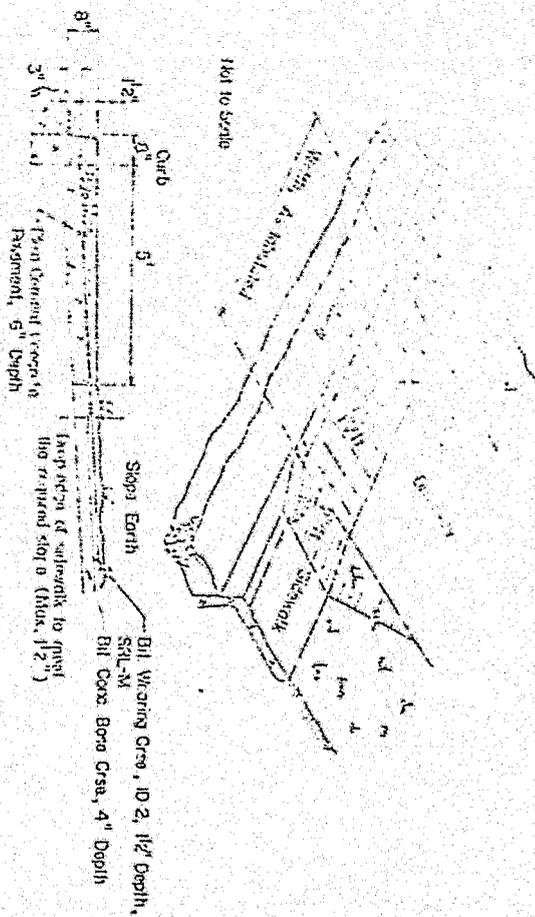


DESIGN DESIGNATION

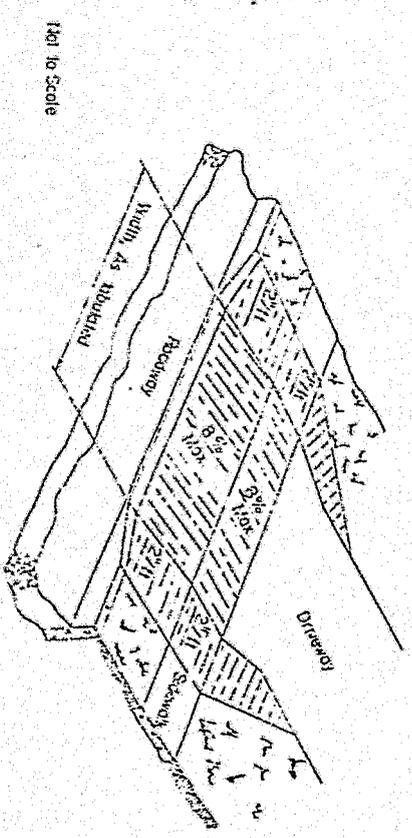
ADT - (1,200 (1985)
ADT - 12,000 (2005)
D.H.V. - 1004
D.S.0%
1.9%

MACOMBER ASSOCIATES, INC.
704 Lehigh Rd. P.O. Box 54
Camp Hill, Pa. 17011

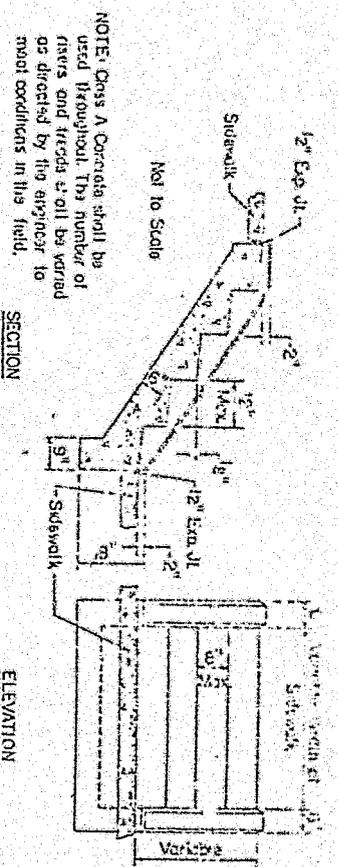
<p>HILLOUSE, JOHN</p> <p>REGISTERED PROFESSIONAL ENGINEER</p> <p>NO. 12345</p>	<p>REGISTERED PROFESSIONAL ENGINEER</p> <p>NO. 12345</p>	<p>REGISTERED PROFESSIONAL ENGINEER</p> <p>NO. 12345</p>	<p>REGISTERED PROFESSIONAL ENGINEER</p> <p>NO. 12345</p>
--	--	--	--



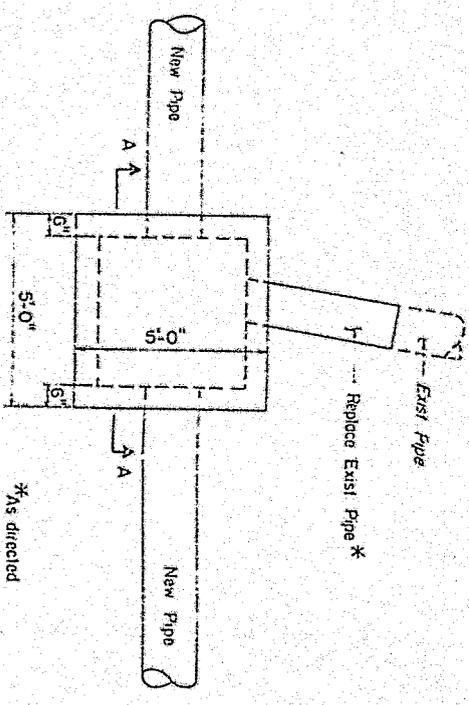
SECTION
DRIVEWAY ADJUSTMENT DETAIL



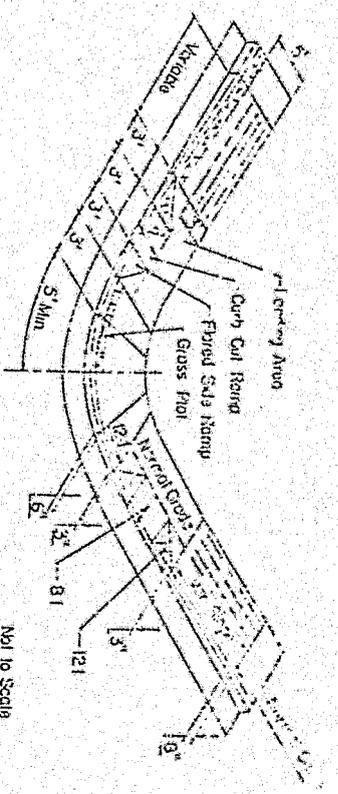
SECTION
DRIVEWAY ADJUSTMENT DETAIL



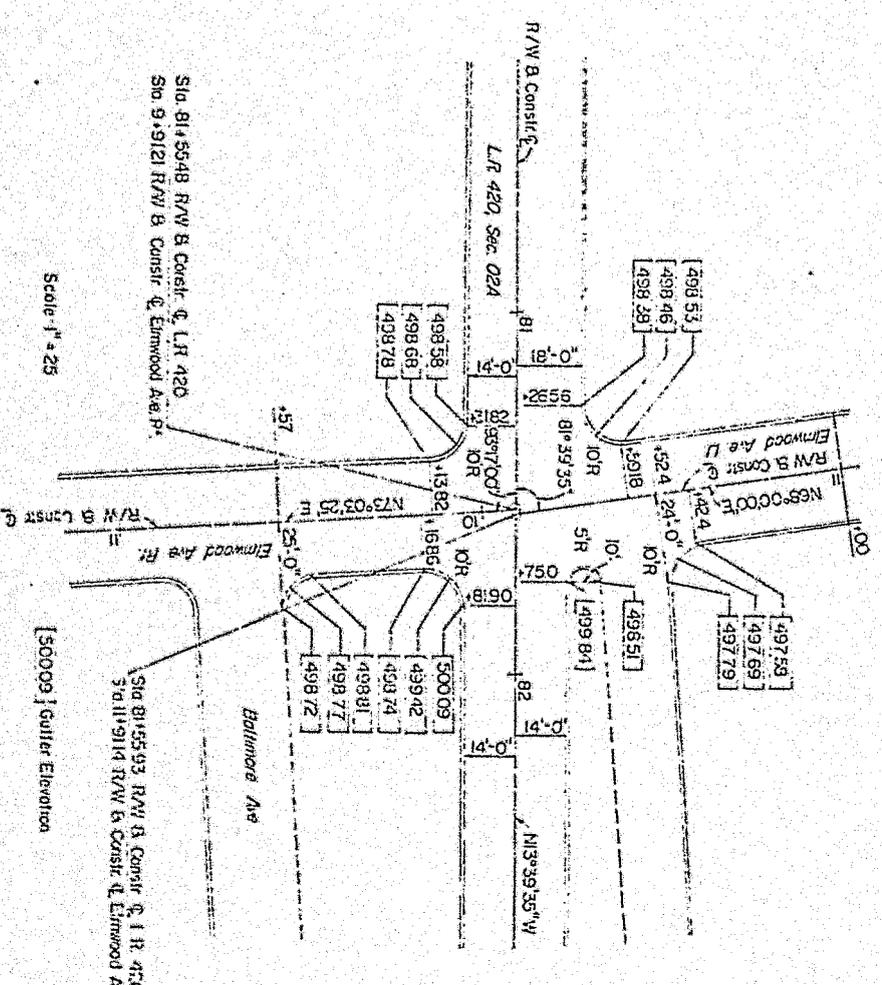
SECTION
CEMENT CONCRETE STEP DETAIL



SECTION A-A
JUNCTION BOX



ELEVATION
TYPICAL HANDICAP RAMP DETAIL



INTERSECTION DETAIL
Intersection of LR 420 & Elmwood Ave.



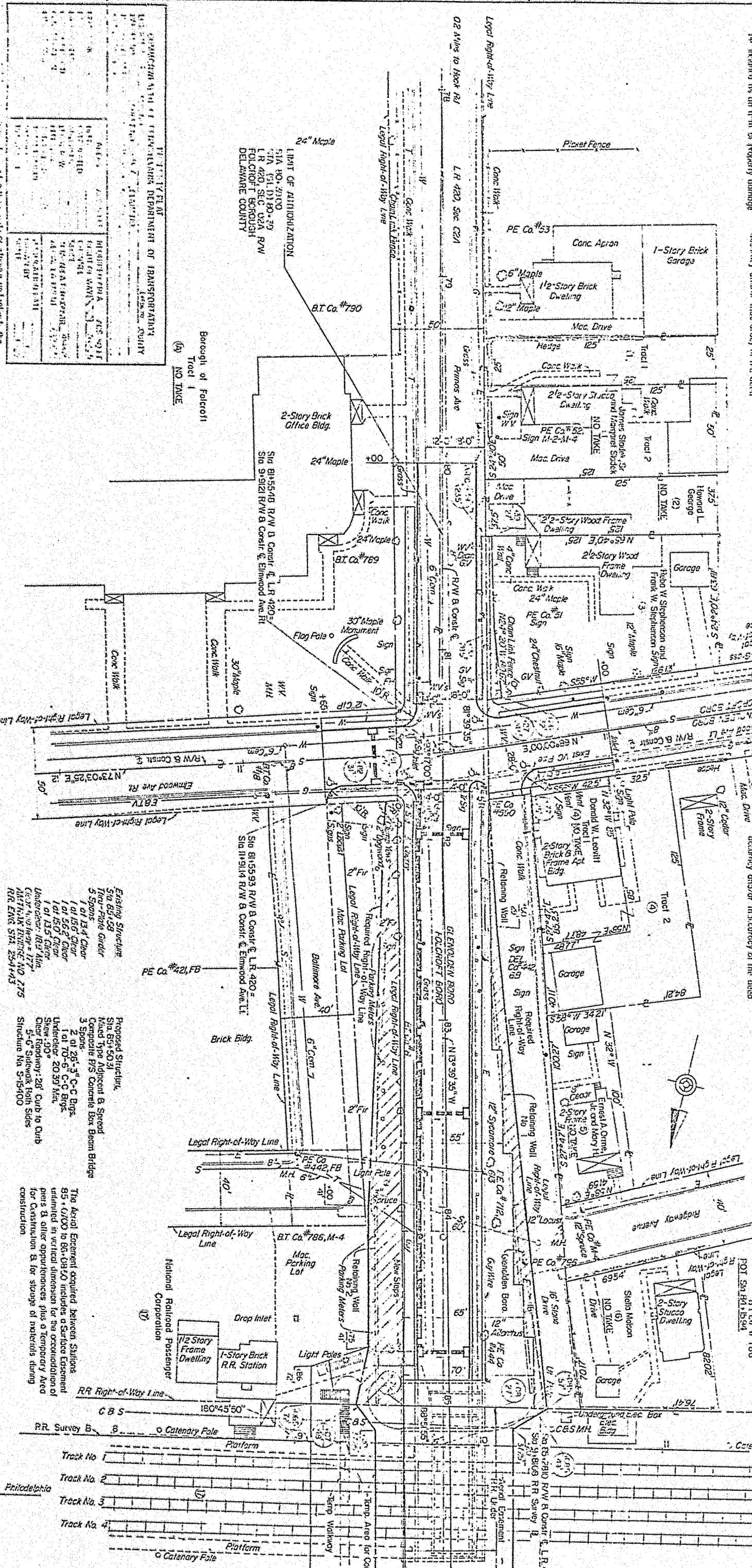
FOI No. 79-12400

NOTES: 1. All structures and trees shown on this plan are shown as they exist on the date of the survey. 2. The plan is prepared from the deed of record in the county courthouse and reflects the accuracy of the deed. 3. The plan is prepared from the deed of record in the county courthouse and reflects the accuracy of the deed.

OWNER	DEVELOPER	RECORDING DATE	SCALE

OWNER	DEVELOPER	RECORDING DATE	SCALE

OWNER	DEVELOPER	RECORDING DATE	SCALE

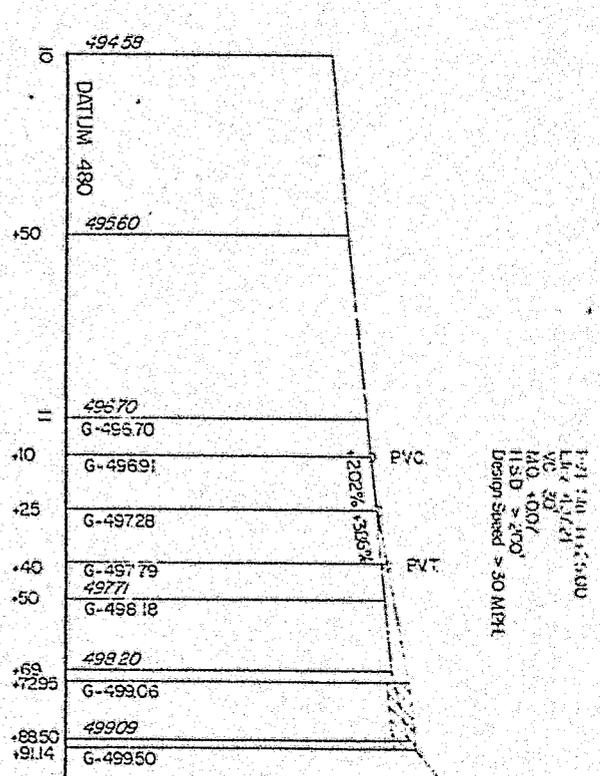
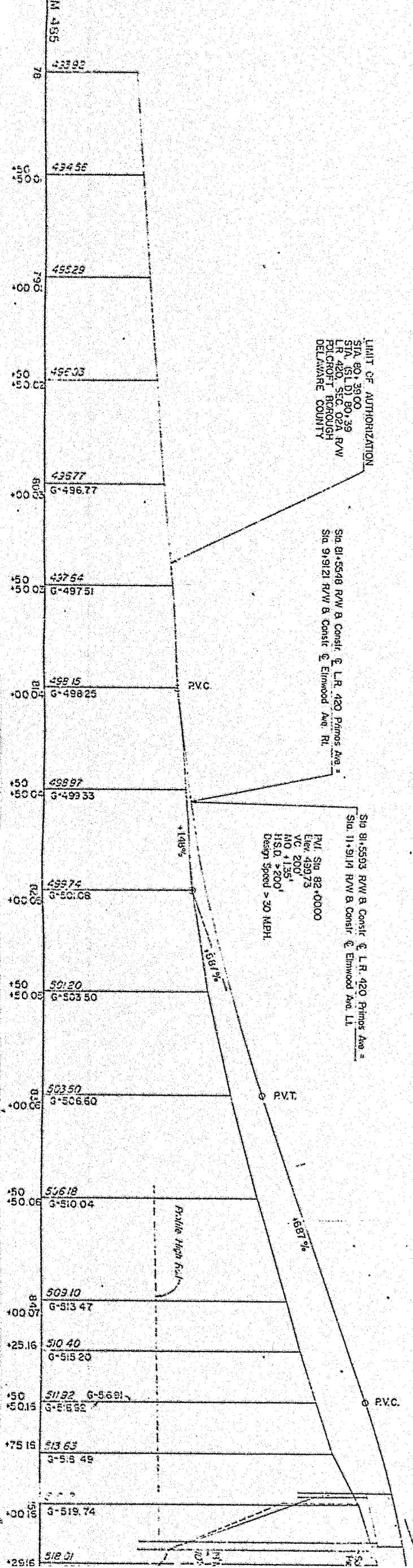


DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS
 DIVISION OF PUBLIC WORKS
 DIVISION OF PLANNING AND DESIGN
 DIVISION OF TRAFFIC ENGINEERING
 DIVISION OF TRANSPORTATION ECONOMICS
 DIVISION OF TRANSPORTATION PLANNING
 DIVISION OF TRANSPORTATION SAFETY
 DIVISION OF TRANSPORTATION SECURITY
 DIVISION OF TRANSPORTATION SYSTEMS
 DIVISION OF TRANSPORTATION TECHNOLOGY
 DIVISION OF TRANSPORTATION TRAINING
 DIVISION OF TRANSPORTATION UTILITIES

Existing Structure
 SIA 85158
 Three-Flare Girder
 5 Spans
 1 of 134' Clear
 1 of 156' Clear
 1 of 56.2' Clear
 1 of 150' Clear
 1 of 133' Clear
 Underpass: 163' Min.
 Clear Height: 177'
 5-G Sidewalk Both Sides
 Structure No. 5-15400

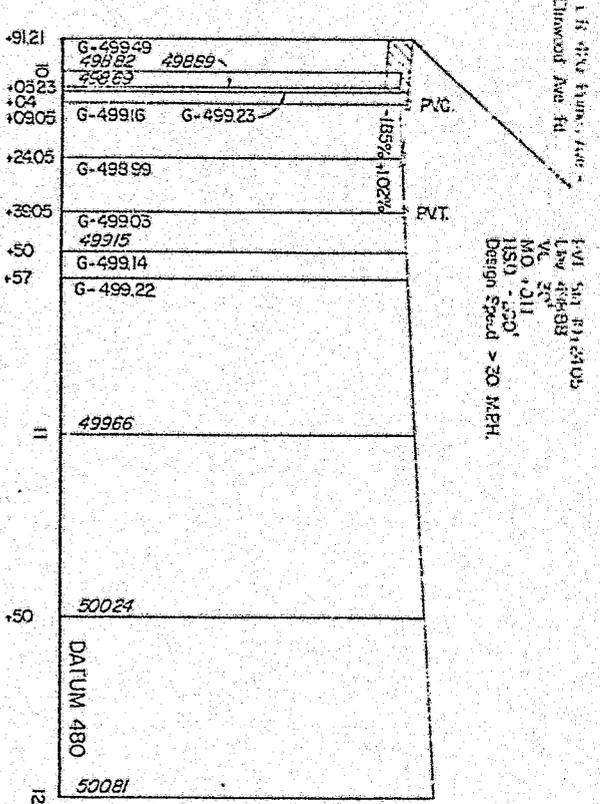
Proposed Structure
 SIA 85150-51
 Mated Type Adjacent B. Spread
 Composite P/S Concrete Box Beam Bridge
 3 Spans
 2 of 265'-3" C-C Bays
 1 of 170'-6" C-C Bays
 Underpass: 20.59' Min.
 Clear Height: 28' Curb to Curb
 5-G Sidewalk Both Sides
 Structure No. 5-15400

The Aerial Easement acquired between Stations 85+07.00 to 85+08.20 includes a Surface Easement (unshaded in vertical dimension) for the accommodation of pipes & other appurtenances plus a Temporary Area for Construction & for storage of materials during construction.



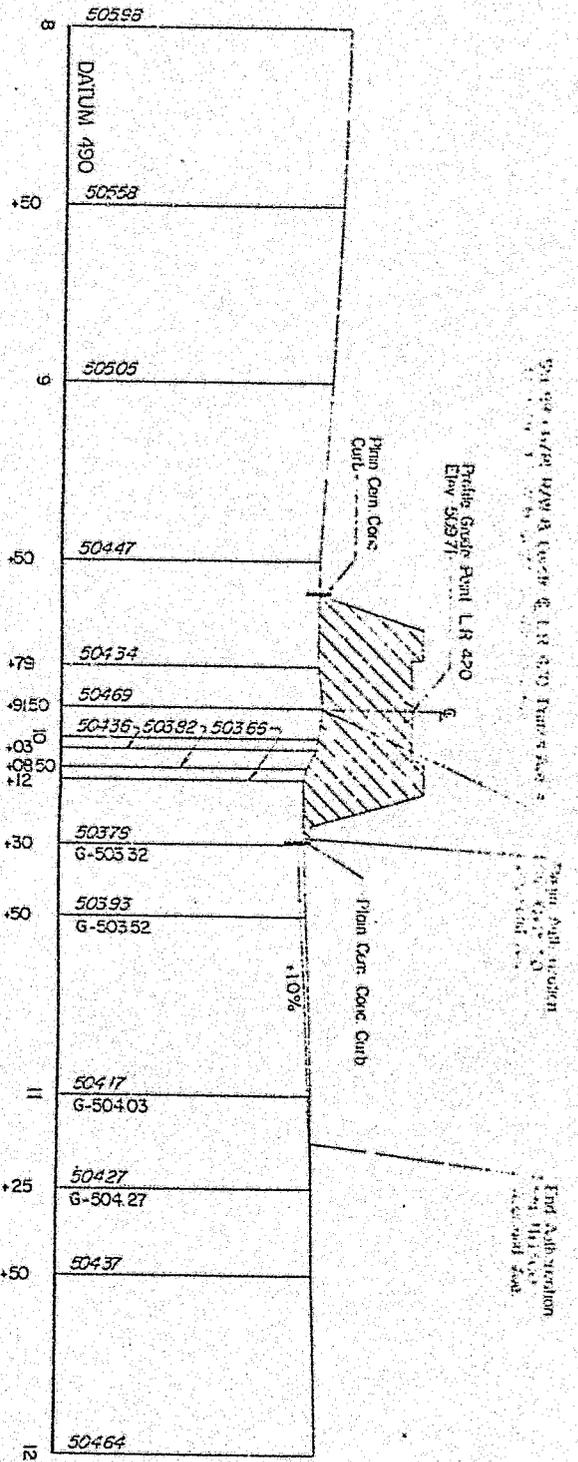
1"=1' HORIZ.
1"=20' VERT.
MO +0.00'
HSO = 200'
Design Speed > 30 MPH

1"=1' HORIZ.
1"=20' VERT.
MO +0.00'
HSO = 200'
Design Speed > 30 MPH



1"=1' HORIZ.
1"=20' VERT.
MO +0.11'
HSO = 200'
Design Speed > 30 MPH

Station	Elevation	Notes
50598	50598	DATUM 490
50558	50558	
50505	50505	
50447	50447	
50434	50434	
50469	50469	
50436, 50392, 50365	50436, 50392, 50365	
50379	50379	
50393	50393	
50417	50417	
50427	50427	
50437	50437	
50464	50464	



Profile Grade Point L.R. 420
Elev. 50371

Plan Cur. Conc. Curb

Plan Cur. Conc. Curb

Amtrak Low Wire
Sta. 63+68
Elev. 5390.1

PVI Sta. 85+75.00
Elev. 525.50
VC 250'
MO -3.84'
SSU = 100'
Design Speed = 27 MPH

The Aerial Easement acquired between Stations 85+07.00 to 85+035.00 includes a Surface Easement unlimited in vertical dimension for the accommodation of pipes, a other appurtenances plus a temporary area for construction & for storage of materials during construction.

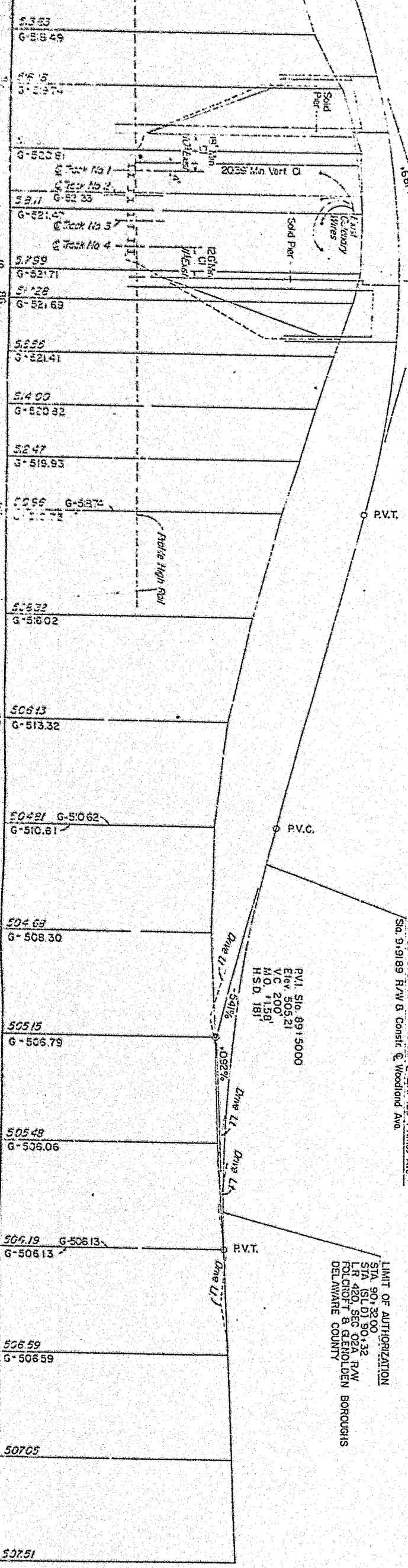
WOODLAND AVE
For Plan See Sheet 7.

L.R. 420
For Plan See Sheet 7.

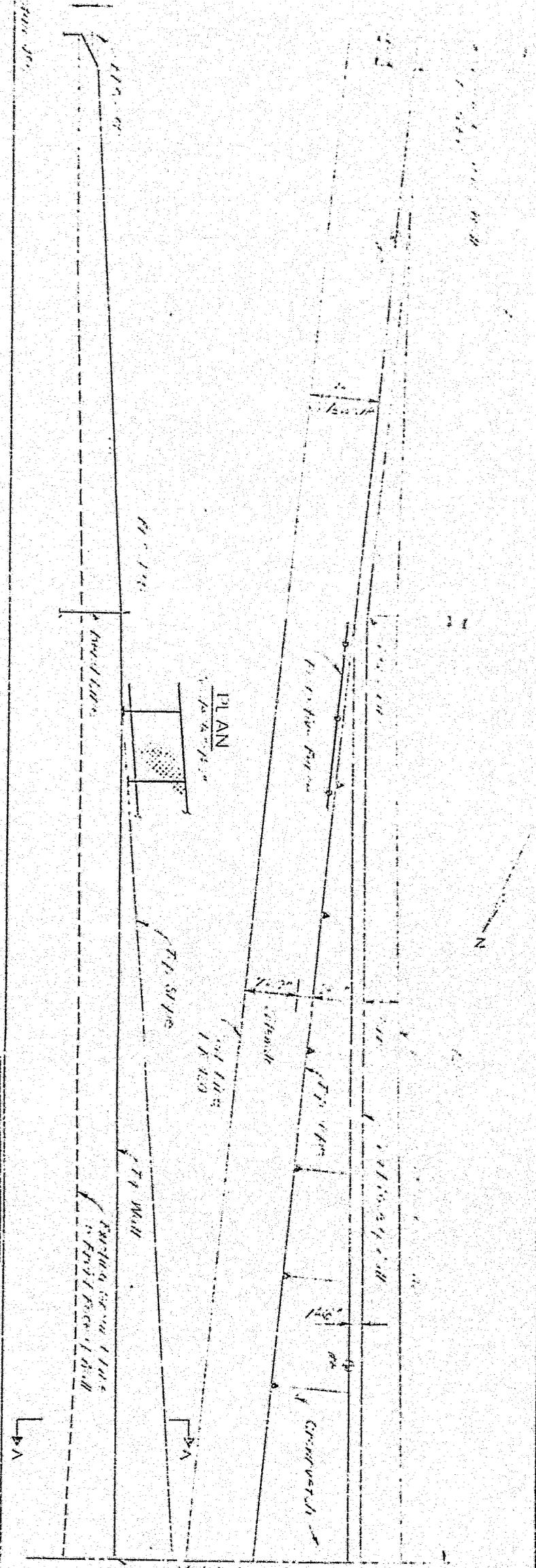
Sta. 88+67.81 R/W & Constr. @ L.R. 420 Primos Ave =
Sta. 9+91.89 R/W & Constr. @ Woodland Ave.

PVI Sta. 89+50.00
Elev. 505.21
VC 200'
MO +1.58'
HSD 181'

LIMIT OF AUTHORIZATION
Sta. 90+38.00
Sta. 91.01 90+32
L.R. 420, SEC. 02A R/W
FOLCROFT & GENOLDEN BOROUGHES
DELAWARE COUNTY



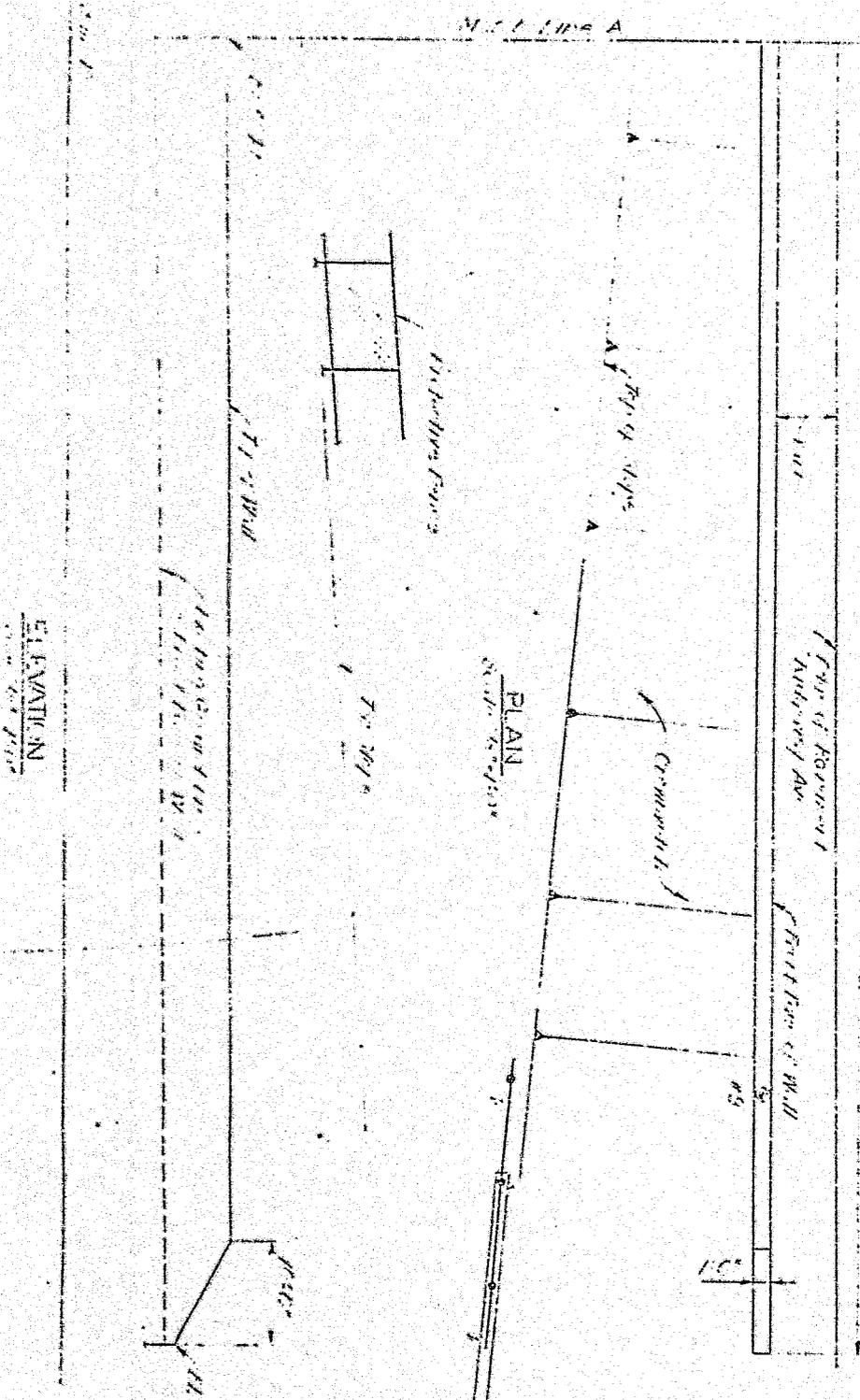
DATUM 485



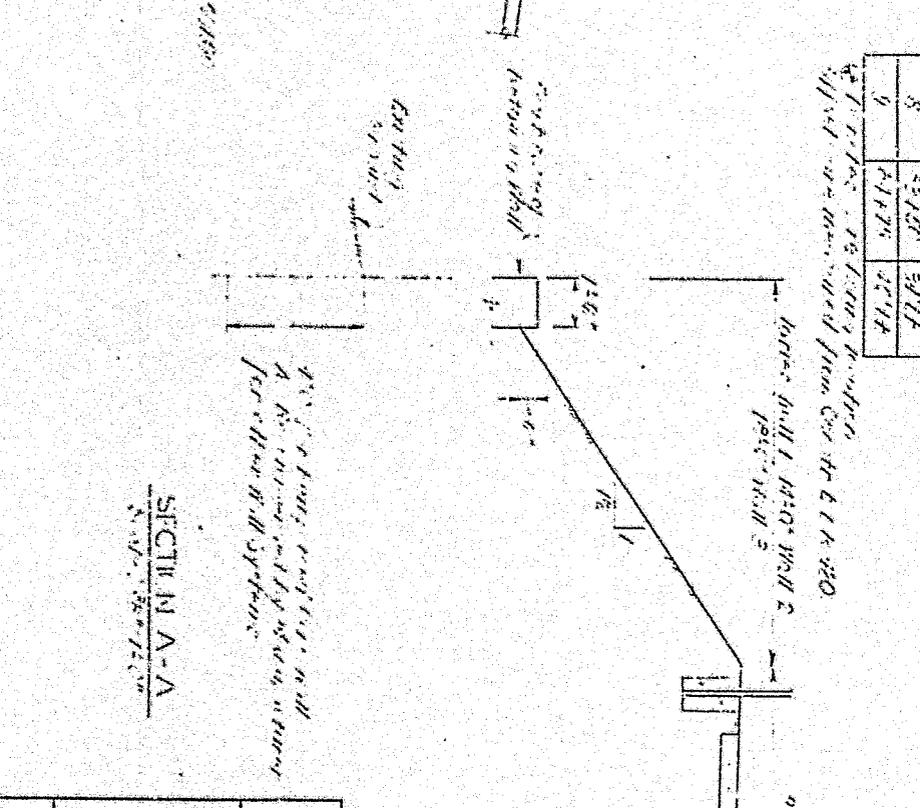
ELEVATION
1:20

NO.	DATE	DESCRIPTION
1	6/1/50	PRELIM
2	6/15/50	REVISED
3	6/15/50	REVISED
4	6/15/50	REVISED

Notes: 1. See General Notes on sheet 414-200.
2. All dimensions are in feet.



PLAN
1:20



SECTION A-A
1:20

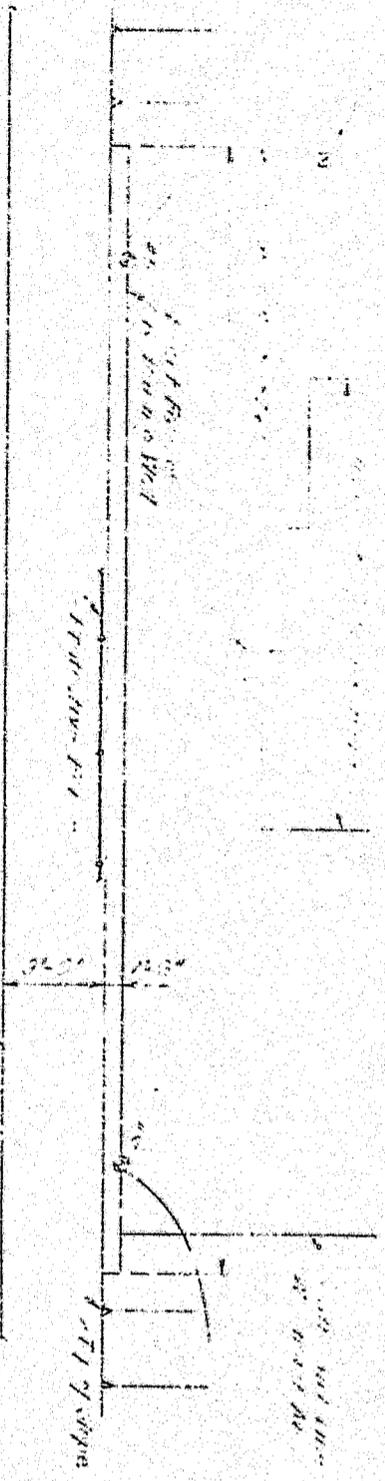
Notes: 1. See General Notes on sheet 414-200.
2. All dimensions are in feet.

Mark	Description	By	CHK'd	App'd	Date

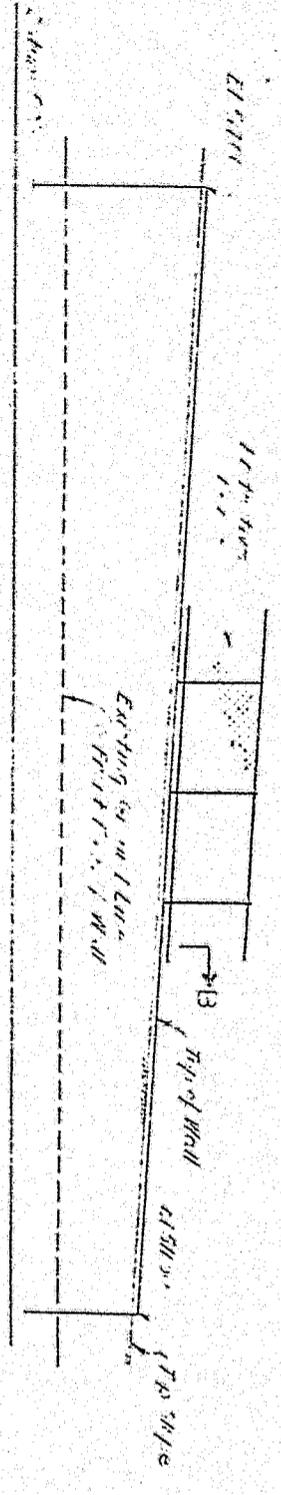
Commonwealth of Pennsylvania
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAY DESIGN

DELAWARE COUNTY
 L.R. 420 S.C. 024
 LR 420-024 (GIMOS A/F)
 STATIONS TO STA 5+00+00
 TRAINING WALL #1
 GENERAL PLAN

APPROVED _____ SHEET _____ OF _____



WALL A
PLAN
S.D. 182/100

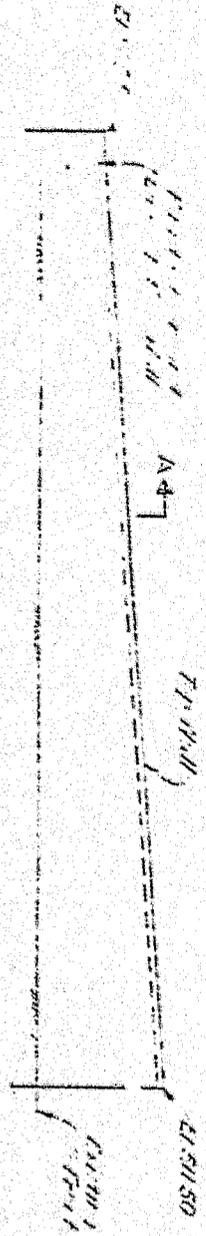


ELEVATION
S.D. 182/100

Existing masonry		
Course	Thick.	Height
1	3/4"	4.00'
2	4 1/2"	28.00'
3	5 1/2"	2.11'
4	5 1/4"	2.11'

① Existing masonry wall above
cylinder on top of the wall.

WALL 2
PLAN
S.D. 182/100



ELEVATION
S.D. 182/100

SECTION B-B
S.D. 182/100

REVISIONS				
Mark	Description	By	Chk'd.	App'd.

Commonwealth of Pennsylvania

DEPARTMENT OF TRANSPORTATION

BUREAU OF HIGHWAY DESIGN

DELAWARE COUNTY

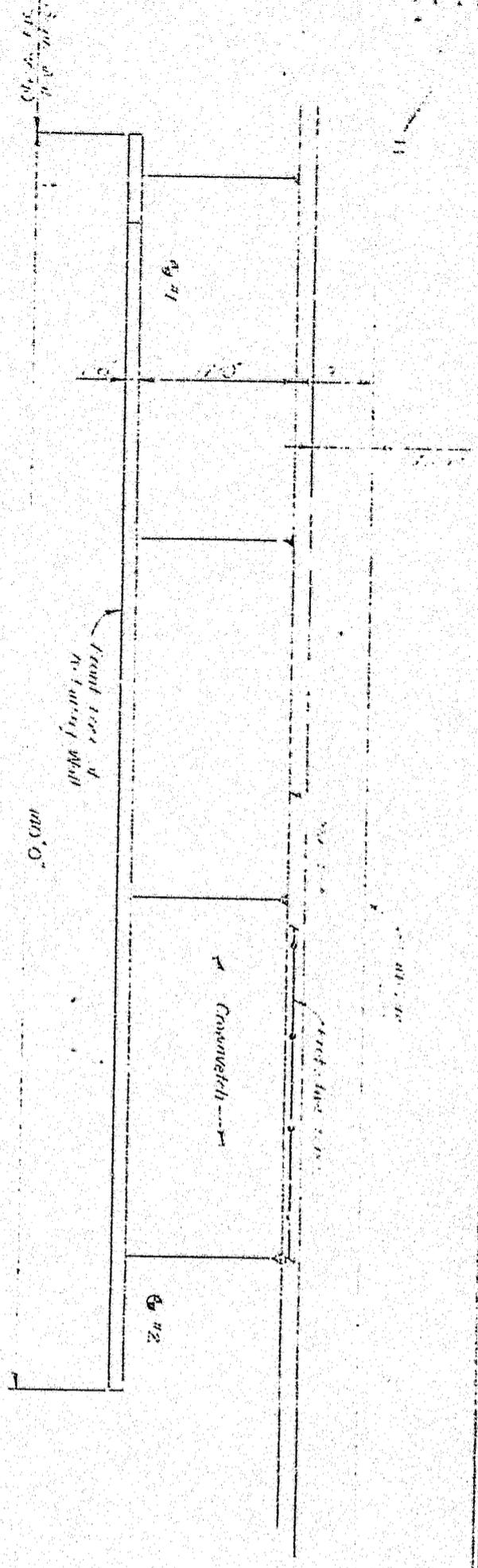
L.R. 420 SEC. 024

L.R. 420-024 THIMOS AVE

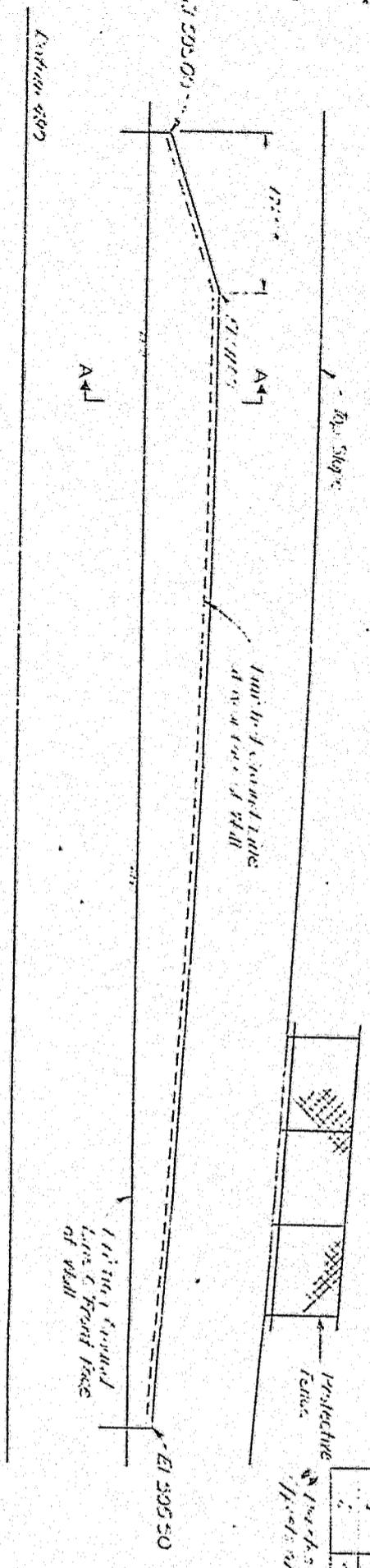
STA 545+00 TO STA 545+50

REVISIONS WALLS 2/24

APPROVED _____ SHEET 1 OF 1



PLAN
Scale: 1/8" = 1'-0"



ELEVATION
Scale: 1/8" = 1'-0"

NO.	DATE	DESCRIPTION
1	5/11/54	AS SHOWN
2	5/11/54	AS SHOWN

NOTE: Check on front face of wall for LR 420

NOTE: See Section A-A, see Sheet 2

REVISIONS				
Mark	Description	By	Chkd	Date

Commonwealth of Pennsylvania
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAY DESIGN
 DELAWARE COUNTY
 LR 420 7' x 0.24
 LR 420-0.24 (PENDING AVE)
 STA 86+10 TO 87+50 RIGHT
 R.C. RETAINING WALL 3
 CONTRACT 4 A11

RECOMMENDED _____ SHEET 4 OF 4

COMMONWEALTH OF PENNSYLVANIA
DEPARTMENT OF TRANSPORTATION

Harrisburg
October 3, 1984

ORIGINAL



IN REPLY REFER TO

Delaware County
L. R. 420, Section 02A (Primos Avenue)
Boroughs of Folcroft and Glenolden
PUC. C. 00020235

RECEIVED

OCT 5 1984

Mr. Jerry Rich, Secretary
Public Utility Commission
Harrisburg, PA 17120

SECRETARY'S OFFICE
Public Utility Commission

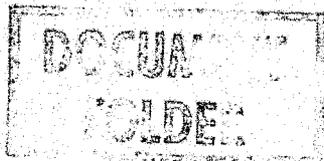
Dear Mr. Rich:

As directed by Paragraphs 1 and 2 of the Commission's Order of May 13, 1975, as amended, at Complaint Docket No. 00020235, attached for Commission's consideration are three (3) sets of plans providing for the reconstruction of the existing crossing and the highway approaches thereto.

A copy of this letter and one (1) set of the afore-mentioned plans are being forwarded to the following parties of record:

- R. D. Johnson, Assistant Chief Engineer/Project Manager, National Railroad Passenger Corporation, Northeast Corridor Operations, 1617 John F. Kennedy Boulevard, Philadelphia, PA 19103
- Penn Central Corporation, IVB Building, 1700 Market Street, Philadelphia, PA 19103
- Edward H. Huss, Staff Counsel, Southeastern Pennsylvania Transportation Authority, 2028 PSFS Building, Philadelphia, PA 19107
- David E. Auerbach, Solicitor, Borough of Folcroft, Legal Arts Building, Media, PA 19063
- Joseph A. Damico, Jr., Solicitor, Borough of Glenolden, William Penn Building, 107 Chelsey Drive, Media, PA 19063
- Francis P. Connors, Solicitor, County of Delaware, 107 West Third Street, Media, PA 19063
- J. T. Sullivan, P. E., Consolidated Rail Corporation, Design and Construction, 12th Floor - 15 N. 32nd Street, Philadelphia, PA 19104
- Richard Tinucci, Solicitor, Borough of Sharon Hill, P. O. Box 602, Media, PA 19063
- Philadelphia Electric Company, 2301 Market Street, Philadelphia, PA 19103
- The Bell Telephone Company of Pennsylvania, John B. King, Vice-President & General Counsel, Law Department, 16th Floor, One Parkway, Philadelphia, PA 19102
- Philadelphia Suburban Water Company, 762 Lancaster Pike, Bryn Mawr, PA 19010

-more-



Mr. Rich, Secretary
PUC. C. 00020235

-2-

October 3, 1984

The Department requests that a field conference of all the interested parties be scheduled by the Commission to discuss the attached plans.

Very truly yours,

For: Alfred F. Lyng, P. E.
Chief Engineer
Highway Administration

Attachments



COMMONWEALTH OF PENNSYLVANIA
PENNSYLVANIA PUBLIC UTILITY COMMISSION
P.O. BOX 3265, HARRISBURG, Pa. 17120

December 20, 1984

IN REPLY PLEASE
REFER TO OUR FILE

C-00020235

Monitor: Chairman Taliaferro

In re:

Borough of Folcroft

v.

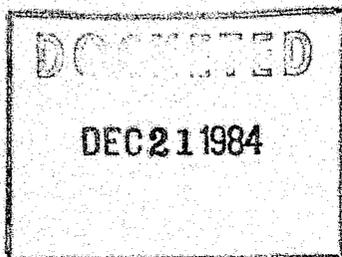
Penn Central Transportation Company, Department of Transportation of the Commonwealth of Pennsylvania, County of Delaware, Southeastern Pennsylvania Transportation Authority, Consolidated Rail Corporation and National Railroad Passenger Corporation

This is to advise that a field investigation and conference will be held at the site of the crossing where L.R. 420 (Primos Avenue) crosses above grade of tracks of Amtrak on January 16, 1985 at 11:00 a.m. to discuss the plans recently submitted by the Department for reconstruction of the subject bridge.

Please arrange to have a representative present.

Very truly yours,

Donald R. Fleisher, P.E.
Bureau of Rail Transportation



Similar Letter List: C-00020235

Mr. W. J. Clements, P.E., Acting Chief
Right-of-Way and Utility Division
Pennsylvania Department of Transportation
509 Transportation and Safety Building
Harrisburg, PA 17120
Dear Mr. Clements:

Mr. R. D. Johnson, Asst. Chief Engineering/Project Manager
National Railroad Passenger Corporation
1617 John F. Kennedy Boulevard
Philadelphia, PA 19103
Dear Mr. Johnson:

Penn Central Corporation
IVB Building
1700 Market Street
Philadelphia, PA 19103
Gentlemen:

Edward H. Huss, Staff Counsel
Southeastern Pennsylvania Transportation Authority
130 South Ninth Street
Philadelphia, PA 19107
Dear Mr. Huss:

Mr. J. T. Sullivan, P.E., Chief Engineer
Design and Construction
Consolidated Rail Corporation
15 North 32nd Street
Philadelphia, PA 19104
Dear Mr. Sullivan:

David E. Auerbach, Solicitor
Borough of Folcroft
Legal Arts Building
Media, PA 19063
Dear Mr. Auerbach:

Joseph A. Damico, Jr., Solicitor
Borough of Glenolden
107 Chelsey Drive
Media, PA 19063
Dear Mr. Damico:

Francis P. Connors, Solicitor
County of Delaware
107 West Third Street
Media, PA 19063
Dear Mr. Connors:

Similar Letter List, continued: C-00020235

Richard Tinucci, Solicitor
Borough of Sharon Hill
P.O. Box 620
Media, PA 19063
Dear Mr. Tinucci:

John B. King, Vice President and General Counsel
The Bell Telephone Company of Pennsylvania
16th Floor, One Parkway
Philadelphia, PA 19102
Dear Mr. King:

Philadelphia Suburban Water Company
762 Lancaster Pike
Bryn Mawr, PA 19010
Gentlemen:



C- 20235
The Penn Central Corporation

1700 Market Street, IVB Building-31st Floor
Philadelphia, Pa. 19103
Telephone (215) 972-3188

Joseph J. Supon
Director, Leased Property and Special Sales

January 2, 1985

Mr. Donald R. Fleisher, P.E.
Bureau of Rail Transportation
Commonwealth of Pennsylvania
Pennsylvania Public Utility Comm.
P. O. Box 3265
Harrisburg, Pa. 17120

RECEIVED

JAN 7 1985

SECRETARY'S OFFICE
Public Utility Commission

RE: Borough of Folcroft

Dear Mr. Fleisher:

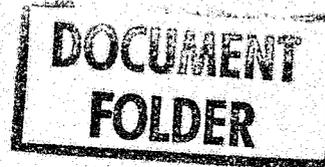
Receipt of your December 20, 1984 notice concerning proposed improvement to the Primos Avenue railroad crossing is acknowledged.

Be advised that The Penn Central Corporation has no ownership to be affected by this proposed project as shown on the enclosed valuation plan.

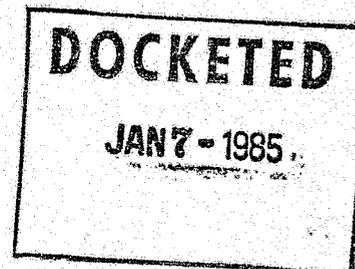
Very truly yours,

Joseph J. Supon
Director, Leased Property &
Special Sales

DPM/fjm



cc: Philip F. Hathaway



0-CH5
TOWNSHIP OF HILL
COUNTY OF HILL
STATE REGION

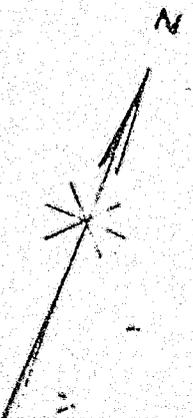
TOWNSHIP
HILL BOROUG

COOKE AVE.

520'

SOLD (75)
475' (2)
See map of Hill Co. for details
(formerly Hill 500)

LDGWAY



THIS TRACING IS 50% ORIGINAL SIZE
DRAFTSMAN TO DOUBLE SCALE



COMMONWEALTH OF PENNSYLVANIA
PENNSYLVANIA PUBLIC UTILITY COMMISSION
P. O. BOX 3265, HARRISBURG, Pa. 17120
January 31, 1985

IN REPLY PLEASE
REFER TO OUR FILE
C-00020235

(See attached list)

DOCUMENT
FOLDER

DOCKETED
FEB 02 1985
DOCKET

Borough of Folcroft
vs.

Penn Central Transportation Company, Department of Transportation of the Commonwealth of Pennsylvania, County of Delaware, South-eastern Pennsylvania Transportation Authority, Consolidated Rail Corporation and National Railroad Passenger Corporation.

NOTICE

This is to advise that a further hearing in the above-captioned proceeding will be held Tuesday, February 26, 1985, at 10:00 a.m., in the Council Room, First Floor, Folcroft Borough Building, 1555 Elmwood Avenue, Folcroft.

This hearing is being held to take testimony on the recently submitted right-of-way plan and preliminary bridge plan for the Primos Avenue Bridge, including initial costs.

P.S. to Judge Smolen:
This hearing is assigned to you under the EXAMINER SYSTEM. You will not be expected to write a decision.

We are enclosing herewith, a list of questions, prepared by our Bureau of Rail Transportation, indicating information to be developed at the hearing, along with other relevant testimony.

If any party intends to offer prepared written testimony, such testimony must be served upon each participant of record no less than twenty (20) days prior to this hearing.

You are advised that if you fail to appear at the hearing, the hearing will proceed without you. In that event, it is possible that you or your client will have issues determined against you and that you might be assigned positive duties or be required to pay money and will have waived any objection thereto.

The presiding officer at this hearing will be Administrative Law Judge Herbert Smolen. Judge Smolen can be contacted at 1302 Philadelphia State Office Building, Broad and Spring Garden Streets, Philadelphia, Pennsylvania 19130; telephone (215) 351-2105.

If you intend to file exhibits, please be advised that two copies of all hearing exhibits to be presented into evidence must be submitted to the Reporter and an additional copy should be furnished to the presiding officer and each party of record.

Enclosure

C-20235 - Parties of Record.

Spencer A. Manthorpe, Chief Counsel
Pennsylvania Department of Transportation
521 Transportation & Safety Building
Harrisburg, Pennsylvania 17120

Harold Cohen, Esquire
National Railroad Passenger Corporation
Suburban Station Building, Room 524
1617 John F. Kennedy Boulevard
Philadelphia, Pennsylvania 19103

Daniel F. Donovan, General Attorney
Consolidated Rail Corporation
1138 Six Penn Center Plaza
Philadelphia, Pennsylvania 19103

Edward H. Huss, Esquire
Southeastern Pennsylvania Transportation
Authority
130 South Ninth Street, Fifth Floor
Philadelphia, Pennsylvania 19107

W. Preston Granbery, Esquire
The Bell Telephone Company of Pennsylvania
One Parkway
Philadelphia, Pennsylvania 19102

T. H. Maher Cornell, Esquire
Philadelphia Electric Company
P. O. Box 8699
Philadelphia, Pennsylvania 19101

John W. Boyer, Jr., President
Philadelphia Suburban Water Company
762 Lancaster Avenue
Bryn Mawr, Pennsylvania 19010

Francis P. Connors, Solicitor
Delaware County
Courthouse
Media, Pennsylvania 19063

David E. Auerbach, Solicitor
Folcroft Borough
Legal Arts Building
Media, Pennsylvania 19063

Joseph A. Damico, Jr., Solicitor
Glenolden Borough
107 Chelsey Drive
Media, Pennsylvania 19063

Times Mirror Cable T V
501 Oak Avenue
Glenolden, Pennsylvania 19036

ccs:
Judge Smolen
Law Bureau
Bureau of Rail Transportation
Mr. Bramson
Ms. Dickson
Ms. Crouse
File

1. Pennsylvania Department of Transportation submit into evidence copies of its plans for the proposed crossing improvement.
2. Department state whether it will be necessary for the Commission to appropriate any property for the purpose of the proposed crossing improvement. If so, submit copies of plans showing any such property and accurate descriptions, by metes and bounds, of each parcel of property to be appropriated by the Commission; said descriptions to be designated as property necessary for highway right-of-way or for purposes of the improvement other than highway right-of-way and to contain the name and post-office address of each record owner and reference to the record of title thereof.
3. Department submit a detailed estimate of the cost of constructing the proposed improvement, including an estimate of the cost which would be incurred by the vacation and relocation, removal or demolition of any non-utility structures, including occupied dwellings, located on property to be appropriated by the Commission; said estimate to be prepared in such a manner as to show separately the quantity, unit prices and costs of each portion of the work.
4. Department submit an estimate of damages for any property to be appropriated for any purposes of the improvement, exclusive of the estimated costs which would be incurred by the vacation and relocation, removal or demolition of any non-utility structures, including occupied dwellings, located on property to be appropriated by the Commission, and an estimate of any damages that may accrue from the construction of any portion of the improvement, exclusive of damages for property appropriated for the purposes of the improvement.
5. Department state which portions of the proposed improvement project it proposes to construct and which portions it suggests should be constructed by other parties.
6. Department state which portions of the proposed improvement project it agrees to assume the cost thereof.
7. Department state which portions of the proposed improvement project it agrees to assume the responsibility for maintenance and the cost thereof.
8. Department state what portion of the property damages resulting from the construction of the crossing improvement it will agree to assume and whether it will reimburse any involved public utility companies for any costs incurred in the alteration or relocation of their respective facilities.

9. Department state whether it will be necessary to establish detours for highway traffic during the time the crossing improvement is being constructed and whether it will establish and maintain such detours, if required.
10. Department state what time it anticipates will be required to complete the construction of the proposed improvement.
11. Department state whether any portion of the crossing improvement project will be financed in whole or in part with funds appropriated for use on such projects by the Federal government or from the Bridge Bill.
12. Philadelphia Electric Company (hereinafter PECO) state whether it objects to the proposed improvement project and whether it has any constructive criticism to offer.
13. PECO submit detailed plans and cost estimates for all required work to its facilities to permit the construction of the subject improvement and submit testimony explaining the plans and estimate, indicating which portions of the work are within public right-of-way and which portions are outside of public right-of-way and state whether work will be performed prior to, concurrent with, or after bridge and approach roadway construction. If work will be performed prior to bridge and roadway construction, what time will be required to complete the work?
14. PECO state whether it will agree to perform the required alteration or relocation work for its facilities and state what portion of the cost involved it will agree to bear at its sole cost and expense.
15. PECO state whether it will agree to assume the responsibility for maintenance of its facilities after alteration or relocation work has been completed.
16. Philadelphia Suburban Water Company (hereinafter PSWC) state whether it objects to the proposed improvement project and whether it has any constructive criticism to offer.
17. PSWC submit detailed plans and cost estimate for all required work to its facilities to permit the construction of a subject improvement and submit testimony explaining the plans and estimate, indicating which portions of the work are within public right-of-way and which portions are outside of public right-of-way and state whether work will be performed prior to, concurrent with, or after bridge and approach roadway construction. If work will be performed prior to bridge and roadway construction, what time will be required to complete the work?

18. PSWC state whether it will agree to perform the required alteration or relocation work for its facilities and state what portion of the cost involved it will agree to bear at its sole cost and expense.
19. PSWC state whether it will agree to assume the responsibility for maintenance of its facilities after alteration or relocation work has been completed.
20. The Bell Telephone Company of Pennsylvania (hereinafter Bell), state whether it objects to the proposed improvement project and whether it has any constructive criticism to offer.
21. Bell submit detailed plans and cost estimates for all required work to its facilities to permit the construction of the subject improvement and submit testimony explaining the plans and estimate, indicating which portions of the work will be within public right-of-way and which portions are outside of the public right-of-way and state whether work will be performed prior to, concurrent with, or after bridge and approach roadway construction. If work will be performed prior to bridge and roadway construction, what time will be required to complete the work?
22. Bell state whether it will agree to perform required alteration or relocation work for its facilities and state what portion of the costs involved it will agree to bear at its sole cost and expense.
23. Bell state whether it will agree to assume the responsibility for maintenance of its facilities after alteration or relocation work has been completed.
24. National Railroad Passenger Corporation (hereinafter Amtrak), state whether it objects to the proposed improvement project and whether it has any constructive criticism to offer.
25. Amtrak submit a cost estimate for all required work to its facilities to permit the construction of the subject improvement, submit testimony explaining the required work and cost estimate and state what time will be required to complete the work necessary to permit the construction of the proposed bridge to proceed.
26. Amtrak state what portion of the work required to alter its facilities it will agree to perform and state what portion of the costs involved it will agree to bear at its sole cost and expense.
27. Amtrak state what portion of the costs of the improvement, other than its own facilities, it will agree to bear at its sole cost and expense.
28. Amtrak state whether it will be necessary to maintain railroad flagmen, watchmen and inspectors during the construction of the proposed bridge.

29. Amtrak state whether it will agree to assume the responsibility for maintenance of its facilities at the crossing after the improvement has been completed.
30. Amtrak state what portion of the improvement, other than its own facilities, it will agree to maintain after the project has been completed.
31. Consolidated Rail Corporation state whether it objects to the proposed improvement project and whether it has any constructive criticism to offer.
32. Southeastern Pennsylvania Transportation Authority state whether it objects to the proposed improvement project and whether it has any constructive criticism to offer.
33. Boroughs of Folcroft and Glenolden each state whether it objects to the proposed improvement project and whether it has any constructive criticism to offer.
34. Boroughs each state whether any facilities belonging to it will be affected by the construction of the proposed improvement and, if so, submit testimony explaining the work that is required and give a cost estimate for such work, indicating which portions of the work are within public right-of-way and which portions are outside of public right-of-way.
35. Boroughs each state whether it will agree to perform the required work to its facilities and what portion of the cost involved it will agree to bear at its sole cost and expense. If work will be performed prior to bridge and roadway construction, what time will be required to complete the work?
36. Boroughs each state what portion of the cost of the improvement, other than its own facilities, it will agree to bear at its sole cost and expense.
37. Boroughs each state whether it will agree to assume the responsibility for maintenance of its facilities at the crossing after the improvement has been completed.
38. Boroughs each state what portion of the improvement, other than its own facilities, it will agree to maintain after the project has been completed.
39. Query whether any of the parties desires to submit any additional relevant testimony.



COMMONWEALTH OF PENNSYLVANIA
PENNSYLVANIA PUBLIC UTILITY COMMISSION
P. O. BOX 3265, HARRISBURG, Pa. 17120

February 20, 1985

IN REPLY PLEASE
REFER TO OUR FILE

C00020235

see 1/31/85 notice

Borough of Folcroft

vs.

Penn Central Transportation Company, Department of Transportation of the Commonwealth of Pennsylvania, County of Delaware, Southeastern Pennsylvania Transportation Authority, Consolidated Rail Corporation and National Railroad Passenger Corporation.

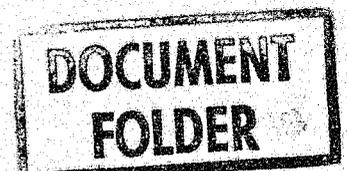
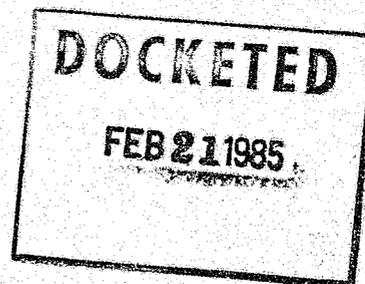
NOTICE

At the request of counsel for The Bell Telephone Company of Pennsylvania, and with the agreement of all other parties, the hearing in the above-captioned proceeding scheduled for February 26, 1985, at Folcroft, is postponed without date.

This hearing will be rescheduled in the near future and you will be promptly informed of the date and place.

Please mark your records accordingly.

ccs:
Judge Smolen
Law Bureau
Bureau of Rail Transportation
Mr. Bramson
Ms. Dickson
Ms. Crouse
File



*V.V.
D. Sacks
100 Allen
Philadelphia*

Edward S. Sacks
Attorney

Bell of Pennsylvania

Law Department
One Parkway, 16th Floor
Philadelphia, Pennsylvania 19102
Phone (215) 466-8714

RECEIVED
FEB 26 1985
Office of the ALJ
Public Utility Commission

February 20, 1985

Honorable William H. Smith,
Chief Administrative Law Judge
P.O. Box 3265
Harrisburg, Pennsylvania 17120

Re: Borough of Folcroft vs. Penn Central Transportation Company, Department of Transportation of the Commonwealth of Pennsylvania, County of Delaware, Southeastern Pennsylvania Transportation Authority, Consolidated Rail Corporation and National Railroad Passenger Corporation. C-00020235.

Dear Judge Smith:

The Bell Telephone Company of Pennsylvania, having approval from all parties of record, hereby requests that the hearing scheduled for Tuesday, February 26, 1985 in the above-referenced matter be postponed. Bell does not have an attorney available to attend this hearing due to previously planned out-of-town business trips and other PUC hearings. We apologize for any inconvenience this may have caused.

Sincerely,

Edward S. Sacks

*File
20*

DOCUMENTED
FEB 26 1985

EQUIPMENT
FOLDER

COMMONWEALTH OF PENNSYLVANIA
PENNSYLVANIA PUBLIC UTILITY COMMISSION
P.O. BOX 3265, HARRISBURG, PA 17120
February 26, 1985

In re: C-00020235

(See letter dated 1/31/85)

Borough of Folcroft
vs.

Penn Central Transportation Company, Department of Transportation of the Commonwealth of Pennsylvania, County of Delaware, Southeastern Pennsylvania Transportation Authority, Consolidated Rail Corporation and National Railroad Passenger Corporation.

NOTICE

This is to advise that further hearing in the above-captioned proceeding, concerning the Primos Avenue bridge, is rescheduled to be held Wednesday, March 20, 1985, at 10:00 a.m., in the Council Room, First Floor, Folcroft Borough Building, 1555 Elmwood Avenue, Folcroft.

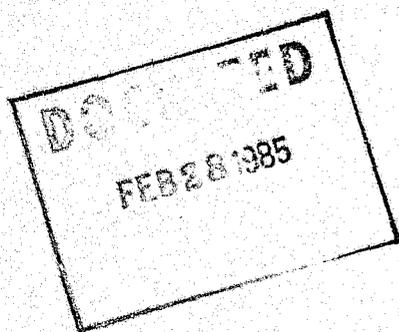
This hearing will be conducted in accordance with the instructions contained in our January 31, 1985 notice. A copy of the questions and procedure enclosed with that notice are attached hereto.

The presiding officer at this hearing will be Administrative Law Judge Herbert Smolen, 1302 Philadelphia State Office Building, Broad and Spring Garden Streets, Philadelphia, Pennsylvania 19130; telephone (215) 351-2105.

Attachment

cc: Judge Smolen
Law Bureau
Bureau of Rail Trans.
Mr. Bramson
Ms. Dickson
Ms. Crouse
File Room

P.S. to Judge Smolen:
This hearing is assigned
to you under the EXAMINER
SYSTEM. You will not be
expected to write a decision.



COMMONWEALTH OF PENNSYLVANIA
PENNSYLVANIA PUBLIC UTILITY COMMISSION
P.O. BOX 3265, HARRISBURG, PA 17120
March 11, 1985

In re: C-00020235

(See letter dated 1/31/85)

Borough of Folcroft
vs.

Penn Central Transportation Company, Department of Transportation of the Commonwealth of Pennsylvania, County of Delaware, Southeastern Pennsylvania Transportation Authority, Consolidated Rail Corporation and National Railroad Passenger Corporation.

NOTICE

At the request of counsel for National Railroad Passenger Corporation, and with the consent of all parties, the further hearing in this proceeding concerning the Primos Avenue Bridge is postponed from March 20, 1985, to Thursday, April 4, 1985, at 10:00 a.m., in the Council Room, First Floor, Folcroft Borough Building, 1555 Elmwood Avenue, Folcroft.

This hearing will be conducted in accordance with the instructions contained in our January 31, 1985 notice.

The presiding officer at this hearing will be Administrative Law Judge Herbert Smolen, 1302 Philadelphia State Office Building, Broad and Spring Garden Streets, Philadelphia, Pennsylvania 19130; telephone (215) 351-2105.

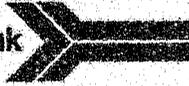
cc: Judge Smolen
Law Bureau
Bureau of Rail Trans.
Mr. Bramson
Ms. Dickson
Ms. Crouse
File Room

P.S. to Judge Smolen:
This hearing is assigned to you under the EXAMINER SYSTEM. You will not be expected to write a decision.

DOCUMENT
FOLDE

DOCKETED
MAR 12 1985

Amtrak



MAR 08 1985

William H. Smith
Chief Administrative Law Judge
Pennsylvania Public Utility Commission
P.O. Box 3265
Harrisburg, PA 17120

RECEIVED
MAR 11 1985
Office of the A.L.J.
Public Utility Commission

Re: Borough of Folcroft vs. Penn Central
Transportation Company, et al.
Docket No. C-00020235

Dear Judge Smith:

I recently requested a continuance of the above matter for which a hearing is currently scheduled on March 20th since I will be on vacation that week. Your office informed me that April 4th was a satisfactory date and that I should attempt to get the agreement of all concerned for that date. I am writing to inform you that all counsel as well as parties at this time not represented by counsel have agreed to continue the hearing in this matter to April 4th. Judge Smolen has also indicated that the rescheduled date is acceptable to him.

By copy of this letter, I am communicating the above to Judge Smolen and all counsel or parties. A list of those parties and counsel is attached.

Very truly yours,

Harold K. Cohen
Counsel - East

cc: ALJ Herbert Smolen
All Parties

**DOCUMENT
FOLDER**

DOCKETED
MAR 12 1985

File Document

SERVICE LIST

Herbert Zahn, Esquire
Pennsylvania Department of Transportation
522 Transportation and Safety Building
Harrisburg, PA 17120

Daniel Donovan, Esquire
Consolidated Rail Corporation
1138 Six Penn Center
Philadelphia, PA 19103

Edward Huss, Esquire
Southeastern Transportation Authority
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Edward Sacks, Esquire
Bell Telephone Company of Pennsylvania
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T. H. Maher Cornell, Esquire
Philadelphia Electric Company
2301 Market Street
Philadelphia, PA 19101

Mark Kropilak, Esquire
Morgan, Lewis & Bockius
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David E. Auerbach, Esquire
Borough of Folcroft
P.O. Box 319
Media, PA 19063

Dennis Dunn, Esquire
Borough of Glenolden
109 Chesley Drive
Media, PA 19063

Times-Mirror Cable TV
501 Oak Avenue
Glenolden, PA 10163

APPEARANCE SHEET

DOCKET NO. G-00020235

CASE NAME Borough of Folcroft vs.

Penn Central Transportation Co.,

et al.

HEARING LOCATION Folcroft, PA.

HEARING DATE April 4, 1985

ALJ Smolen

HEARING REPORT

CHECK THOSE BLOCKS WHICH APPLY:

FOLCROFT HAS UNTIL
Hearing concluded 4/30/85 TO REQUEST ADD'L HEARING

Record closed as of 4/30/85

MEMO Briefs to be filed BY 4/30/85 ON LIMITED ISSUE

BOROUGH OF FOLCROFT HAS
Further hearing UNTIL 4/30/85 TO REQUEST ADD'L HEARING ON BOROUGH'S

Estimated add'l days ABILITY TO PAY FOR

MAINTENANCE OF CERTAIN CURB AND SIDEWALK

BENCH DECISION

THIS IS A HEARING EXAMINER CASE

DOCKETED

MAY 2 1985

REMARKS: MEMOS OF LAW TO BE LIMITED TO (1) AUTHORITY OF COMMISSION TO REQUIRE FOLCROFT TO MAINTAIN SIDEWALKS IN VIEW OF BOROUGH ORDINANCE PLACING RESPON. ON PROPERTY OWNERS (2) POSITION OF PARTIES AS TO WHO SHOULD BE RESP. FOR SIDEWALK MAINTENANCE

RECEIVED

APR 09 1985

Office of the Administrative Law Judge

Public Utility Commission

Names and addresses of parties or counsel of record
Please Print Clearly
Late Information May Result in Delay of Process

NAME	ADDRESS	APPEARING FOR
Harold K. Cohen	529 Sub. Sta. Bldg. City: Phila State: PA Zip: 19103	Amtrak
HERBERT G. ZAHN	522 T & S BLDG City: HSBG State: PA Zip: 17120	PENN ROT
William W. Bone	One Parkway City: Phila State: Pa Zip: 19102	Bell of Pa
MARK KROPILAK	MORGAN, LEWIS & BOCKIOS 2000 One Logan Square City: Phila State: Pa Zip: 19103	Philade (Phia) Suburban Water Co.

DOCUMENT FOLDER

Check this box if additional parties of counsel of record appear on back.

Linda Flynn
Reporter

AB

DAVID AUERBACH

Address 344 W. FRONT ST.		
City MEDIA	State PA	Zip 19063

Bow of
Falconcraft

EDWARD H. HUSS

Address 130 SOUTH 9TH ST.		
City PHILA	State PA	Zip 19077

SEPTA

Denise M. Dunn

Address 109 Chesley Drive		
City Media	State PA	Zip 19063

Borough of
Glenside

T. H. MAHER CORNELL

Address 232 W. 2501 MARKET ST		
City PHILA	State PA	Zip 19101

THE ELEC CO

JOHN J. GANNAGHER

Address 631 N. Office Bldg		
City HARRISBURG	State PA	Zip 17120

P.O. C
TRIAL STAFF

Address		
City	State	Zip

Address		
City	State	Zip

Address		
City	State	Zip

Address		
City	State	Zip

Address		
City	State	Zip

PETRIKIN, WELLMAN, DAMICO, CARNEY & BROWN

A PROFESSIONAL CORPORATION
ATTORNEYS AND COUNSELORS AT LAW

THE WILLIAM PENN BUILDING
109 CHESLEY DRIVE
MEDIA, PA. 19063

AREA CODE 215
565-2670

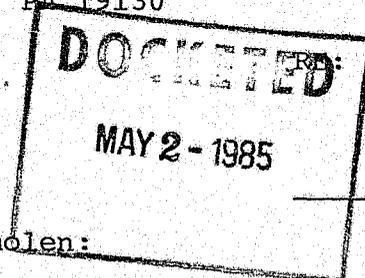
MALCOLM B. PETRIKIN
JOHN W. WELLMAN
JOSEPH A. DAMICO, JR.
EDWARD J. CARNEY, JR.
STEVEN G. BROWN
DONALD T. PETROSA
STEVEN A. COHEN
KRISTINE F. HUGHEY
DENIS M. DUNN
MARY K. POLACHECK

REC'D

April 17, 1985

APR 18 1985
PHILADELPHIA OFFICE
ALJ
PA. P.U.G.

The Honorable Herbert Smolen
Administrative Law Judge
Pennsylvania Public Utility Commission
Room 1302
Philadelphia State Office Building
Broad and Spring Garden Streets
Philadelphia, PA 19130



Borough of Folcroft v. Penn
Central Transportation Company,
et al. (Primos Avenue Bridge)
Docket No.: C-00020235
Our File: 32233 M-44

Dear Judge Smolen:

Our office represents the Borough of Glenolden in the above captioned matter. At the hearing held in this matter on April 4, 1985, the Borough of Glenolden indicated that it did not wish to assume the maintenance responsibility for the sidewalks on the approaches to the Primos Avenue Bridge on Glenolden's half of the bridge. The reasons for this objection are that the Borough is not in the business of maintaining sidewalks and that on at least one side of the bridge there is no adjoining property owner to assume this responsibility. Further, for the side of the bridge that does have an adjoining property owner, the proposal contemplates 200 feet of sidewalk which will be well above the grade of that adjoining property owner's property and accordingly will not benefit his property.

At the time of the hearing, H. Gilroy Damon, the Borough Engineer, stated that when the bridge was initially proposed the Borough objected to the presence of a sidewalk on the Glenolden side. Mr. Damon further stated that he was not authorized at that particular hearing to say whether the Borough continued to object to the presence of the sidewalks. Since that time, our office has consulted with the Borough Officials and the Borough has decided that it wants the sidewalks to remain on the approaches.

I contacted Raj Kohli, the PennDot Engineer who testified in this case, on Tuesday, April 16th and told him of the Borough's decision. He informed me that I should contact the Public Utility

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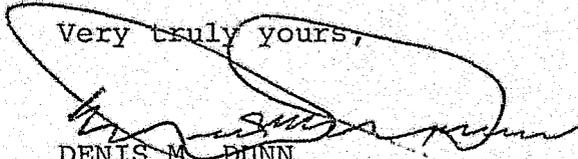
The Honorable Herbert Smolen
April 17, 1985
Page -2-

Commission and that he would alert his engineers not to remove the sidewalks from the plan.

Even though the Borough has requested that the sidewalks remain on the plan, the Borough still requests that it not be given the responsibility for maintaining those sidewalks for the reasons stated above.

I am serving a copy of this letter on all parties of record.

Very truly yours,



DENNIS M. DUNN

Counsel for Borough of Glenolden

DMD/dmo

cc: Herbert Zahn, Esquire
Daniel Donovan, Esquire
Edward Huss, Esquire
Edward Sacks, Esquire
Rudolph A. Chillemi, Esquire
Harold K. Cohen, Esquire
David E. Auerbach, Esquire
Times-Mirror Cable TV
Mark J. Kropilak, Esquire

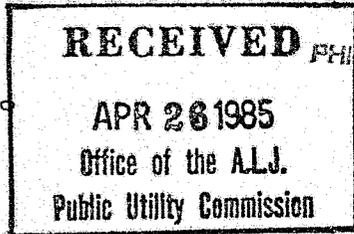
ECKELL, SPARKS, LEVY, AUERBACH & MONTE
ATTORNEYS AT LAW
LEGAL ARTS BUILDING
344 W. FRONT STREET
P. O. BOX 319
MEDIA, PENNSYLVANIA 19063
DELAWARE COUNTY

MURRAY S. ECKELL
W. DONALD SPARKS
ARTHUR LEVY
DAVID E. AUERBACH
JOSEPH L. MONTE, JR.
RONALD J. KLIMAS
NICHOLAS J. EMPER
STANLEY R. KOTZEN
STEPHEN J. POLAHA
GREGORY G. STAGLIANO
DONALD A. BROWNS
KAREN E. FRIEL
FRANCIS A. URSO

NICHOLAS D. VADINO, JR.
(1961-1977)

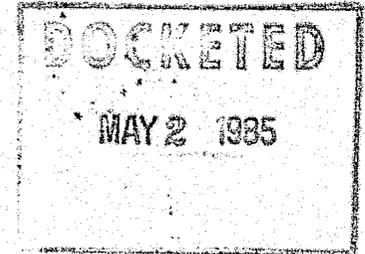
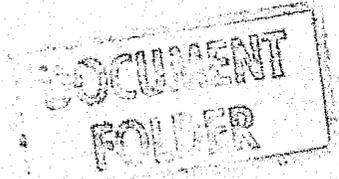
AREA CODE 215
565-3700

RECEIVED



APR 24 1985
PHILADELPHIA OFFICE
ALJ
PA. P.U.C.

April 19, 1985



The Honorable Herbert Smolen
Administrative Law Judge
Pennsylvania Public Utility Commission
Room 1302
Philadelphia State Office Building
Broad and Spring Garden Streets
Philadelphia, Pennsylvania 19130

RE: Borough of Folcroft v. Penn Central Transportation Company, et al. (Primos Avenue Bridge) Docket No.: C-00020235

Dear Judge Smolen:

As you know, I represent the Borough of Folcroft in the above-captioned matter. At the hearing held in this matter on April 4, 1985, I indicated to you on behalf of the Borough that we do not feel that the Borough of Folcroft should be responsible with regard to the maintenance of the sidewalk on the approach to the Primos Avenue Bridge.

You gave me an opportunity to determine whether or not we requested an additional hearing with regard to this matter, and after consultation with my clients, we do not feel that this is necessary. However, I do want to point out that it is the strong feeling of the Borough Council that the Borough should not be responsible for the maintenance of the sidewalk on the approaches to the bridge. The Borough, of course, is not in the business of maintaining this type of sidewalk and does not have the capability of doing so. The Borough has very few employees because of its small size, and we do not feel that we should be made to undertake this responsibility.

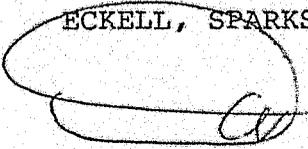
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Administrative
Borough*

The Honorable Herbert Smolen
Administrative Law Judge
Page 2
April 19, 1985

Further, the Borough has budget difficulties, and we do not feel that the residents of the Borough should be forced to incur any further taxes to provide funds for this type of maintenance. As you remember, the representative of PenDot recommended that the Borough be permitted to pass any cost of maintenance along to any adjacent property owner in accordance with our Ordinance. We would, therefore, request that if the Borough is given any responsibility with regard to the sidewalk that we would be able to enforce our local Ordinance which has been introduced into evidence in this case.

Respectfully submitted,

ECKELL, SPARKS, LEVY, AUERBACH & MONTE



By: DAVID E. AUERBACH

DEA:ljb

cc: Herbert Zahn, Esquire
Daniel Donovan, Esquire
Edward Huss, Esquire
Edward Sacks, Esquire
Rudolph A. Chillemi, Esquire
Harold K. Cohen, Esquire
Denis M. Dunn, Esquire
Time-Mirror Cable TV
Mark J. Kropilak, Esquire

Fleisher

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COMMONWEALTH OF PENNSYLVANIA

April 26, 1985

MAY 29 1985

SECRETARY'S OFFICE

SUBJECT: Borough of Folcroft vs. ~~Public Utility Commission~~ Pennsylvania General Transportation Company, et al.
Docket Number C-00020235 - HEARING EXAMINER case

TO: William H. Smith
Chief Administrative Law Judge

FROM: Herbert Smolen
Administrative Law Judge

HS

**BUREAU OF
MAY 1 1985
RAIL
TRANSPORTATION**

I previously forwarded to you two separate memos with attachments in connection with the above matter, so that you would be able to forward same to the appropriate office inasmuch as this is a HEARING EXAMINER matter.

Subsequently, I have received a letter dated April 25, 1985 from Bell of Pennsylvania concerning its position in this matter. The original of said letter is attached hereto.

In addition, I have now received the Notes of Testimony (pages 91 through 299) of the hearing held April 4, 1985. Included with the Notes of Testimony are the Exhibits introduced at said hearing. Again, inasmuch as this is a HEARING EXAMINER proceeding, these materials should be forwarded to the appropriate office for decision and disposition. I would appreciate greatly if you would do so.

If there are any questions, please call.

HS:k1

Attachments

**REPORT
FOLDER**

**RECEIVED
APR 30 1985
Office of the A.L.J.
Public Utility Commission**

William M. Posner
Attorney

RECEIVED

Bell of Pennsylvania

Law Department
One Parkway - 16th Floor
Philadelphia, Pennsylvania 19102
Phone (215) 466-8871

APR 26 1985

PHILADELPHIA OFFICE

ALJ

PA. P.U.C.

April 25, 1985

Honorable Herbert Smolen
Administrative Law Judge
Pennsylvania Public Utility Commission
1302 Philadelphia State Office Building
1400 West Spring Garden Street
Philadelphia, PA 19130

FOLDE

JUN 5 1985

Borough of Folcroft v. Penn Central Transportation
Company, et al. (Primos Avenue Bridge) - C-00020235.

Dear Judge Smolen:

As you may recall, I indicated during the hearing in the above matter on April 4, 1985, that The Bell Telephone Company of Pennsylvania's ("Bell") file reflected the potential that Bell, at an earlier hearing, had agreed to bear its own costs for the relocation of any facilities involved in the Primos Avenue Bridge rehabilitation. Nevertheless, Bell offered a witness, John Thomas, at the April 4, 1985 hearing, and took the position that Bell sought reimbursement. That was done in order to protect the record in the event that the commitment referenced above had not, in fact, been made.

I have now had the opportunity to review the transcript of the April 1974 hearing, presided over by Meyer L. Casman, Esquire. At that hearing, Mr. Kunkel, representing Bell, made the following statement (Tr. 82-83):

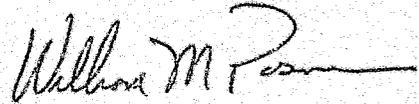
"I will not protract this proceeding by putting a witness on the stand. I think the record should show, however, that there are some telephone facilities in the vicinity of both the crossings involved. The work of relocating these facilities, if that should prove necessary, will not be expensive, and the relocation will be performed by the Telephone Company at its own expense. The Telephone Company does request that the Commission recognize in any order it may issue that might direct the construction of a new bridge at either location that the Telephone Company be accorded the right to place conduit ducts in these structures of the bridges."

Hon. H. Smolen
April 25, 1985
PAGE 2

In view of the foregoing statement made at the 1974 hearing, Bell feels duty-bound to adhere to that commitment, and to agree, in this case, to bear its own relocation expenses. Accordingly, the testimony given on April 4, 1985 -- at least to the extent that it can be interpreted to request reimbursement of Bell's expenses -- may be disregarded by the Pennsylvania Public Utility Commission in its consideration of this matter.

Thank you for your cooperation in this matter.

Very truly yours,



William M. Posner

cc: Jerry Rich, Secy.-Pa. PUC
David E. Auerbach, Esquire
Harold K. Cohen, Esquire
Rudolph A. Chillemi, Esquire
Daniel Donovan, Esquire
Denis M. Dunn, Esquire
Edward Huss, Esquire
Mark J. Kropilak, Esquire
Times-Mirror Cable TV Co.
Herbert Zahn, Esquire

Fleisher

COMMONWEALTH OF PENNSYLVANIA

April 23, 1985

SUBJECT: Borough of Folcroft vs. Penn Central Transportation Company, et al.
Docket Number C-0020235
HEARING EXAMINER CASE

TO: William H. Smith
Chief Administrative Law Judge

FROM: Herbert Smolen
Administrative Law Judge



I conducted a hearing in this matter on April 4, 1985 as a HEARING EXAMINER. Attached hereto is a copy of the Appearance Sheet which sets forth the nature of the Memoranda of Law to be submitted by the parties.

Under date of April 17, 1985, I received a letter from Dennis M. Dunn, Esquire, Counsel for the Borough of Glenolden. I am forwarding the original of said letter to you herewith so that you may, in turn, forward it to the appropriate office which should properly receive it.

If there are any questions, please call.

HS:kl

Attachment

BUREAU OF
APR 23 1985
RAIL
TRANSPORTATION

RECEIVED
APR 25 1985
Office of the A.L.J.
Public Utility Commission

APPEARANCE SHEET

HEARING REPORT

DOCKET NO. C-00020235

CASE NAME Borough of Folcroft vs.

Penn Central Transportation Co.,

et al.

HEARING LOCATION Folcroft, PA.

HEARING DATE April 4, 1985

ALJ Smolen

CHECK THOSE BLOCKS WHICH APPLY:

Hearing concluded 4/30/85 ^{FOLCROFT HAS UNTIL} _{TO REQUEST ADD'L HEARING}

Record closed 207 4/30/85

MEMO Briefs to be filed BY 4/30/85 ^{ON LIMITED} _{ISSUE}
 BOROUGH OF FOLCROFT HAS

Further hearing UNTIL 4/30/85 _{TO REQUEST} ADD'L HEARING ON BOROUGH.

Estimated add'l days ABILITY TO PAY FOR _{MAINTENANCE OF CERTAIN} CURB AND SIDEWALK

BENCH DECISION

REMARKS: MEMOS OF LAW TO BE LIMITED TO (1) AUTHORITIES OF COMMISSIONER REQUIRE FOLCROFT TO MAINTAIN SIDEWALKS IN VIEW OF BOROUGH ORDINANCE PLACING RESPONS. ON PROPERTY OWNERS
(2) POSITION OF PARTIES AS TO WHO SHOULD BE RESP. FOR SIDEWALK MAINTENANCE

THIS IS A HEARING EXAMINER CASE

Names and addresses of parties or counsel of record
Please Print Clearly
Incomplete Information May Result in Delay of Process

NAME	ADDRESS	APPEARING FOR
Harold K. Cohen	527 Sub-Sta. Bldg. City: Phila State: PA Zip: 19103	Amttrak
HERBERT G. ZAHN	5th T & S BLDG City: HSBG State: PA Zip: 17120	PENNDOT
William P. Bone	One Parkview City: Phila State: PA Zip: 19102	Rediff R
MARK KROFILAK	MORAN, LEWIS & LOCKUS 3000 One Logan Square City: Phila State: PA Zip: 19103	Philadelphia Suburban Water Co.

Check this box if additional parties of counsel of record appear on back.

Linda J. [Signature]
Reporter

AVID AUERBAH	Address 344 W FRONT ST. City MEDIA State PA Zip 19063	Bow of Felcroft
EDWARD H LUCAS	Address 130 SOUTH 9TH ST. City PHIL State PA Zip 19077	SEATA
DENIS M. DURM	Address 109 Chesley Drive City Media State PA Zip 19063	Borough of Glenolden
W. WILSON CORNELL	Address 2301 MARKET ST City PHIL State PA Zip 19101	THE ELEC Co
H. J. GALLAGHER	Address 631 N. Office Bldg City HARRISBURG State PA Zip 17120	P.O. C TRIAL STAFF
	Address City State Zip	

ECKELL, SPARKS, LEVY, AUERBACH & MONTE
ATTORNEYS AT LAW
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344 W. FRONT STREET
P. O. BOX 319
MEDIA, PENNSYLVANIA 19063
DELAWARE COUNTY

Fletcher
BUREAU OF
APR 20 1985
RAIL
TRANSPORTATION

MURRAY S. ECKELL
W. DONALD SPARKS
ARTHUR LEVY
DAVID E. AUERBACH
JOSEPH L. MONTE, JR.
RONALD J. KLIMAS
NICHOLAS J. EMPER
STANLEY R. KOTZEN
STEPHEN J. POLAHA
GREGORY G. STAGLIANO
DONALD A. BROWNS
KAREN E. FRIEL
FRANCIS A. URSO

RECEIVED

NICHOLAS D. VADINO, JR.
(1961-1977)

AREA CODE 215
565-3700

RECEIVED
APR 26 1985
Office of the ALJ.
Public Utility Commission

APR 22 1985
PHILADELPHIA OFFICE
ALJ
PA. P.U.C.

April 19, 1985

The Honorable Herbert Smolen
Administrative Law Judge
Pennsylvania Public Utility Commission
Room 1302
Philadelphia State Office Building
Broad and Spring Garden Streets
Philadelphia, Pennsylvania 19130

RE: Borough of Folcroft v. Penn Central Transportation Company, et al. (Primos Avenue Bridge) Docket No.: C-00020235

Dear Judge Smolen:

As you know, I represent the Borough of Folcroft in the above-captioned matter. At the hearing held in this matter on April 4, 1985, I indicated to you on behalf of the Borough that we do not feel that the Borough of Folcroft should be responsible with regard to the maintenance of the sidewalk on the approach to the Primos Avenue Bridge.

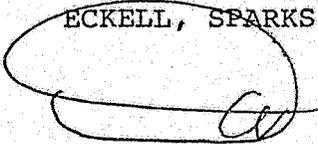
You gave me an opportunity to determine whether or not we requested an additional hearing with regard to this matter, and after consultation with my clients, we do not feel that this is necessary. However, I do want to point out that it is the strong feeling of the Borough Council that the Borough should not be responsible for the maintenance of the sidewalk on the approaches to the bridge. The Borough, of course, is not in the business of maintaining this type of sidewalk and does not have the capability of doing so. The Borough has very few employees because of its small size, and we do not feel that we should be made to undertake this responsibility.

The Honorable Herbert Smolen
Administrative Law Judge
Page 2
April 19, 1985

Further, the Borough has budget difficulties, and we do not feel that the residents of the Borough should be forced to incur any further taxes to provide funds for this type of maintenance. As you remember, the representative of PenDot recommended that the Borough be permitted to pass any cost of maintenance along to any adjacent property owner in accordance with our Ordinance. We would, therefore, request that if the Borough is given any responsibility with regard to the sidewalk that we would be able to enforce our local Ordinance which has been introduced into evidence in this case.

Respectfully submitted,

ECKELL, SPARKS, LEVY, AUERBACH & MONTE



By: DAVID E. AUERBACH

DEA:ljb

cc: Herbert Zahn, Esquire
Daniel Donovan, Esquire
Edward Huss, Esquire
Edward Sacks, Esquire
Rudolph A. Chillemi, Esquire
Harold K. Cohen, Esquire
Denis M. Dunn, Esquire
Time-Mirror Cable TV
Mark J. Kropilak, Esquire

PETRIKIN, WELLMAN, DAMICO, CARNEY & BROWN

A PROFESSIONAL CORPORATION
ATTORNEYS AND COUNSELORS AT LAW

THE WILLIAM PENN BUILDING
109 CHESLEY DRIVE
MEDIA, PA. 19063

MALCOLM B. PETRIKIN
JOHN W. WELLMAN
JOSEPH A. DAMICO, JR.
EDWARD J. CARNEY, JR.
STEVEN G. BROWN
DONALD T. PETROSA
STEVEN A. COHEN
KRISTINE F. HUGHEY
DENIS M. DUNN
MARY K. POLACHECK

AREA CODE 215
565-2670

*Copy -
Original to
file*

REC-6

April 17, 1985

APR 18 1985
PHILADELPHIA OFFICE
ALJ
PA. P.U.G.

The Honorable Herbert Smolen
Administrative Law Judge
Pennsylvania Public Utility Commission
Room 1302
Philadelphia State Office Building
Broad and Spring Garden Streets
Philadelphia, PA 19130

RE: Borough of Folcroft v. Penn
Central Transportation Company,
et al. (Primos Avenue Bridge)
Docket No.: C-00020235
Our File: 32233 M-44

Dear Judge Smolen:

Our office represents the Borough of Glenolden in the above captioned matter. At the hearing held in this matter on April 4, 1985, the Borough of Glenolden indicated that it did not wish to assume the maintenance responsibility for the sidewalks on the approaches to the Primos Avenue Bridge on Glenolden's half of the bridge. The reasons for this objection are that the Borough is not in the business of maintaining sidewalks and that on at least one side of the bridge there is no adjoining property owner to assume this responsibility. Further, for the side of the bridge that does have an adjoining property owner, the proposal contemplates 200 feet of sidewalk which will be well above the grade of that adjoining property owner's property and accordingly will not benefit his property.

At the time of the hearing, H. Gilroy Damon, the Borough Engineer, stated that when the bridge was initially proposed the Borough objected to the presence of a sidewalk on the Glenolden side. Mr. Damon further stated that he was not authorized at that particular hearing to say whether the Borough continued to object to the presence of the sidewalks. Since that time, our office has consulted with the Borough Officials and the Borough has decided that it wants the sidewalks to remain on the approaches.

I contacted Raj Kohli, the PennDot Engineer who testified in this case, on Tuesday, April 16th and told him of the Borough's decision. He informed me that I should contact the Public Utility

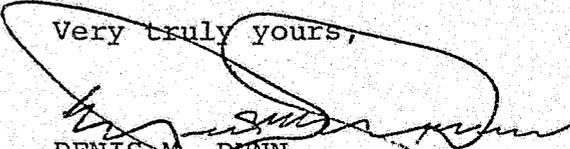
The Honorable Herbert Smolen
April 17, 1985
Page -2-

Commission and that he would alert his engineers not to remove the sidewalks from the plan.

Even though the Borough has requested that the sidewalks remain on the plan, the Borough still requests that it not be given the responsibility for maintaining those sidewalks for the reasons stated above.

I am serving a copy of this letter on all parties of record.

Very truly yours,



DENIS M. DUNN

Counsel for Borough of Glenolden

DMD/dmo

cc: Herbert Zahn, Esquire
Daniel Donovan, Esquire
Edward Huss, Esquire
Edward Sacks, Esquire
Rudolph A. Chillemi, Esquire
Harold K. Cohen, Esquire
David E. Auerbach, Esquire
Times-Mirror Cable TV
Mark J. Kropilak, Esquire



COMMONWEALTH OF PENNSYLVANIA
PENNSYLVANIA PUBLIC UTILITY COMMISSION
P.O. BOX 3265, HARRISBURG, Pa. 17120

ORIGINAL

April 30, 1985

IN REPLY PLEASE
REFER TO OUR FILE

C-20235

Jerry Rich, Secretary
Pa. Public Utility Commission
Post Office Box 3265
Harrisburg, Pennsylvania 17120

RECEIVED
APR 30 1985
SECRETARY'S OFFICE
Public Utility Commission

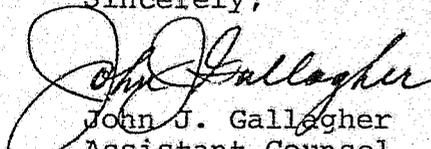
In re: Borough of Folcroft
v.

Penn Central Transportation Company,
Department of Transportation of the
Commonwealth of Pennsylvania, County
of Delaware, Southeastern Pennsyl-
vania Transportation Authority,
Consolidated Rail Corporation and
National Railroad Passenger Corp.

Dear Secretary Rich:

Enclosed please find the original and nine (9) copies
of the Memorandum of Law to be filed in the above-captioned
matter.

Sincerely,


John J. Gallagher
Assistant Counsel

JJG/dmj

Enclosures

cc: Honorable Herbert Smolen
Parties of Record

1 DOCUMENT
FOLCROFT

INTRODUCTION

On January 16, 1974, the Borough of Folcroft filed a complaint with the Commission, in which it alleged that hazardous conditions existed at the crossings where Folcroft Avenue (a borough street) partly in the Borough of Folcroft and partly in the Borough of Glenolden, and where Primos Avenue (State Highway Route 420), partly in Delaware County, cross over and above the grade of electrified tracks of Penn Central Transportation Company (now owned by National Railroad Passenger Corporation, "AMTRAK"). An order was issued on June 25, 1974, sustaining the Borough's complaint and directing that Primos Avenue bridge be posted for a maximum load limit of seven (7) tons and that a new modern structure be built by the Department to replace the existing structure. The Department was directed to submit preliminary plans and cost estimates of the new structure within ninety (90) days. Following a hearing on the plans, the Commission directed, by order dated May 13, 1975, that the Department submit detailed construction and right-of-way plans within fifteen (15) months.

On September 23, 1977 the Department petitioned the Commission for a two (2) year time extension and a hearing on the matter was held on May 10, 1977. In the interim, the Borough notified the Commission that Primos Avenue bridge had deteriorated to the point that the structure had to be closed to vehicular traffic.

On October 6, 1977, the Commission issued an order approving an extension of time until September 30, 1978 for the Department to submit detailed plans. Additionally, the subject structure was reopened, but by Commission order of August 29, 1978, the posting was reduced to a two and a half (2-1/2) ton maximum load limit.

On October 6, 1978, the Department again petitioned for a time extension (until September 30, 1978) but the Commission denied this petition. The Commission, in its order of December 21, 1978, concluded that further delay in the implementation of an improvement at this location would impose an unjustifiable burden on the traveling public. The Department neither appealed nor sought administrative review of the Commission's order of December 21, 1978.

As a result of the Department's non-compliance, then Chief Counsel for the Commission George Kashi, in a letter dated February 5, 1979, requested then Attorney General Edward Biester pursuant to Section 503 of the Public Utility Code, 66 Pa. C.S. §503, to initiate the appropriate action against the Department to assure compliance. The Attorney General never responded to the Commission's request. Section 503 of the Public Utility Code, 66 Pa. C.S. §503, has since been amended to provide the Chief Counsel of the Commission exclusive power to enforce the provisions of the Public Utility Code or Commission regulations or orders by mandamus, injunction or quo warranto, or other appropriate remedy at law or in equity.

PRESENT MATTER

At the hearing of April 4, 1985, the Department agreed to furnish all material and do all work necessary to remove the existing bridge, to construct the new bridge and the highway approaches to the new bridge. In addition, the Department will agree to assume the responsibility for maintenance of the substructure and superstructure of the new bridge. The Department agreed to maintain the highway approaches to the crossing between the curbs and the retaining walls. The Boroughs of Folcroft and Glenolden were recommended by the Department to maintain the curbs and sidewalks on the approaches to the crossing located within each borough. The Borough of Folcroft in its testimony presented a Borough ordinance as one of its exhibits. This ordinance prescribed that individual property owners within the Borough were responsible for the maintenance and repair of sidewalks and curbs which fronted their properties. The Borough is attempting in this proceeding to apply the mandate of its ordinance to a proceeding involving the Commission's jurisdiction at rail-highway crossings. It is the Borough's assertion that its ordinance takes precedence over the Public Utility Code, 66 Pa. C.S, §101, et seq.

The purpose of this memorandum is to present a legal analysis of the validity of municipal ordinances within the specific area of public rail-highway crossings within the Commonwealth of Pennsylvania.

ISSUES PRESENTED

1. Do municipal ordinances supercede the power of the Commission to allocate costs for future maintenance of a public rail-highway crossing and its approaches?

Answer: No.

2. Is evidence of a party's "ability to pay" relevant in a Commission determination of allocation of the costs of future maintenance?

Answer: No.

DISCUSSION

- I. DO MUNICIPAL ORDINANCES SUPERCEDE THE POWER OF THE COMMISSION TO ALLOCATE COSTS FOR FUTURE MAINTENANCE OF A PUBLIC RAIL-HIGHWAY CROSSING AND ITS APPROACHES?

The exclusion and comprehensive jurisdiction and power conferred upon the Public Utility Commission under Section 2702 and 2704 (66 Pa. C.S. §§2702 and 2704) relative to all public rail-highway facilities within the Commonwealth are well established. Pennsylvania Railroad Co. v. Pa. P.U.C., 136 Pa. Super. Ct., 1 A.2d 86 (1939); Pittsburgh Railways Co. v. Pa. P.U.C., 198 Pa. Super. Ct. 415, 182 A.2d 80 (1962); Department of Transportation v. Pennsylvania Public Utility Commission, 21 Pa. Commonwealth Ct. 407, 346 A.2d 371 (1975); Department of Transportation v. Pennsylvania Public Utility Commission, 21 Pa. Commonwealth Ct. 415, 346 A.2d 376 (1975).

In matters, the regulation of which has been placed within the jurisdiction of the Commission, the jurisdiction of the Commission is exclusive, and neither the courts nor municipalities nor departments of state government may regulate in such areas. Philadelphia Electric Co. v. Human Relations Commission, 5 Pa. Commonwealth Ct. 329, 290 A.2d 699 (1972).

The Commission has exclusive jurisdiction over all railroad-highway crossings in the Commonwealth. 66 Pa. C.S. §2702. That jurisdiction has been extended to the approaches of a crossing by case law. Department of Transportation v. Pa. P.U.C., 64 Pa. Commonwealth Ct. 299, 440 A.2d 657 (1982); Springettsbury v. Pa. P.U.C., 5 Pa. Commonwealth Ct. 102, 289 A.2d 762 (1972).

Additionally, the Commission has the exclusive authority to assess the costs of any work ordered performed upon the parties to this proceeding in such proper proportions as the Commission may determine. 66 Pa. C.S. §2704(a). In apportioning costs in rail-highway crossing cases, the Commission is not limited to any fixed rule but takes all relevant factors into consideration, the only requirement being that its order be just and reasonable. Department of Transportation v. Pa. P.U.C., 21 Pa. Commonwealth Ct. 407, 346 A.2d 371 (1975). See also Consolidated Rail Corporation v. Pa. P.U.C., 55 Pa. Commonwealth Ct. 576, 423 A.2d 1108 (1980).

As stated in Middletown Borough v. Pennsylvania Public Utility Commission, 143 Superior Ct. 444, 17 A.2d 904 (1940) the Public Utility Commission has jurisdiction to order abolition and partial relocation of a grade crossing even if its final order deprives the Borough of control of its streets.

The Borough ordinance if valid essentially supercedes the Commission's power to allocate the cost of future maintenance of the sidewalks and curbs on the approaches to the bridge. Such a result clearly contravenes the explicit language of the Public Utility Code and the precedent previously cited. In addition, if the Commission eventually orders the Boroughs to maintain the curbs and sidewalks on their respective approaches (Trial Staff recommendation) the Boroughs are powerless to pass that maintenance responsibility to the affected landowners. Again, such an attempt would amount to a usurpation of Commission authority, i.e., final allocation of costs.

II. IS EVIDENCE OF A PARTY'S "ABILITY TO PAY" RELEVANT IN A COMMISSION DETERMINATION OF ALLOCATION OF THE COSTS OF FUTURE MAINTENANCE?

Unlike the existence of federal funds, which is a relevant factor to consider, Department of Highways v. Pa. P.U.C., 179 Pa. Superior Ct. 376, 116 A.2d 855 (1955), financial ability to pay has been held to be not relevant or determinative of the issue of allocation of costs. In Department of Highways v. Pa. P.U.C., 190 Pa. Superior Ct. 1, 11, 151 A.2d 850, 854 (1959), the Superior Court stated:

We fail to see, in the absence of some specific provision authorizing such factor to be considered relevant or determinative, how the financial ability to pay, in a litigation such as here involved, would control or be determinative of the issue on allocation. Counsel for the Department has cited no authority to us for this position, and we have been unable to find such authority. Nor do we believe that the legislature contemplated that the Commission, in making allocation of costs enter into complicated questions of municipal taxation.

If ability to pay is a major factor to be considered by the Commission, the result would be the existence of many substandard, inadequate, unsafe and closed bridges remaining in place and continuing to deteriorate throughout the Commonwealth, because the parties to any rail-highway crossing case would invariably plead "inability to pay." The argument that a party does not have funds available in its budget to pay costs assessed against it by the Commission is misleading and improper. The nature of crossing cases is such that the parties cannot anticipate the immediate need for funds for crossing repairs, reconstruction or abolition and therefore would not have such funds available in a current budget. However, a party, especially a municipality, would have the ability to obtain the necessary funds through bond issues, taxation, etc. The Commonwealth Court, as recently as December 1983, recognized this ability to obtain funds in Commonwealth of Pennsylvania, Department of Transportation v. Pa. P.U.C., ____ Pa. Commonwealth Ct. ____, 469 A.2d 1149 (1983), by stating in footnote 12 of its opinion:

Beaver County further contends that the Commission did not consider the ability of the parties to pay the subject costs. The County argues that, since the September 12, 1980 hearing on allocation of costs, they have experienced a deficit which amounts to a change of circumstances. Because the County has raised this issue for the first time on appeal, this issue is waived. (Citation omitted.) Moreover, the subject governing bodies have sufficient revenue-raising ability to offset any deficit (emphasis added).

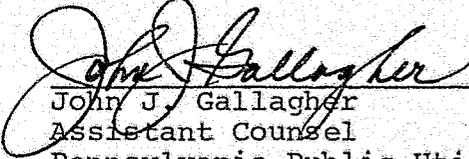
To inquire into present and future ability to pay would therefore take the Commission into those complex matters of taxation which was not within the contemplation of the legislature. Staff therefore submits that such evidence is not relevant in this proceeding and not determinative of the issue of cost allocation.

SUMMARY

The Commission's jurisdiction at rail-highway crossings within the Commonwealth is exclusive and supercedes any Borough ordinance which attempts to define maintenance obligations within those rail-highway crossings.

Finally, financial ability to pay has been held to be not relevant or determinative of the issue of allocation of costs.

Respectfully submitted,



John J. Gallagher
Assistant Counsel
Pennsylvania Public Utility
Commission

Dated: April 30, 1985

CERTIFICATE OF SERVICE

I hereby certify that I am this 30th day of April, 1985,
serving by first class United States mail the foregoing document
upon the persons listed below:

Herbert G. Zahn, Assistant Counsel
Pa. Dept. of Transportation
521 Transportation & Safety Bldg.
Harrisburg, Pennsylvania 17120

Harold Cohen, Esquire
National Railroad Passenger Corp.
Suburban Station Bldg., Rm. 524
1617 John F. Kennedy Boulevard
Philadelphia, Pennsylvania 19103

Edward H. Huss, Esquire
Southeastern Pennsylvania Trans-
portation Authority
130 South Ninth Street, Fifth Flr.
Philadelphia, Pennsylvania 19107

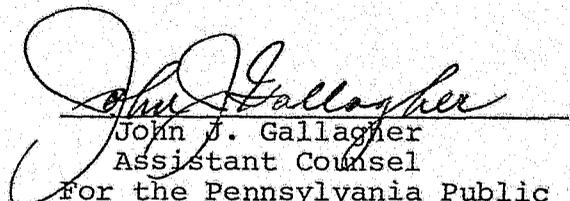
William Posner, Esquire
The Bell Telephone Co. of Pa.
One Parkway
Philadelphia, Pennsylvania 19102

T. H. Maher Cornell, Esquire
Philadelphia Electric Company
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Philadelphia, Pennsylvania 19101

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Media, Pennsylvania 19063

David E. Auerbach, Solicitor
Folcroft Borough
Legal Arts Building
Media, Pennsylvania 19063

Joseph A. Damico, Jr., Solicitor
Glenolden Borough
107 Chelsey Drive
Media, Pennsylvania 19063


John J. Gallagher
Assistant Counsel
For the Pennsylvania Public
Utility Commission

ORIGINAL

Amtrak



MAY 3 1985

RECEIVED

MAY 9 1985

SECRETARY'S OFFICE
Public Utility Commission

Mr. Jerry Rich, Secretary
Pennsylvania Public Utility Commission
P. O. B. 3265
Harrisburg, PA 17120

Re: OH 7.75, Primos Ave.
Folcroft, Pa.
Pa. P.U.C. C-0002035

Dear Mr. Rich:

At the hearing in the above referenced matter on Thursday, April 4, 1985, Amtrak was given 30 days in which to submit a cost estimate for the work required to its facilities to permit construction of the new Primos Ave. bridge.

We submit herewith, Amtrak's Force Account Estimate in the amount of \$334,570. The estimate reflects the work required of railroad forces to facilitate the erection and removal of the temporary pedestrian bridge, removal of the existing highway bridge and erection of the new bridge.

We hereby certify that a copy of this estimate was sent to all parties of record in this matter by First Class, U. S. postage paid mail.

Very truly yours,

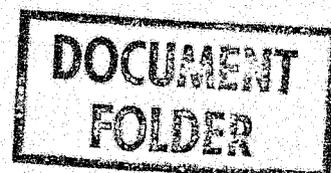
A handwritten signature in black ink, appearing to read "J. N. Michel".

J. N. Michel, P.E.
Senior Director
Engineering Design

RHC/ebj

Attachment: Force Account Estimate (3 pages)

cc: "Parties of Record" (W/Attachment)



PARTIES OF RECORD

Carl Helmetag, Jr., General Counsel
Trustees of Penn Central Transportation Company
3100 Industrial Valley, Bank Building
1700 Market Street
Philadelphia, Pennsylvania 19103

Edward H. Huss, Staff Counsel
Southeastern Pennsylvania Transportation Authority
130 South Ninth Street, Fifth Floor
Philadelphia, Pennsylvania 19107

David E. Auerbach, Solicitor
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Legal Arts Building
Media, Pennsylvania 19063

Joseph A. Damico, Jr., Solicitor
Borough of Glenolden
Chamber of Commerce Building
Baltimore Pike
Media, Pennsylvania 19063

Francis P. Connors, Solicitor
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Herbert Zahn, Esquire
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Ralph Cornell, Esquire
Philadelphia Electric Company
2301 Market Street
Philadelphia, Pennsylvania 19103

Mark J. Kropilak, Esquire
Philadelphia Suburban Water Company
Morgan, Lewis, and Bockius
One Logan Square
Philadelphia, Pennsylvania 19109

PROJECT TITLE: Folcroft, Pennsylvania
 OH. Bridge No. 7.75, Primos Avenue
 Bridge Reconstruction Over and Across Amtrak's Main Line
 Delaware County LR 420, Section 02A
 PA. P.U.C. C-00020235

NATIONAL RAILROAD PASSENGER CORPORATION
 OFFICE OF THE CHIEF ENGINEER

DATE: April 25, 1985

BY: Earl Watson III *E. Watson III*

ITEM NO.	DESCRIPTION	ESTIMATED QUANTITY	UNIT	ESTIMATED AMOUNT
1.	<u>CONSTRUCTION</u>			
	Field Engineer	60	M/D	18,000
	Shop Drawing Review	12	M/D	3,600
2.	<u>PROTECTION</u>			
	Flagman	220	M/D	60,500
	Class 'A' men (2)	200	M/D	60,000
	Track Foreman	90	M/D	27,000
	Signal Maintainer	30	M/D	9,000
	B&B Inspector	15	M/D	4,500
	Pick-up Trucks (4)	235	Days	8,225
3.	<u>ELECTRIC TRACTION</u>			
a.	Remove Attachments, Reprofile Catenaries for 1,2,3 & 4 Tracks Between W-96, W-96A, W-96B & W-97			
	Asst. Supervisor E.T.	8	M/D	2,600
	Foreman	8	M/D	2,600
	Gang Foreman	8	M/D	2,600
	Linemen (5)	40	M/D	12,000
	Wire Train	8	Days	40,000
	Pick-up Trucks (2)	16	Days	560

**DOCUMENT
FOLDER**

DOCKETED
MAY 9 1985

PROJECT TITLE: Folcroft, Pennsylvania
 OH. Bridge No. 7.75, Primos Avenue
 Bridge Reconstruction Over and Across Amtrak's Main Line
 Delaware County LR 420, Section 02A
 PA. P.U.C. C-00020235

NATIONAL RAILROAD PASSENGER CORPORATION
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ITEM NO.	DESCRIPTION	ESTIMATED QUANTITY	UNIT	ESTIMATED AMOUNT
b.	<u>Bond & Ground Temp. Pedestrian Bridge and Temp. Protection Shields</u>			
	Asst. Supervisor E.T.	5	M/D	1,625
	Foreman	5	M/D	1,625
	Gang Foreman	5	M/D	1,625
	Linemen (5)	25	M/D	7,500
	Pick-up Trucks (2)	10	Days	350
	Line Truck	5	Days	425
c.	<u>Bond & Ground Permanent Protective Barrier & Fence</u>			
	Asst. Supervisor E.T.	10	M/D	3,250
	Foreman	10	M/D	3,250
	Gang Foreman	10	M/D	3,250
	Linemen (5)	50	M/D	15,000
	Pick-up Trucks (2)	20	Days	700
	Line Truck	10	Days	850
d.	<u>Bond & Ground Water Line on New Bridge</u>			
	Asst. Supervisor E.T.	2	M/D	650
	Foreman	2	M/D	650
	Gang Foreman	2	M/D	650
	Linemen (5)	10	M/D	3,000
	Pick-up Trucks (2)	4	Days	140
	Line Truck	2	Days	170

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ITEM NO.	DESCRIPTION	ESTIMATED QUANTITY	UNIT	ESTIMATED AMOUNT
e.	Ground Structures			
	<u>W-96A, W-96B & W-95A</u>			
	Asst. Supervisor E.T.	2	M/D	650
	Foreman	2	M/D	650
	Gang Foreman	2	M/D	650
	Linemen (5)	10	M/D	3,000
	Pick-up Trucks (2)	4	Days	140
	Line Truck	2	Days	170
4.	<u>ACCOUNTING</u>	12	M/D	3,000
				<hr/>
	SUB-TOTAL			304,155
	CONTINGENCY (10%)			<hr/> 30,415
	TOTAL			\$ 334,570

ORIGINAL

**Philadelphia
Suburban
Water Company**

762 Lancaster Avenue, Bryn Mawr, Pennsylvania 19010 215/LA5-1400

RECEIVED

JUN 7 1985

June 4, 1985

SECRETARY'S OFFICE
Public Utility Commission

Jerry Rich, Secretary
Pennsylvania Public Utility
Commission
P.O. Box 3265
Harrisburg, Pennsylvania 17120

Re: Borough of Folcroft v. Penn Central Transportation Company, et al.
(Primos Avenue Bridge) Docket No. C-00020235

Dear Secretary Rich:

Please note that my address as counsel for Philadelphia Suburban Water Company in the above-captioned case has changed to the address listed above.

By copy of this letter, I am advising all parties of this change.

Sincerely,

Mark J. Kropilak

Mark J. Kropilak
Assistant Corporate Counsel

MJK/sss

cc: Honorable Herbert Smolen
Administrative Law Judge
ALL PARTIES OF RECORD

