

BEFORE

THE PENNSYLVANIA PUBLIC UTILITY COMMISSION

In re: C-844440, Pennsylvania Department of Transportation vs. Consolidated Rail Corporation, Synder Township, Blair County and The Bell Telephone Company of Pennsylvania Alleging that the bridge carrying State Highway Route 55 (former U.S. Traffic Route 220) above the grade of the railroad tracks in Synder Township, Blair County, is in need of structural repairs.

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Verbatim report of hearing held in  
Small Hearing Room, Hyland Hall Annex,  
Hollidaysburg, Pennsylvania

Wednesday,  
June 26, 1985  
at 10:00 a.m.

BEFORE

LARRY GESOFF, ADMISTRATIVE LAW JUDGE

- - - - -

APPEARANCES:

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Appearing on behalf of Pennsylvania Department of Transportation

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 Altoona, Pennsylvania 16601  
 Appearing on behalf of Juniata Township

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## P R O C E E D I N G S

1  
2 JUDGE GESOFF: This is the time and place set for  
3 Pennsylvania Utility Commission to here the Complaint of  
4 Pennsylvania Department of Transportation against  
5 Consolidated Rail Corporation, Synder Township, Blair County  
6 and Bell Telephone Company of Pennsylvania. This matter is  
7 docketed at C-844440, and involves a bridge carrying State  
8 Highway Route 55 above the grade of railroad tracks in  
9 Synder Township, Blair County and a license that said bridge  
10 is in need of structural repairs, for the purpose -- Off the  
11 Record.

12 (Whereupon, a brief discussion was  
13 held off the record).

14 JUDGE GESOFF: (Continuing) Said bridge is in need  
15 of structural repairs.

16 Okay. My name is Larry Gesoff and I'm the  
17 Administrative Law Judge assigned to this proceeding. At  
18 this time I would like counsel to identify themselves for  
19 the benefit of the court reporter, starting with Mr.  
20 Herskovitz.

21 MR. HERSKOVITZ: Richard Herskovitz appearing on  
22 behalf of the Public Utility Commission Trial Staff.

23 MR. MARTIN: Stephen Martin representing the  
24 Department of Transportation.

25 MR. GIBBONEY: My name is Allen Gibboney and I am

1 Solicitor for Synder Township.

2 MR. DONOVAN: Daniel F. Donovan appearing for  
3 Conrail.

4 JUDGE GESOFF: Very well. Is anyone here representing  
5 Blair County? Let the Record show no response and let the  
6 Record also indicate that Blair County has been given ample  
7 notice of this proceeding, and the report I have does not  
8 show that on the Answer to the Complaint filed on behalf of  
9 Blair County.

10 Is there anything preliminary that any parties wish to  
11 raise?

12 MR. MARTIN: Yes, Your Honor, preliminarily, I would  
13 apologize, we do not have at this time a Proof of  
14 Publication on file, however I would state for the Record,  
15 Notice of this hearing was published in the Altoona Mirror  
16 on June 10th and on June 17th and I would request a  
17 permission to submit that.

18 JUDGE GESOFF: Yes, that's permissible.

19 MR. MARTIN: Thank you, Your Honor.

20 JUDGE GESOFF: Anything further? All right. We can  
21 begin with PennDOT.

22 MR. MARTIN: And, Your Honor, I would call Mr. Milton  
23 Davis to the stand.

24 Whereupon,

25 Milton H. Davis

1 having been duly sworn, testified as follows:

2 DIRECT EXAMINATION

3 BY MR. MARTIN:

4 Q Would you state your name for the Record?

5 A Milton H. Davis.

6 Q And by whom are you employed?

7 A Pennsylvania Department of Transportation.

8 Q In what capacity are you employed?

9 A Assistant Grade Crossing Engineer.

10 Q Mr. Davis, are you authorized today to testify on  
11 behalf of the Department of Transportation in this  
12 proceeding?

13 A Yes, sir, I am.

14 Q Mr. Davis, are you familiar with the questions and  
15 procedures promulgated by the Commission for this hearing?

16 A Yes, I am.

17 Q Mr. Davis, let me refer --

18 MR. MARTIN: Just a moment. Your Honor, could I move  
19 that these be incorporated into the Record so I may be --  
20 the questions and procedures?

21 JUDGE GESOFF: Yes. Fine.

22 BY MR. MARTIN:

23 Q Mr. Davis, referring to question and procedure No.  
24 1, would you briefly describe the general location of the  
25 existing railroad-highway crossing involved in this

1 proceeding?

2 A The crossing involved in this proceeding is located  
3 in the Village of Grazierville, Synder Township, Blair  
4 County, approximately three-quarter mile southwest of the  
5 Borough of Tyrone, which is also in Blair County.

6 Q And what public highway is involved in this  
7 proceeding?

8 A The highway involved in this proceeding is  
9 Legislative Route 55.

10 Q Could you indicate where the Legislative Route 55  
11 begins and ends?

12 A L.R. 55 begins in Hollidaysburg Borough Blair  
13 County and extends to the Huntington Borough, Huntington  
14 County.

15 Q Can you briefly describe the type of pavement  
16 alignment and general physical condition of the highway  
17 approaches to this crossing?

18 A The highway through the immediate crossing area is  
19 on tangent and generally 40 feet wide. The horizontal  
20 alignment of the highway approaching the area of the  
21 crossing is on a gradual curve to the left. The horizontal  
22 alignment of the highway departing the area of the crossing  
23 is on a gradual curve to the right. The first curve to the  
24 left. Did I have it that way?

25 JUDGE GESOFF: Yes.

1           A (Continuing) The horizontal alignment of the south  
2 approach begins at a point approximately 800 feet south of  
3 the structure is on a 369 foot long 1 degree 22 minute curve  
4 to the left compounding into a 515 foot long 2 degree 30  
5 minute curve to the left at a point approximately 400 feet  
6 preceding the structure. The structure itself is on tangent  
7 alignment.

8           The vertical alignment begins at a point approximately  
9 800 feet south of the structure is on an ascending 4.74  
10 percent grade, tying into an 850 foot long crest vertical  
11 curve transitioning from an ascending 5 percent grade to a  
12 descending 5 percent grade. The crest curve begins at a  
13 point of approximately 400 feet south of the structure and  
14 continues across the structure.

15           The north approach to the crossing is a continuation  
16 of the same three lane undivided pavement of the same type  
17 and width as the 400 feet adjacent to the structure on the  
18 south approach, except at a point approximately 600 feet  
19 north of the structure the pavement transitions from a 40  
20 foot width to a 30 foot width. The horizontal alignment  
21 leaving the structure on the north approach passes thru a  
22 554 foot long 5 degree 30 minute curve to the right and at a  
23 point approximately 800 feet north of the structure, enters  
24 a 775 foot long 3 degree curve to the left. The vertical  
25 alignment leaves the structure on the north approach as a

1 continuation of the 850 foot long crest vertical curve  
2 passing over the structure from the south approach. The  
3 vertical curve ends approximately 300 feet north of the  
4 structure and continues for approximately the next 300 feet  
5 on a descending 5 percent grade. At a point approximately  
6 600 feet north of the structure, the vertical alignment  
7 enters a 350 foot long sag vertical curve transitioning from  
8 the descending 5 percent grade to a descending 1.13 percent  
9 grade.

10 Both the north -- both the south and north approaches  
11 have been resurfaced with bituminous material and are in  
12 excellent condition.

13 Q Mr. Davis, what is the Department of  
14 Transportation's present obligations with respect to  
15 maintenance of this existing above grade crossing?

16 A By Public Utility Commission Order dated March 20,  
17 1939, at Complaint Docket No. 1005, the Pennsylvania  
18 Department of Highways, now Pennsylvania Department of  
19 Transportation, was ordered to maintain the roadway paving  
20 on the structure and maintain the approaches thereto. The  
21 Department of Transportation has no obligation to the  
22 maintenance of the structure excluding the roadway pavement  
23 thereon.

24 MR. MARTIN: Your Honor, at this time I would like  
25 marked for identification PennDOT Exhibit No. 1.

1 JUDGE GESOFF: Let it be so marked.

2 (PennDOT Exhibit No. 1 was produced and marked for  
3 identification.)

4 MR. MARTIN: I don't have enough copies.

5 BY MR. MARTIN:

6 Q Mr. Davis, I show you what has been marked as  
7 PennDOT Exhibit No. 1, Public Utility Commission Order to  
8 which you have just referred.

9 A Yes, sir.

10 Q Thank you. Mr. Davis, could you indicate please  
11 what the present and estimated future highway traffic at  
12 this location is?

13 A The present highway traffic using this structure is  
14 8394 vehicles per day, 2 percent which are trucks. It is  
15 anticipated that future highway traffic will increase at a  
16 normal one and one quarter percent per year, compounded  
17 annually.

18 Immediately south of this structure there is an  
19 intersection with new traffic Route 220 that bypasses Tyrone  
20 to the east and is presently carrying a lot of the traffic  
21 that originally was carried by the structure.

22 JUDGE GESOFF: Off the Record.

23 (Whereupon, a brief discussion was  
24 held off the record).  
25

1 BY MR. MARTIN:

2 Q Mr. Davis, referring to Question No. 6 promulgated  
3 by the Commission, could you describe in detail the  
4 additions which presently exist at this crossing, which in  
5 the opinion of the Department renders it dangerous or unsafe  
6 to the traveling public?

7 A Holes continually develop in the pavement of the  
8 north approach in the area of the structures north backwall,  
9 due to the deteriorated state of the backwall and its  
10 inability to adequately retain the approach roadway fill.

11 Several areas along the curbline of the structure deck  
12 have completely holed through.

13 Both of these conditions render the crossing dangerous  
14 to the safety of the highway traffic. If struck by a  
15 vehicle, the holes could cause loss of control of the  
16 vehicle. Not only could an out-of-control vehicle collide  
17 with an oncoming vehicle, but could also impact the main  
18 structural members of the truss.

19 And in addition to the foregoing, a structural  
20 analysis made of that the structure has revealed the  
21 structure is incapable of properly handling current  
22 Pennsylvania legal loadings. Based upon current  
23 Pennsylvania Department of Transportation guidelines for  
24 structure ratings, the maximum allowable loading which  
25 should be permitted to traverse the structure is 29 tons.

1 The current maximum allowable load by 1982, Pennsylvania  
2 Vehicle Code is 40 tons.

3 Q Mr. Davis, referring to Question No. 7, is it the  
4 opinion of the Department that this crossing is adequate,  
5 and if it is not, would you indicate please what the  
6 Department believes the Commission should order be done with  
7 this location?

8 A The structure in its present state of disrepair, is  
9 inadequate for safety, accommodation and convenience of the  
10 traveling public.

11 It is imperative that structure be rehabilitated  
12 before further deterioration renders rehabilitation  
13 impractical or impossible.

14 Q Mr. Davis, referring to Question 8, would you  
15 indicate please whether any structural inspections or any  
16 construction plans have been prepared for this bridge  
17 structure?

18 A An independent consultant hired by the Pennsylvania  
19 Department of Transportation performed an in-depth  
20 inspection of the structure in November of 1981. A report  
21 of that inspection was prepared by the consultant in July of  
22 1982. A copy of the report was sent to Conrail on July 21,  
23 1982 and again on October 3, 1983.

24 In addition, in the interest of the safety of the  
25 traveling public, the Pennsylvania Department of

1 Transportation previously inspected the structure in August  
2 of 1976, November of 1979 and again in June 1983 and  
3 November 1984.

4 Q Mr. Davis, will the Department of Transportation  
5 agree to prepare any construction plans or estimates for  
6 work on the structure?

7 A No.

8 Q Why is that?

9 A With regard to preparation of plans, repair plans,  
10 rather than the construction plans is what is needed. We  
11 cannot agree to assume the responsibility for preparing  
12 plans to correct the deteriorated condition of the structure  
13 that has been caused by the lack of proper maintenance. The  
14 argument that salt ions caused the deteriorated condition is  
15 a pretense that disguises the fact that salt only  
16 accelerates an already deteriorating condition. If the  
17 steel and concrete deck had received adequate preventative  
18 maintenance, the salt ions would not have been able to react  
19 with the steel members or reinforcing.

20 Q Mr. Davis, referring to Question 9, would you  
21 indicate please what portion of the work the Department of  
22 Transportation would agree to perform and what cost the  
23 Department would agree to assume?

24 A The Pennsylvania Department of Transportation will  
25 agree to perform only roadway paving repair after adequate

1 structural repair has been performed by others to sustain a  
2 satisfactory roadway surface. The Department will agree to  
3 bear only the cost of the roadway paving repair and to  
4 maintain, only the roadway paving upon improvement  
5 completion.

6 Furthermore, if the Department is caused to bear more  
7 than 25 percent of the cost of correcting the railroad  
8 company's negligence, it will probably be necessary for us  
9 to use Federal Highway Gasoline tax revenues to help defray  
10 the cost. If we use Federal funds, the Federal Highway  
11 Administration has overruled the Commonwealth's right to  
12 charge Conrail with any past negligence. Unfortunately the  
13 only thing Conrail can be charged with is the responsibility  
14 to continue with the maintenance which was assigned to its  
15 predecessor, the Pennsylvania Railroad Company.

16 Q Mr. Davis, would you refer again to what's been  
17 marked as PennDOT Exhibit No. 1, and in answer to Question  
18 21 promulgated by the Commission, is this the Order which  
19 directed the construction of this bridge?

20 A Yes, sir.

21 Q Is there anything, Mr. Davis, that I have neglected  
22 to ask you that you wanted to add at this time?

23 A Referring to our Complaint, that was received from  
24 the Commission on November 2nd, 1984, the third item which  
25 asks, "What is your Complaint (describe the problem)". The

1 second sentence states, "The structure of subject crossing  
2 is generally in fair condition". I would prefer to use the  
3 word marginal rather than fair at this point in time. I  
4 think that's all that I have from looking through the --

5 MR. MARTIN: Go off the Record a minute.

6 JUDGE GESOFF: Off the Record.

7 (Whereupon, a brief discussion was  
8 held off the record).

9 MR. MARTIN: Your Honor, I would like marked as  
10 PennDOT Exhibit No. 2 an inspection report prepared by the  
11 Department.

12 JUDGE GESOFF: It may be so marked.

13 (PennDOT Exhibit No. 2 was produced and marked for  
14 identification.)

15 BY MR. MARTIN:

16 Q Mr. Davis, I show you what has been marked as  
17 PennDOT Exhibit No. 2, would you identify what this is,  
18 please.

19 A Yes, sir. PennDOT Exhibit No. 2 is a copy of the  
20 in-depth inspection report prepared by A. G. Lichtenstein &  
21 Associates, Incorporated. It is the report that I referred  
22 to in my direct testimony -- in my previous testimony  
23 regarding the railroad overhead bridge No. 224.13 which  
24 carries Old U.S. Route 220. It is now relocated, as I said,  
25 to the east of Tyrone and it carries Legislative Route 55

1 still over the tracks of Conrail and Synder Township, Blair  
2 County, and the report was dated July 1982.

3 Q Thank you, Mr. Davis.

4 MR. MARTIN: Your Honor, I apologize, again, I do not  
5 have enough copies of this document to go around. I can  
6 give a copy to the Township. Conrail has already been  
7 forwarded a copy. If, Your Honor, please, could we use one  
8 of the reporter's copies for the Commission?

9 JUDGE GESOFF: Yes.

10 MR. MARTIN: That's all I have on direct of Mr. Davis,  
11 Your Honor, and he is available for cross-examination.

12 JUDGE GESOFF: All right. Your Exhibit No. 1 has a  
13 second Order attached to it. Do you want to explain that,  
14 two articles attached to it.

15 MR. DONOVAN: Go off the record a second.

16 (Whereupon, a brief discussion was  
17 held off the record).

18 JUDGE GESOFF: Back on the Record. I asked for an  
19 explanation.

20 MR. MARTIN: Your Honor, it appears and duly  
21 indicating from our files, there are no laws in Pennsylvania  
22 to the Exhibit No. -- articles in PennDOT Exhibit No. 1.

23 JUDGE GESOFF: They relate to this crossing; is that  
24 correct?

25 MR. MARTIN: They do relate to this crossing, yes.

1 The second and third Orders do not address any maintenance  
2 question or construction issues. They deal with other  
3 matters which are not particularly relevant to this  
4 proceeding. But as I said, they do involve this crossing.  
5 If other parties object to their inclusion when I move to  
6 admit these, they can be delayed.

7 JUDGE GESOFF: Thank you. Cross-examination by Trial  
8 Staff first.

9 MR. HERSKOVITZ: Couple questions.

10 CROSS-EXAMINATION

11 BY MR. HERSKOVITZ:

12 Q Mr. Davis, you made the statement that salt  
13 accelerates only -- accelerates an already deteriorating  
14 condition; is that correct?

15 A That's correct.

16 Q You're speaking of road salt that the Department  
17 puts on the wearing surface during the winter?

18 A The Department does not put salt on the wearing  
19 surface of the structure. It puts salt on the approaches.  
20 It's under the direction -- its maintenance forces are under  
21 the direction not to add salt to the structure itself,  
22 however, of course, structures -- salt is carried on the  
23 structure by wheels of approaching vehicles.

24 Q Right. Now, you also made the statement that  
25 because salt accelerates the deteriorated condition that

1 there should have been preventative maintenance performed to  
2 stop that deterioration; is that correct?

3 A Yes, sir.

4 Q What would you consider to be preventative  
5 maintenance?

6 A Cleaning and painting the steel. If Conrail at any  
7 time would have desired to prep the deck with any retardant,  
8 such as bauxite paint or linseed oil or whatever, the  
9 Department would have gladly removed the paving so that they  
10 could have done that.

11 Q All right. Do you know whether -- you have been on  
12 that bridge; is that correct?

13 A Yes, sir, I have.

14 Q Do you know, near the curves and sidewalks, whether  
15 or not there are cinders and dirt throughout the entire  
16 sidewalk and curve area?

17 A Cinders, dirt, and deteriorated portions of the  
18 concrete curbing, yes, sir.

19 Q It is your opinion -- was it PennDOT's maintenance  
20 responsibility to clear that debris away as part of its  
21 maintenance obligation?

22 A No.

23 Q Do you know where the cinders came from; was it  
24 from PennDOT's <sup>instructions</sup> instructions perhaps?

25 A Yes, it probably was.

1 Q That would lead to deterioration though that  
2 condition?

3 A Not if preventative maintenance would have been  
4 performed.

5 Q The fact if I were to tell you these cinders were  
6 on part of the roadway, would you say that was PennDOT's  
7 maintenance obligation to remove?

8 A The Department's obligation is to maintain the  
9 wearing surface, and it has maintained the wearing surface  
10 as it best can in view of the deteriorated condition of the  
11 those curb gutters that have just deteriorated and  
12 disintegrated.

13 Q You made the statement and it's in your inspection  
14 report that the bridge should be posted for a lower than  
15 legal load limit I believe?

16 A Where do you see that in the report, counsel?

17 Q That's in the recommendation section. I believe on  
18 page -- well, I don't see a page number on it, but it is  
19 right on the first page of your recommendation. On page  
20 21. In Recommendation No. 1 the final sentence states,  
21 "These railings indicate that a restrictive load posting  
22 should be considered by the DOT".

23 JUDGE GESOFF: Off the Record.

24 (Whereupon, a brief discussion was  
25 held off the record).

1 JUDGE GESOFF: Back on the Record.

2 BY MR. HERSKOVITZ:

3 Q Now, on page 21 of the inspection report, the last  
4 sentence of your first recommendation states, "These ratings  
5 indicate that a restrictive load posting should be  
6 considered by the DOT", and you also made the statement in  
7 your direct testimony that the rating from this inspection  
8 report is 29 tons, although the legal load limit in  
9 Pennsylvania is 40 ton?

10 A That's correct.

11 Q Now, what should this bridge be posted for?

12 A Twenty-nine tons.

13 Q Twenty-nine tons. You also recommended in your  
14 report that -- and that's on the same page 21 and it's in  
15 paragraph 3. "Concurrently, a complete deck evaluation  
16 study should be performed to determine the feasibility of  
17 providing a new wearing surface, et cetera".

18 Is the Department willing to perform such a study?

19 A We are not willing to perform any study on anything  
20 accept the wearing surface. Anything having to do with the  
21 structure, substructure, superstructure and deck should be  
22 performed by a party other than the Department.

23 JUDGE GESOFF: Now, answer the question yes or no.

24 A No.

25 JUDGE GESOFF: Now, wait a minute. I think I see a

1 contradiction here. You're saying you're willing to prepare  
2 the studies for wearing surfaces, and the sentence that was  
3 read to you on page 21 refers to a wearing surface. Now,  
4 are you -- is PennDOT not willing to prepare such a study?

5 THE WITNESS: Is that the question?

6 JUDGE GESOFF: That's what I thought the question  
7 was.

8 BY MR. HERSKOVITZ:

9 Q The question says it deals with the wearing  
10 surface, that they are interrelated as I see. Or maybe they  
11 are not interrelated. Maybe you should explain that  
12 sentence.

13 A What does the sentence say, concurrently?

14 Q It says concurrently.

15 A A complete deck evaluation?

16 Q Right.

17 A Then I'm saying to you the deck is part of the  
18 superstructure.

19 Q But this relates to the feasibility of providing a  
20 new wearing surface which is what you're saying the  
21 Department had a responsibility of?

22 A And my answer was, no, sir.

23 JUDGE GESOFF: Still no?

24 THE WITNESS: Right.

25 BY MR. HERSKOVITZ:

1 Q Okay.

2 A We do believe that the Department and the public  
3 should be responsible for assuming obligations to correct a  
4 condition that has not been preserved, that has not been  
5 corrected by the party responsible for maintaining the  
6 structure.

7 Q Is it your opinion that plans are required for  
8 rehabilitation of this structure in accordance with your  
9 recommendations?

10 A Yes, there should be a plan prepared for  
11 rehabilitation.

12 Q Should the structure be reinspected prior to plans  
13 being prepared in light of the fact this report is some  
14 three years old?

15 A In the preparation of plans, the engineering firm  
16 retained to prepare those plans would have to inspect the  
17 structure to determine the localized deteriorated portions  
18 of that bridge and according to -- and prepare plans  
19 accordingly.

20 Q How long would it take to prepare such plans?

21 A Well, if you're asking how long it would take the  
22 Department, it would take the Department better than nine  
23 months to simply retain an engineering firm, and then after  
24 the firm has been retained, it would take approximately a  
25 year or maybe nine months again to prepare those plans. So

1 in essence, you get the Commission and all the parties of  
2 record, I would say it would take somewhere in the area of  
3 approximately a year and three quarters to two years in  
4 time.

5 Q Assuming there is no administrative problems and  
6 it's not the Department that's going to prepare the plans,  
7 how long would it take any other party to prepare the plans;  
8 the actual work of preparing the plans?

9 A I'm not familiar with any private firms.

10 Q How long would it take -- without the  
11 administrative problems that PennDOT would agree to, how  
12 long would it actually take to draft the plans if they said,  
13 all right, today go?

14 A Approximately a year.

15 Q I don't see any estimates of costs for any of this  
16 work that is recommended in the report. Do you know whether  
17 there is an estimate of cost for this work; do you have an  
18 estimate?

19 A No, we don't.

20 Q If plans were prepared by other parties, do they  
21 have to follow PennDOT's specifications and meet approval of  
22 PennDOT, besides the Commission of course?

23 A If plans were prepared in accordance with the  
24 Public Utility Commission Order, I would imagine the  
25 Commission would refer those plans to us for our perusal,

1 but it is not required that the Department approves those  
2 plans, per se. It's up to the Commission to approve them.

3 Q I don't recall if you stated, when you were giving  
4 the daily traffic count -- what's the nature of the traffic,  
5 is it local traffic, thru traffic?

6 A It is primarily local. It is traffic that does  
7 want to either travel to or leave the Borough of Tyrone. If  
8 it wanted to travel further north or travel further south,  
9 it could use the new 220 bypass, which was built in the  
10 early 1970's. The intersection just immediately south of  
11 the structure was built in 1969.

12 Q If there is thru traffic that wants to bypass  
13 Tyrone, they would use the bypass?

14 A Yes, sir.

15 Q Instead of the street?

16 A Yes, sir.

17 MR. HERSKOVITZ: Thank you. That's all I have.

18 JUDGE GESOFF: Mr. Gibboney?

19 CROSS-EXAMINATION

20 BY MR. GIBBONEY:

21 Q Mr. Davis, I'm the --

22 A Does he represent Snyder Township?

23 Q That's correct. Mr. Davis, you make no allegation  
24 that there has been any negligence in the maintenance or  
25 care of this bridge by Snyder Township?

1 A That's correct, sir.

2 Q And you make no allegation that there is any  
3 responsibility for the care or maintenance of this bridge by  
4 Synder Township?

5 A That's correct, sir.

6 Q There is no allegation that Synder Township is in  
7 anyway connected with or has any responsibility with regard  
8 to any matters concerning this bridge?

9 A I can't agree with that. The plans -- to answer  
10 it, the structure is located within the confines of Synder  
11 Township and Snyder Township's residents, the people in the  
12 neighboring area, do use that structure.

13 Q Within the confines of the Order that you have  
14 produced, there is no ordered responsibility to Synder  
15 Township concerning anything in connection with that bridge  
16 at this time?

17 A That's correct, sir.

18 MR. GIBBONEY: I have no other questions.

19 CROSS-EXAMINATION

20 BY MR. DONOVAN:

21 Q Mr. Davis, you stated that the Department of  
22 Transportation had made a series of inspections of the  
23 bridge, and you mentioned a report of 1976; is that correct?

24 A Yes, I remember mentioning the year 1976..

25 JUDGE GESOFF: August 1976 inspection?

1           A It wasn't a report, sir, it was just in 1976 we had  
2 noted that the deficiencies were already developing.

3           Q I see. And could you tell us what those  
4 deficiencies were?

5           A I am sure that the backwalls and --

6           Q Now, are you testifying now based upon the report  
7 that you referred to that was made in 1976?

8           A No, sir. As I understand it, there was no report  
9 in 1976. It was just simply an inspection, that from my  
10 correspondence inferred or stated that there were  
11 deficiencies already developing. Now, what those  
12 deficiencies were, I'm not sure.

13          Q I see. Would you think, based upon your experience  
14 as a bridge engineer for the Department of Transportation  
15 and the Department of Highways before that, that the  
16 reference to the deficiencies would indicate to you that  
17 there had been spalling of the concrete?

18          A Yes, sir.

19          Q Would it also indicate to you that perhaps that  
20 report or that the finding of that inspection indicated a  
21 failing of the concrete also?

22          A The spalling in itself is failing.

23          Q The spalling of the concrete is that a -- that's  
24 concrete that's created overnight, Mr. Davis?

25          A No, it is not.

1 Q It's a condition that is created over a fairly long  
2 period of time; isn't that correct?

3 A That's correct.

4 Q Now, do you have any indication in your file  
5 whether or not there were earlier reports than 1976  
6 indicating that these deficiencies existed?

7 A No.

8 Q I take it there were no reports addressing that  
9 particular subject in your files? In other words, either  
10 yes or no; is that correct?

11 A There were no reports in my file from my review of  
12 the file prior to 1976.

13 Q Yes.

14 A If you just take a moment, I will go through it  
15 again. It will just take a minute.

16 Counselor, there is not in my file. We have a  
17 district bridge engineer here today --

18 Q We will get to him next.

19 A -- could perhaps shed more light on past  
20 maintenance.

21 Q Do you have any records in your file that would  
22 indicate whether or not the Department of Transportation  
23 and/or the Department of Highways ever resurfaced this  
24 structure?

25 A If it is not included in this July 1982 report, I

1 do not.

2 Q Incidentally, I take it, Mr. Davis, since you  
3 sponsored this exhibit, PennDOT's Exhibit 2, that you concur  
4 in the findings and recommendations of that report, is that  
5 correct?

6 /  
7 Is that a complicated question, Mr. Davis?

8 A This report was prepared by other than the  
9 Department personnel.

10 Q My question is, you as the sponsor of this exhibit,  
11 do you concur in that exhibit in the conclusions that are  
12 reached?

13 A And I said, I'm trying to review just what I did  
14 say to answer that question accurately.

15 I concur that the load carrying capacities of the  
16 structural components have been reduced from those of the  
17 original construction. I concur that there is localized  
18 deteriorations caused by lack of preventative maintenance.  
19 I do not -- I did not perform the inspection nor was it  
20 performed under my direction and, therefore, cannot verify  
21 the inventory operating and safe load conditions of the  
22 structure.

23 I concur that the underside of the deck exhibits fine  
24 cracks and leaching throughout, with evidence of moisture  
25 seepage thru the deck; that the aforesaid situation is more  
prominent below each curbline where numerous large spalls in

1 the underside of the deck occur. That the structure has no  
2 as-built thru-the-deck drainage system.

3 MR. DONOVAN: Your Honor, I'm going to interpose at  
4 this time. This is an exhibit sponsored by Mr. Davis. He  
5 was asked by his counsel whether or not he had anything to  
6 add to his testimony. It seems to me if there were any  
7 exceptions, that would have been put into the Record. He  
8 made no response. Now, we are going through a lengthy  
9 dissertation of what he agrees with in the report. It seems  
10 to me if he is the sponsor he should have reviewed this  
11 report and pointed out in his direct testimony what he took  
12 exception to in that report.

13 MR. MARTIN: Your Honor, the report is offered as an  
14 exhibit to indicate the conditions of the bridge as told to  
15 the Department by its consultant. It is not offered to give  
16 recommendations to the Commission with regard to what should  
17 or should not be done at this crossing. I don't think it is  
18 fair to ask Mr. Davis to simply adopt the consultant's  
19 report in toto when he has already on direct testimony  
20 indicated what the recommendations the Department now has  
21 for the Commission are.

22 JUDGE GESOFF: Well, I agree with that, and it also  
23 seems to me, Mr. Donovan, your question more or less  
24 elicited the response that you're getting.

25 MR. DONOVAN: Be my guest.

1           A (Continuing) I concur that runoff is intended to  
2 follow the roadway profile curblines and then flow towards  
3 the ends of the span. I concur that the -- with the  
4 findings also that the fascia stringers and floorbeam truss  
5 connections typically exhibit hollow gunite encasing.

6           JUDGE GESOFF: Mr. Davis, to speed this a long, do you  
7 agree in toto with the 1, 2, 3, 4 and 8? Just say so rather  
8 than repeating each one.

9           THE WITNESS: Yes, sir.

10          BY MR. DONOVAN:

11           Q What page are you reading from, Mr. Davis?

12           A Nineteen at this point in time.

13           JUDGE GESOFF: Inclusions, the first page.

14           A I concur with No. 2 in toto. I concur with No. 3  
15 in toto. I concur with No. 4 in toto. I concur with No. 5  
16 in toto. I concur with No. 6 in toto. I concur with No. 7  
17 in toto as of the inspection date prior to July 1982.

18           JUDGE GESOFF: And what does that mean?

19           THE WITNESS: That means since that date -- I'm pretty  
20 sure that my previous testimony did state that there has  
21 been some loss of <sup>section</sup> -sunction to the structural members. And  
22 No. 8 I agree in the same respect, that since prior to the  
23 1982 inspection, the 1 inch width gap in the southwest  
24 wingwall is 1 inch wide indicating possible lateral movement  
25 which was from -- which was initially one quarter inch. It

1 is possible that also that 1 inch has further widened.

2 And you were asking me about recommendations, too,  
3 sir. Do you want me to go through those, also?

4 Q No, I'll ask you some questions. You are  
5 familiar, are you not, Mr. Davis, with the original design  
6 construction of this bridge?

7 A Generally.

8 Q Do you know what the original design was for the  
9 drainage from this structure?

10 A The water was to travel from the center line  
11 towards the curbline and then from -- along the curb off of  
12 the structure.

13 Q And that would be to either end of the structure;  
14 is that correct?

15 A Yes, sir, that's correct. This structure is part  
16 of a vertical clearance, it is on a crest part of a vertical  
17 <sup>CURVE</sup> curb.

18 Q Did you make any determination in your inspection  
19 of this structure of how many layers of asphalt concrete  
20 have been applied to the deck of this structure since its  
21 construction?

22 A We have not.

23 Q Let's assume that over a period of years there had  
24 been successive layers of asphalt concrete to the point  
25 where the surface water flows from the roadway surface and

1 was not caught by the curbs on the structure and instead  
2 flowed not only to either end but also off the sides, would  
3 that have accelerated the deterioration in the sidewalk area  
4 and on the lower side of the structure?

5 MR. MARTIN: Your Honor, I would like to interpose an  
6 objection. Hypotheticals have no foundation in the  
7 testimony already given.

8 JUDGE GESOFF: It has no foundation.

9 MR. MARTIN: In testimony so far given.

10 MR. DONOVAN: Judgment in the foundation, Your Honor,  
11 is found on page 19 of the consultant's report in which it  
12 says that there have been, since the original construction,  
13 addition of successive bituminous overlays to the deck.

14 JUDGE GESOFF: Where is this?

15 THE WITNESS: It is on --

16 MR. DONOVAN: It is on page 19 first paragraph.

17 THE WITNESS: No. 1. The last sentence.

18 JUDGE GESOFF: No. 1, last sentence doesn't say that.  
19 Where does it say additional bituminous layers?

20 MR. DONOVAN: On page 19.

21 JUDGE GESOFF: That's enough for me. Overruled.

22 A If the structure had been properly maintained,  
23 water that ran anywhere over the concrete surfaces would not  
24 have penetrated and would have not caused any significant  
25 damage.

1           Q If the bituminous overlays had not been placed,  
2 would the drainage design of the structure have frustrated  
3 the design?

4           A Yes, sir.

5           Q Now, if the successive bituminous overlays on the  
6 deck had increased to a point where it exceeded or was even  
7 with the curbs that were designed to carry the water to  
8 either end, would that have in effect frustrated the design  
9 drainage? Let me -- if you don't understand the question --

10          JUDGE GESOFF: Well, that seems to me that you can  
11 answer that with a yes or no.

12          A I have a real hard problem with the word  
13 frustrated.

14          Q Would it -- let me put it very simply. Would it  
15 have prevented the drainage from flowing off in the manner  
16 in which it was designed?

17          A I already answered that in the affirmative.

18          Q Now, I take it you are familiar with the 1939  
19 Order?

20          A Generally.

21          Q Okay. And do you recall that this bridge was  
22 constructed in 1939 with Federal funds; is that correct?

23          JUDGE GESOFF: Do you say in reference to the Order?

24          A Yes, sir, I think I found it. Yes, sir.

25          Q Now, isn't it a fact, Mr. Davis, that the

1 Department of Highways agreed to construct this structure  
2 provided "Federal funds" could be utilized?

3 A That's correct.

4 Q Now, I take it that the design of this structure  
5 met all of the necessary Federal and State Highway design  
6 criteria in existence in 1939; is that correct?

7 A Yes, sir. The plans were then approved by the  
8 Pennsylvania Public Utility Commission also.

9 Q Do you know, Mr. Davis, whether or not a  
10 reconstruction of this bridge or a rehabilitation of this  
11 bridge would be eligible for Federal funding?

12 A Yes, sir. As I have stated in answer to my -- to  
13 Question No. 9, the Commission interviewed --

14 Q So the answer is, yes?

15 A Yes.

16 Q When you say that the funds are available, are they  
17 available for rehabilitation?

18 A Yes, sir.

19 Q And they are also available for reconstruction?

20 A If the Department first requested Federal  
21 authorization and the Federal Government In Turn did  
22 authorize the expenditures of funds after it had reviewed  
23 the proposal that would be submitted for rehabilitation or  
24 reconstruction, the Federal Government would agree to help  
25 to defray the cost.

1 Q Now, on page 2 of the consultants's report, in the  
2 second paragraph there is a statement that there is a lack  
3 of adequate drainage system for the bridge.

4 A You say on page 2?

5 Q Yes, sir. Under the heading <sup>FINDINGS</sup> Summary of Finance.

6 JUDGE GESOFF: Second paragraph, second sentence.

7 A Yes, the first -- the second sentence says --

8 JUDGE GESOFF: You don't have to repeat it. Just say  
9 yes or no. Just say yes or no.

10 A Yes. I'm sorry, sir.

11 Q Now, I refer you again to page 10 of that same  
12 report, their inspection of findings, and in particular  
13 paragraph B, and in the second sentence of paragraph B the  
14 consultant observed that, "No as-designed thru-the-deck  
15 drainage system exists". Is that correct?

16 A That's correct.

17 Q Now, the consultant concluded that the design load  
18 capacity of this structure is not being attained today; is  
19 that correct?

20 A That's correct.

21 Q Would the original design as constructed --  
22 structure meet the load carrying criteria standard of  
23 today?

24 THE WITNESS: Your Honor, that can't be answered with  
25 a simple yes or no.

1 JUDGE GESOFF: Well, then explain your answer.

2 A Okay. If we were to design a new structure, we  
3 would use criteria today considerably higher than was used  
4 at that time this structure was designed. We are using and  
5 will continue to use structures throughout the state where  
6 the design criteria -- when the structures were built, are  
7 not as -- do not permit the structures to carry as much  
8 loading as was currently designed for them. And, therefore,  
9 if the structure had been adequately maintained, we would  
10 have not had to put or suggested recommending putting the  
11 29-ton-load carrying capacity on it.

12 Q May I direct your attention, Mr. Davis, to page 19  
13 of the consultant's report, and particularly to the second  
14 sentence on that page in which the consultant said, "The  
15 load carrying capacities of the structural components have  
16 been reduced from those of original construction with the  
17 addition of successive bituminous overlays to the deck".

18 Now, isn't it true, Mr. Davis, that the addition of  
19 layers of asphalt, particularly concrete, on the deck of  
20 this bridge, increases the dead load of the bridge?

21 A That's correct, it does.

22 Q And would you explain for us what dead load means  
23 in that context?

24 A It means components of the structure weighing a  
25 certain amount has -- being -- existing rather than live

1 loading which involves moving vehicles.

2 Q And isn't it true when you increase the dead load  
3 you on the other hand decrease the live-load capacity?

4 A Yes, sir.

5 Q Now, you mentioned in your prepared testimony, I  
6 believe, that there were no barriers to prevent vehicles  
7 from striking the superstructure; is that correct?

8 A Yes, sir, that was even back in the days when the  
9 structure was built. The only thing that is different was  
10 -- the only thing existing, probably like a 6 inch curb and  
11 that in itself could have easily been mounted by a vehicle  
12 and obstructed the structural members.

13 Q So if there were, say, 8 inches of bituminous added  
14 to the deck, that would have exceeded the height of the six  
15 inch curb; isn't that correct?

16 A Yes, that's correct.

17 Q Now, with respect to barriers to prevent vehicles  
18 from tying the superstructure, other than the existence of  
19 the curb, the design of the structure did not contemplate  
20 such barriers, did they?

21 A No, it did not.

22 Q You mentioned an average daily traffic control of  
23 8394, when was that taken?

24 A Recently.

25 Q And that would be then since the construction of

1 the new bypass that you referred to?

2 A Yes, sir.

3 Q Now, you mentioned that this bridge does not meet  
4 the 1982 Pennsylvania Vehicle Code load carrying standard of  
5 40 ton. When did that come into effect?

6 A Our bridge engineers here can probably give you a  
7 lot more information on that regard.

8 Q Was it subsequent to the construction of the bridge  
9 in 1939?

10 A Yes.

11 Q Do you know a gentlemen with the Department of  
12 Transportation by the name of Johnny Matthews?

13 A Yes, sir.

14 Q And do you work and have liaison with Mr. Matthews?

15 A Yes, sir.

16 Q I take it that he is a competent civil engineer?

17 A Yes, sir.

18 Q And what is his capacity with the Bureau?

19 A He is our District Engineer.

20 Q District?

21 A District 9 Hollidaysburg.

22 Q And I take it that you think highly of his  
23 reputation as a bridge engineer?

24 A I think highly of him as an engineer. Not  
25 specifically for the bridges.

1 Q Are you familiar, Mr. Davis, with a letter that was  
2 written by Johnny Matthews to J. T. Sullivan, Chief Engineer  
3 Designing Construction of Consolidated Rail Corporation on  
4 December 9, 1983?

5 A Yes, I am.

6 Q And did he not -- did Mr. Matthews not state in  
7 that letter that, "For design and economical evaluation  
8 purposes, the life expectancy of a concrete deck ranges from  
9 40 to 50 years". Did he say that?

10 A Yes, sir.

11 Q And if my calculations are correct, if this  
12 structure were built in 1939, that deck is now 46 years old;  
13 is that correct?

14 A That's correct, sir.

15 MR. DONOVAN: I have nothing further.

16 JUDGE GESOFF: Any redirect?

17 MR. MARTIN: Just a couple of questions, Your Honor.

18 REDIRECT EXAMINATION

19 BY MR. MARTIN:

20 Q Mr. Davis, you were speaking on cross-examination  
21 about the production of load carrying capacity of this  
22 structure. Can all of the reductions of the load carrying  
23 capacity be contributed solely to the successive layers of  
24 asphalt which may have been put on?

25 A Not even significantly.

1 Q Let me refer you again to a letter dated December  
2 29th from J. E. Matthews, District Engineer to J. T.  
3 Sullivan. It is the same letter to which Mr. Donovan  
4 referred. Do you recall the sentence? You simply read part  
5 of the sentence you read for Mr. Donovan.

6 A Yes, sir, I do.

7 Q Would you read that entire sentence for us.

8 A "For design and economical evaluation purposes, the  
9 life expectancy of a concrete deck ranges from 40 to 50  
10 years and the life expectancy of the overall structure with  
11 proper maintenance (ranges from 50 to 100 years)".

12 Q And how about the sentence after that. Would you  
13 read that, please.

14 A "Furthermore, our records do not indicate that the  
15 concrete deck or any part of the structure has ever had  
16 repair work done other than for the Department's repairs to  
17 the roadway paving on the structure".

18 MR. MARTIN: If, Your Honor, please, if I may have a  
19 minute. I would like to have this letter marked PennDOT  
20 Exhibit No. 3.

21 JUDGE GESOFF: Yes.

22 MR. DONOVAN: Do you have -- is this your final copy  
23 of the letter?

24 MR. MARTIN: This is a copy of the final letter. This  
25 is not the original copy.

1 MR. DONOVAN: I have, Your Honor, the original copy.  
2 I think that would be -- I think this would be the best  
3 evidence.

4 JUDGE GESOFF: We will mark that as PennDOT Exhibit 3.

5 MR. DONOVAN: Your Honor, could we have copies of this  
6 letter made for the Record?

7 JUDGE GESOFF: Yes. They can be made. We can mark  
8 the exhibit and if you can't make copies today you can  
9 submit them at a later date.

10 MR. MARTIN: Your Honor, I think I would like to mark  
11 Exhibit 3 and 4. There are handwritten notes on the bottom  
12 that may be objectionable, but I perhaps would like to  
13 introduce it.

14 JUDGE GESOFF: Let's deal with Exhibit 3. Why don't  
15 you give that to the reporter. All right. 3, mark it, give  
16 back to her. You can make a copy of it and then send her  
17 the original and one copy, and myself and the Court wants a  
18 copy of it. That's Exhibit 3. Now, how about Exhibit 4?

19 (PennDOT Exhibit No. 3 was produced and marked for  
20 identification.)

21 MR. MARTIN: Okay. Your Honor, I would like marked as  
22 PennDOT Exhibit 4 the file copy from PennDOT's files of the  
23 same letter.

24 MR. DONOVAN: May we have an offer on this?

25 JUDGE GESOFF: Well, there is some notation at the

1 bottom which is why you're putting in the file copy; is that  
2 correct?

3 MR. MARTIN: Yes.

4 JUDGE GESOFF: What is the notation and whose is it  
5 by?

6 MR. DONOVAN: Who is it by first?

7 MR. MARTIN: The notation I understand is by Tex  
8 Walker who is the head of our grade crossing in Harrisburg.  
9 It refers to a telephone conversation which was related to  
10 the --

11 MR. DONOVAN: Your Honor, I'm going to object unless  
12 Mr. Walker is here for cross-examination.

13 JUDGE GESOFF: I'm going to have to sustain that  
14 objection. That's clearly hearsay unless there is another  
15 party to the telephone conversation present in the room.

16 MR. MARTIN: Your Honor, Mr. Walker is no longer with  
17 the Department and I believe this may qualify as a -- excuse  
18 me, Your Honor, can we go off the Record?

19 JUDGE GESOFF: Off the Record.

20 (Whereupon, a brief discussion was  
21 held off the record).

22  
23 JUDGE GESOFF: Back on the Record.

24 MR. MARTIN: Exhibit 4 has a note on the bottom, the  
25 offer of which is Mr. Reese who is here and available for

1 cross-examination.

2 (PennDOT Exhibit No. 4 was produced and marked for  
3 identification.)

4 JUDGE GESOFF: All right. Why don't we wait. Are you  
5 going to put him on as a witness?

6 MR. MARTIN: I wasn't planning to.

7 JUDGE GESOFF: You are now. Let's wait. Wait to  
8 offer it until you put him on.

9 MR. HERSKOVITZ: Can Staff have a copy so they know  
10 what's going on?

11 JUDGE GESOFF: So Exhibit 4 is temporarily withdrawn  
12 until you have a witness response.

13 MR. MARTIN: Very well, Your Honor.

14 JUDGE GESOFF: Anything more? Redirect?

15 MR. MARTIN: Nothing further, Your Honor.

16 JUDGE GESOFF: Any recross, Mr. Herskovitz? Any  
17 recross?

18 MR. HERSKOVITZ: No.

19 JUDGE GESOFF: Very well. Mr. Gibboney, any recross?

20 MR. GIBBONEY: None.

21 JUDGE GESOFF: Mr. Donovan, any recross?

22 MR. DONOVAN: Just one, Your Honor.

23 RE-CROSS-EXAMINATION

24 BY MR. DONOVAN:

25 Q And, again, I invite your attention, Mr. Davis, to

1 the consultant report, particularly page 3, and I ask you if  
2 the consultant didn't find that, "The bituminous overlays  
3 have increased the dead load on the structure  
4 significantly"?

5 JUDGE GESOFF: Second paragraph, first sentence, and  
6 I --

7 Q And he goes on, plus or minus 20 percent, and  
8 should be removed down to the top of the concrete deck;  
9 isn't that correct?

10 A That's what it says and again I do not agree with  
11 that.

12 Q I see.

13 MR. DONOVAN: I have nothing further.

14 JUDGE GESOFF: You're excused, sir. Thank you.

15 (Witness excused.)

16 JUDGE GESOFF: Want to move the exhibit?

17 MR. MARTIN: Want me to?

18 JUDGE GESOFF: Why don't you move 1 through 3.

19 MR. MARTIN: Very well, Your Honor. I move the  
20 admission of PennDOT Exhibits 1 through 3 at this time.

21 JUDGE GESOFF: Any objection? They are admitted.

22 (PennDOT Exhibit Nos. 1 through 3 were admitted into  
23 evidence.)

24 JUDGE GESOFF: Before you call your next witness, let  
25 me just note for the Record that on June 19th I received a

1 copy of a letter from counsel for Bell Telephone, copies of  
2 which were sent to all parties, indicating that Bell has  
3 some facilities on the bridge and that they will be unable  
4 to ascertain -- agree to, if it be affected, until the  
5 Commission issues an Order requiring work to be done. That  
6 Bell has no objection to and will abide by the Commission's  
7 determination on the scope of work to be done. That Bell  
8 will perform a relocation of its facilities at its initial  
9 cost and expense subject to a cost allocation hearing later  
10 stated in the proceeding.

11 MR. MARTIN: Your Honor, at this time I would like to  
12 call Mr. Reese.

13 MR. DONOVAN: Your Honor, in the interest of time, I  
14 would have no objection to this file copy of the letter  
15 going into the Record. Or you have more than just the  
16 letter, is that what you wanted?

17 MR. MARTIN: Just the letter.

18 MR. DONOVAN: I have no objection.

19 JUDGE GESOFF: All right. It may be.

20 MR. DONOVAN: As long as Mr. Reese is here to support  
21 it.

22 JUDGE GESOFF: Well, they have to be supported.

23 Please, Mr. Reese, come.

24 Whereupon,

25 Lewis S. Reese

1 having been duly sworn, testified as follows:

2 DIRECT EXAMINATION

3 BY MR. MARTIN:

4 Q Mr. Reese, I show you what has been marked as  
5 PennDOT Exhibit No. 4. I would like to draw your attention  
6 to the handwritten note at the bottom of that letter. Could  
7 you verify for the Record please whether or not that is the  
8 note that you offered?

9 A Yes, that is.

10 JUDGE GESOFF: Who is Tex Walker?

11 THE WITNESS: Tex Walker is Conrail's official in  
12 Philadelphia.

13 JUDGE GESOFF: Do you know what his capacity is?

14 THE WITNESS: Not officially.

15 JUDGE GESOFF: Does he work on the crossing?

16 THE WITNESS: He's one of the railroad engineers. Mr.  
17 Donovan I believe could --

18 JUDGE GESOFF: He's a crossing engineer?

19 MR. DONOVAN: Yes, sir.

20 JUDGE GESOFF: Now, who is Kunselman?

21 THE WITNESS: Mr. Kunselman is our District Bridge  
22 Engineer.

23 JUDGE GESOFF: Okay. Who is the other person on the  
24 three-way conference?

25 THE WITNESS: Myself.

1 JUDGE GESOFF: All right. Anything further?

2 MR. MARTIN: That's official to authenticate as an  
3 exhibit. I move its admission, Your Honor.

4 JUDGE GESOFF: Any objection?

5 MR. DONOVAN: Just a question.

6 RE-CROSS-EXAMINATION

7 BY MR. DONOVAN:

8 Q Did you state that you didn't know what Mr.  
9 Walker's title was?

10 A I do not know at this particular time what his  
11 official title is.

12 Q What Department is he in, Mr. Reese?

13 A He is in the Engineering Department of J. T.  
14 Sullivan.

15 Q And Mr. Sullivan is Chief Engineer Designing  
16 Constructor; is that right?

17 A That's correct.

18 Q And do you know how many Assistant Chief Engineers  
19 are under Mr. Sullivan?

20 A I was not aware that there were any Chief Engineers  
21 other than Mr. Sullivan.

22 Q Assistant Chief Engineers?

23 A Excuse me, I misunderstood you. No, I do not.

24 Q If I told you that there were three Assistant Chief  
25 Engineers under Mr. Sullivan, and that Mr. Walker is two or

1 three steps down from one of those Assistant Chief  
2 Engineers, would you say I was mistaken?

3 A I would not be in a position to dispute your  
4 statement.

5 MR. DONOVAN: Okay. I have nothing further.

6 JUDGE GESOFF: Any redirect?

7 MR. MARTIN: Yes, Your Honor.

8 REDIRECT EXAMINATION

9 BY MR. MARTIN:

10 Q Mr. Reese, in your capacity -- excuse me. What is  
11 your capacity with the Department of Transportation?

12 A I am District Grade Crossing Liaison with District  
13 9 in Hollidaysburg.

14 Q And in that capacity do you have dealings with Mr.  
15 Walker of Conrail on a regular basis?

16 A On occasion.

17 Q Do you believe him to have the authority to confer  
18 with you on grade crossing problems?

19 A Yes, I do.

20 MR. MARTIN: Nothing further, Your Honor.

21 MR. DONOVAN: Nothing.

22 MR. MARTIN: I would move for admission of Exhibit No.

23 4.

24 JUDGE GESOFF: Any objection?

25 MR. DONOVAN: No.

1 JUDGE GESOFF: It is admitted. Thank you, sir, you're  
2 excused.

3 (Witness excused.)

4 (PennDOT Exhibit No. 4 was admitted into evidence.)

5 (Whereupon, a brief discussion was  
6 held off the record).

7 Whereupon,

8 Winford R. Addison

9 having been duly sworn, testified as follows:

10 DIRECT EXAMINATION

11 BY MR. DONOVAN:

12 Q Would you please state your full name for the  
13 Record?

14 A Winter R. Addison.

15 Q And what is your business address?

16 A 15 North 32nd Street, Philadelphia, Pennsylvania.

17 Q And are you employed by Consolidated Rail  
18 Corporation?

19 A I am.

20 Q And in what capacity?

21 A Senior Civil Engineer.

22 Q And are you a Grade Crossing Engineer?

23 A Yes, I am.

24 Q And what are your duties presently as Conrail's  
25 Senior Civil Engineer?

1           A   Coordinate and supervise projects where highways  
2 cross the railroads and Conrail railroads in Western  
3 Pennsylvania.

4           Q   And are you authorized to testify for Conrail this  
5 morning?

6           A   I am.

7           Q   Now, I refer you to the Commission's questions and  
8 procedures that were issued in this case, Mr. Addison, and  
9 particularly the first question posed to Conrail, No. 10.  
10 And I ask you to state the exact corporate name of the owner  
11 and operator of the line of railroad that runs under this  
12 bridge?

13          A   Consolidated Rail Corporation.

14          Q   And how long has the Consolidated Rail Corporation  
15 owned that particular line of railroad?

16          A   Since April the 1st, 1976.

17          Q   Now, how many tracks presently run under the  
18 structure?

19          A   There are three mainline tracks under the existing  
20 structure.

21          Q   Now, you were present during the testimony of Mr.  
22 Davis, and he described the type of structure that we were  
23 talking about. Do you generally agree with this type of  
24 construction that he described?

25          A   Yes, I do.

1 Q And he also stated that it was constructed  
2 approximately 1939 by the Department of Highways, do you  
3 concur in that?

4 A Yes, sir.

5 Q And do you know of any outstanding Order of the  
6 Public Utility Commission or any agreement which obligated  
7 Conrail to maintain any portion of this structure?

8 A No, sir, I do not.

9 Q Now, the Question No. 14, Mr. Addison, requests,  
10 "Conrail shall submit testimony indicating whether, in its  
11 opinion, the existing crossing is adequate in strength and  
12 physical dimension to accommodate safely the class and  
13 volume of traffic normally using the highway at this  
14 location"?

15 A In Conrail's opinion, the existing structures are  
16 adequate in strength and physical dimensions to accommodate  
17 the present traffic.

18 Q Now, you have had an opportunity to review the  
19 consultant's report that was introduced as the Department's  
20 exhibit this morning; is that correct?

21 A Yes, I have.

22 Q And does that report indicate that the load  
23 carrying capacity presently shows a capacity of 29 tons?

24 A Yes, it does.

25 Q And does that report indicate to you that were it

1 not for the successive layers of bituminous paving on the  
2 structure that the structure would be capable of carrying  
3 the design load?

4 A Yes, it does.

5 Q Now, Question No. 15 requests testimony with regard  
6 to who Conrail has prepared any detailed inspections or has  
7 prepared any engineering studies, estimates or construction  
8 plans for the alteration of the structure?

9 A Conrail has prepared no plans, engineer studies,  
10 estimates of construction cost for the allocation of the  
11 crossing.

12 Q Certain recommendations were made this morning with  
13 regard to repairs or further studies. What portion of the  
14 work contemplated by that recommendation would Conrail agree  
15 to perform?

16 A Conrail will not agree to perform any of the work,  
17 will not agree to assume any of the -- portion of the cost  
18 and will not agree to maintain any portion of the crossing  
19 in the future, in the event that complaint is sustained --

20 Q Now, you heard testimony with regard to the  
21 original design drainage function for this bridge this  
22 morning, did you not?

23 A Yes, I did.

24 Q Now, is the original design for that drainage  
25 functioning today?

1 A No, it is not.

2 Q And would you state for us the reason why it is  
3 not?

4 A Well, one of the reasons is the excessive -- the  
5 layers of wiring surface built up on the concrete deck to --  
6 higher than the curblin<sup>ed</sup>e. That the water spills over under  
7 the side instead of it running off the hinge<sup>ends</sup> like it was  
8 originally designed to do.

9 Q And what effect has that had on the substructure of  
10 the bridge?

11 A Well, that has sped up the deterioration of the  
12 deck.

13 Q Has it allowed water to permeate the deck?

14 A Yes, it has.

15 Q Now, did you observe any expansion joints on this  
16 structure?

17 A Yes.

18 Q Were you able to view the expansion joints from the  
19 upper side of the structure?

20 A Yes.

21 Q And what did you observe in looking at those  
22 expansion joints?

23 A Well, the expansion joints had been paved over when  
24 the wearing surface had been put on top of the bridge.

25 Q What?

1           A   When the wearing surface had been placed on top of  
2 the bridge.

3           JUDGE GESOFF:   You said the expansion joints are --

4           MR. DONOVAN:   Paved over I think you said.

5           THE WITNESS:   Yes.

6           JUDGE GESOFF:   Oh, all right.

7 BY MR. DONOVAN:

8           Q   And did it appear that excessive layers of  
9 bituminous had been placed each time over the expansion  
10 joints?

11          A   Yes.

12          Q   Who is Tex Walker that is referred to in testimony  
13 this morning?

14          A   He is principal Engineer of Construction.

15          Q   And who is his immediate supervisor?

16          A   Donald S. Taylor.

17          Q   And what is his capacity?

18          A   Assistant Chief Engineer.

19          Q   Who is Donald S. Taylor's immediate superior?

20          A   J. T. Sullivan.

21          Q   And what is he?

22          A   Chief Engineer Designer of Construction.

23          Q   And who is Mr. Sullivan's immediate superior?

24          A   B. J. Gordan.

25          Q   And what is his capacity?

1 A Chief Engineer Officer.

2 MR. MARTIN: Do we need to go entirely up the  
3 corporate ladder here?

4 MR. DONOVAN: I would like it. If you want to offer  
5 -- I'm attempting to show Mr. Walker is not an official. He  
6 does not speak for management of Consolidated Rail  
7 Corporation.

8 MR. MARTIN: Very well, Your Honor.

9 MR. DONOVAN: So how far did we get?

10 JUDGE GESOFF: Chief Engineer.

11 A Chief Engineer, Mr. B. J.

12 Q And who is Mr. Gordan's immediate superior?

13 A R. B. Hasselman.

14 Q And what is his capacity?

15 A He is Vice President of Operation. That's the end  
16 of it.

17 Q Did you miss anybody in there like Mr. Owens?

18 A Yes, I did. Mr. Owens.

19 Q And Mr. Owens is the immediate superior of whom?

20 A Mr. Gordan.

21 Q And who is his immediate superior?

22 A Mr. Hasselman.

23 Q And who is Mr. Hasselman?

24 A He is the Vice President of Operations.

25 Q And he is one of the many Vice Presidents at

1 Consolidated Rail Corporation?

2 A Yes, he is.

3 Q Now, do you know whether or not Mr. Walker's  
4 position with Consolidated Rail Corporation clothed<sup>d</sup> him with  
5 authority to speak for management of Consolidated Rail  
6 Corporation?

7 A Yes, sir, it does.

8 MR. DONOVAN: I have nothing further.

9 JUDGE GESOFF: Before any cross, let me ask you the  
10 function of the expansion joint?

11 THE WITNESS: Well, the expansion joint is on the  
12 bridge deck and allows for the expansion construction of the  
13 bridge and transfers the <sup>weight?</sup> wheel from the bridge structure to  
14 the bridge abutment.

15 JUDGE GESOFF: Okay. Is that a function of weight or  
16 of heat extreme?

17 THE WITNESS: Heat. Strictly heat.

18 JUDGE GESOFF: Temperature extreme?

19 THE WITNESS: Temperataure extreme, yes.

20 JUDGE GESOFF: They are paved over. They can't  
21 operate the way they are designed?

22 THE WITNESS: Well --

23 JUDGE GESOFF: I might be introducing some of your  
24 cross. If I am --

25 MR. MARTIN: Go ahead, Your Honor.

1 THE WITNESS: They have a -- tend to crack.

2 JUDGE GESOFF: Expansion -- asphalt paved over, that  
3 moves -- that leaves a crack in that. Ice can get in in the  
4 winter time and form small potholes.

5 JUDGE GESOFF: In the surface?

6 THE WITNESS: In the surface.

7 JUDGE GESOFF: Does it have any effect on expansion  
8 joints and their operation?

9 THE WITNESS: I don't think it has an adverse effect.  
10 It's more --

11 JUDGE GESOFF: If the roadways weren't paved over at  
12 the expansion joints, would that mean that there would be  
13 gaps?

14 THE WITNESS: Well, whenever you repave or resurface a  
15 highway, the adjustment evasion should be made at that time  
16 to the expansion joints.

17 JUDGE GESOFF: I see. So that would eliminate the  
18 problem of holes?

19 THE WITNESS: Ice in the cracks and holes, yes, it  
20 would.

21 JUDGE GESOFF: Okay. Mr. Herskovitz, do you have any  
22 cross-examination?

23 CROSS-EXAMINATION

24 BY MR. HERSKOVITZ:

25 Q Did you say these were mainline tracks below this

1 bridge?

2 A Yes, they are. The mainline tracks, Pittsburgh  
3 mainline.

4 Q The existence of this bridge would be much safer  
5 for the railroad operations than at that grade crossing at  
6 this location; is that not a fair statement?

7 A Yes, it would be safer.

8 MR. HERSKOVITZ: That's all I have. Thank you.

9 CROSS-EXAMINATION

10 BY MR. MARTIN:

11 Q Mr. Addison, back to the expansion joints. Do I  
12 understand correctly that you stated that the paving over of  
13 the expansion joints with asphalt does not interfere with  
14 the functions of those joints?

15 A It would not adversely.

16 Q Okay. Then the problem was the asphalt would then  
17 crack and allow water to seep in that, is that the problem?

18 A Yes, it would.

19 Q If the expansion joints were not paved over at all,  
20 wouldn't water seep into that anyway at a much greater rate?

21 A Well, some water would seep in but most of the  
22 water runs off to the side and down the gutters.

23 Q Mr. Addison, I'm sorry, I forget you stated your  
24 position with Conrail, is what?

25 A Senior Civil Engineer.

1 Q And where exactly do you fit in the steps of the  
2 ladder of authority you described?

3 A I'm near the bottom.

4 Q And are you vested with authority to speak for the  
5 Department of Penn Central to describe and resolve problems  
6 or even discuss problems with crossings of state highways  
7 and Conrail lines?

8 A Yes, I am.

9 Q And are you stating that Mr. Walker has less  
10 authority than you?

11 A No, sir.

12 Q Can you give me any reason why the Department of  
13 Penn Central wouldn't rely on representations made by you or  
14 Mr. Walker to them?

15 MR. DONOVAN: I object. How can he speak for  
16 motivation of the Department employees.

17 JUDGE GESOFF: I'll sustain it.

18 BY MR. MARTIN:

19 Q Mr. Addison, I would refer you to PennDOT Exhibit  
20 No. 1 which is the 1940 -- I forget the date. Whatever the  
21 date is.

22 JUDGE GESOFF: '39.

23 BY MR. MARTIN:

24 Q '39 Order of the Commission. Have you seen that  
25 Order, Mr. Addison?

1           A Yes, sir, I have.

2           Q Do you know whether that Order assigns maintenance  
3 responsibility for the substructure and superstructure of  
4 the bridge?

5           A Yes, it does.

6           Q And could you tell me who assigns that maintenance  
7 responsibility, too?

8           A Pennsylvania Railroad.

9           Q And Pennsylvania Railroad is a predecessor and  
10 interest to this line?

11           , MR. DONOVAN: I object to that legal conclusion, Your  
12 Honor. If he asked the question, did the Penn Central  
13 trustees under the direction of Congress, by reason of the  
14 Original Railway Reorganization Act of 1973, convey a line  
15 of railroad to Conrail on April 1, 1976, I have no  
16 objection. If he is asking this man for a legal conclusion  
17 as to whether or not Conrail is a successor in interest, I  
18 object.

19           JUDGE GESOFF: Sustained.

20           MR. MARTIN: Then let me ask the question without  
21 repeating it the way Mr. Donovan stated it.

22           JUDGE GESOFF: Do you want to ask the question posed  
23 by Mr. Donovan?

24           MR. DONOVAN: If I might, did the trustess of Penn  
25 Central Transportation Company convey the line of railroad

1 under this bridge to Consolidated Rail Corporation pursuant  
2 to the Original Railway Reorganization Act of 1973.

3 THE WITNESS: Yes, it did.

4 BY MR. MARTIN:

5 Q Mr. Addison, Penn Central Railroad owned this line  
6 prior to 1976?

7 A Yes, they did.

8 Q Does Conrail own it now?

9 A Conrail owns the line. Yes, it does.

10 Q Okay. Mr. Addison, you stated that there were  
11 problems with water permeating the deck. Assuming that the  
12 original design -- original design drainage was not  
13 functioning, was there nothing that Conrail could have done  
14 in the ten years that they have owned this bridge to  
15 minimize or perhaps reduce any adverse effect they might  
16 have had?

17 MR. DONOVAN: Your Honor, that I think the question --  
18 I object. I think the question presupposes that Conrail has  
19 an obligation. Obviously, I think that any anybody could  
20 have taken action to forestall the --

21 MR. MARTIN: Let me rephrase the question.

22 BY MR. MARTIN:

23 Q In your experience as a bridge engineer, is there  
24 some treatment or other repair or maintenance that could  
25 have been affected with respect to the superstructure of



1 having been duly sworn, testified as follows:

2 DIRECT EXAMINATION

3 BY MR. GIBBONEY:

4 Q State your name, please?

5 A My name is Leonard Facciani.

6 Q Your business address is?

7 A 221 Station Street, Johnstown, Pennsylvania.

8 JUDGE GESOFF: Spell your last name.

9 THE WITNESS: F-a-c-c-i-a-n-i.

10 BY MR. GIBBONEY:

11 Q What is your occupation, Mr. Facciani?

12 A I am a Consultanting Engineer.

13 Q Are you a Licensed Professional Engineer within the  
14 Commonwealth of Pennsylvania?

15 A Yes.

16 Q Are you the Township Engineer for the Township of  
17 Synder?

18 A Yes.

19 Q Have you been authorized by the Board of  
20 Supervisors of Synder Township to give testimony here today?

21 A Yes.

22 Q Referring to Question 17 of the questions and  
23 procedures for this hearing, "Township of Synder shall  
24 submit testimony indicating whether, in its opinion, the  
25 involved railroad-highway crossing should be altered to

1 effectuate the prevention of accidents and promote the  
2 safety of public".

3 Would you address that issue, please?

4 A The Township is not getting involved in the  
5 operation of the bridge. They are not -- they felt that  
6 bridge is not within the jurisdiction and have not concerned  
7 themselves with it. They have not budgeted any monies for  
8 the maintenance of the bridge.

9 JUDGE GESOFF: Wait a minute. Just answer the  
10 question posed. Should the Township, in its opinion, have  
11 the crossing be altered to effectuate the prevention of  
12 accidents to promote the safety of the public?

13 A Well, the Township has never made a study on the  
14 bridge. So, therefore, they are in no position to make any  
15 comments about the bridge.

16 BY MR. GIBBONEY:

17 Q Now, addressing Question No. 18, "Township of  
18 Synder shall submit testimony indicating what portion of the  
19 costs of construction and future maintenance of any  
20 alteration of the crossing ordered by the Commission it will  
21 agree to assume, in the event that the instant complaint is  
22 sustained"?

23 A The Township has never made any studies on the  
24 bridge and, therefore, they cannot make any comments about  
25 future operation or changes on the bridge.

1 JUDGE GESOFF: No, this has to do --

2 Q Is the Township of Synder prepared to agree at this  
3 time to be responsible for or pay any costs in connection  
4 with the maintenance or repair of the bridge?

5 A They are not.

6 MR. GIBBONEY: Thank you.

7 JUDGE GESOFF: Any cross-examination, Mr. Herskovitz?

8 CROSS-EXAMINATION

9 BY MR. HERSKOVITZ:

10 Q Do you agree with -- you were here when Mr. Davis  
11 testified that it is primarily local traffic that uses the  
12 bridge, thru traffic uses the bypass?

13 A I generally agree with that, yes.

14 Q Do you agree with that statement?

15 A Because of the existence of the bypass.

16 MR. HERSKOVITZ: I have no further questions.

17 JUDGE GESOFF: Mr. Martin?

18 MR. MARTIN: Just a couple, Your Honor.

19 CROSS-EXAMINATION

20 BY MR. MARTIN:

21 Q Is there a sidewalk on this bridge? Let me  
22 rephrase that. Should there be a sidewalk on this bridge as  
23 designed, as constructed, discounting the condition it may  
24 be in?

25 A Synder Township has not studied that bridge

1 whatsoever.

2 Q Have you seen the bridge?

3 A I have driven over the bridge.

4 Q Do you recall if it --

5 A I have not made a close inspection of the bridge  
6 whatsoever.

7 JUDGE GESOFF: Have you ever visited the bridge accept  
8 passing over it by a car?

9 THE WITNESS: I just passed over the bridge.

10 JUDGE GESOFF: In a car?

11 THE WITNESS: In a car.

12 JUDGE GESOFF: Ever stopped and get out of the car?

13 THE WITNESS: No, we haven't.

14 JUDGE GESOFF: Not we, you?

15 THE WITNESS: No, I haven't. The Township is just not  
16 -- felt that they were responsible for the maintenance or  
17 any aspect of the bridge.

18 BY MR. MARTIN:

19 Q Insofar as you're speaking for the Township, should  
20 the Commission, in addition to the road repairs that we have  
21 been discussing, should the Commission also order  
22 rehabilitation of the sidewalk; is the Township willing to  
23 participate in any of that cost?

24 A The Township is not willing to participate in any  
25 cost. They have no money budgeted for this bridge and there

1 are just no funds.

2 Q Supposing the Commission denies any cost of  
3 reconstruction and rehabilitation of the sidewalk, would the  
4 Township be willing to bear maintenance responsibilities for  
5 the sidewalk?

6 A In order to address that question properly, I think  
7 the Township would have to take a look at the sidewalk and  
8 get a very brief study and see how they would want to  
9 participate in that matter. As it stands, they are not  
10 interested in participating at all in the problems of the  
11 bridge.

12 Q Do you know whether or not the sidewalk is used by  
13 pedestrians?

14 A I do not know.

15 MR. MARTIN: I have nothing else, Your Honor.

16 JUDGE GESOFF: Mr. Donovan?

17 CROSS-EXAMINATION

18 BY MR. DONOVAN:

19 Q With relation to this bridge, where does the new  
20 bypass intersect with the road carried over this bridge, say  
21 on the westside?

22 A Well, possibly about 1.3 miles.

23 Q Twenty-three miles?

24 A 1.3, around that area.

25 Q Okay. And where does it reintersect this highway,

1 northerly or easterly from the structure?

2 A How far it intersects, the 220? Again at Bald  
3 Eagle. That is roughly seven, eight miles. Well, there is  
4 a span from, I'd say, maybe eleven miles between the -- of  
5 the bypass on 220.

6 Q So that people -- and that 11 miles is entirely  
7 within Synder County?

8 A Within Synder?

9 Q Did -- or Synder Township, excuse me?

10 A No, it isn't.

11 Q It goes into other townships as well?

12 A It goes into Tyrone Borough. First you go into  
13 Synder Township, Tyrone Borough, then Synder Township, then  
14 Bald Eagle.

15 Q And all of these are in Blair County, though?

16 A Yes.

17 Q So that this bridge would serve at least any  
18 residents of that 11-mile area, anybody having business  
19 within that 11-mile area and anybody just going through  
20 within that 11-mile area; is that correct?

21 A That's correct.

22 Q So would you say that those would be residents of  
23 Synder County as well as Tyrone Borough and general citizens  
24 of Blair County?

25 A Yes.

1 MR. DONOVAN: I have nothing further.

2 JUDGE GESOFF: Any redirect?

3 REDIRECT EXAMINATION

4 BY MR. GIBBONEY:

5 Q Mr. Facciani, what's been referred to as the newly  
6 constructed bypass known as U.S. Route 22, does it also  
7 exist --

8 MR. MARTIN: 220.

9 Q 220. I'm sorry. Does it also pass into a portion  
10 of Antis Township, Blair County?

11 A Yes, I believe that cuts a corner of Antis  
12 Township.

13 Q And Antis Township is the Township which enjoins  
14 Synder Township in Blair County?

15 A That's correct.

16 MR. GIBBONEY: That's all.

17 JUDGE GESOFF: Anything else? All right. Thank you,  
18 sir, you're excused.

19 (Witness excused.)

20 JUDGE GESOFF: Mr. Herskovitz?

21 MR. HERSKOVITZ: I call Bill Calder.

22 Whereupon,

23 William B. Calder

24 having been duly sworn, testified as follows:

25 DIRECT EXAMINATION

1 BY MR. HERSKOVITZ:

2 Q Please state your name and business address?

3 A William B. Calder, Box 3265, Harrisburg,  
4 Pennsylvania 17120.

5 Q And by whom are you employed?

6 A By the Public Utility Commission.

7 Q And in what capacity?

8 A As a Bridge Engineer.

9 Q Are you a Professional Engineer in the Commonwealth  
10 of Pennsylvania?

11 A Yes, sir.

12 Q Are you familiar with the structure which is the  
13 subject matter in this proceeding?

14 A Yes, sir.

15 Q Have you been to the crossing?

16 A Yes.

17 Q And you visually inspected the structure?

18 A Yes.

19 Q And you are somewhat familiar with the inspection  
20 report that was admitted as PennDOT Exhibit No. 2 today?

21 A Yes, I reviewed it briefly this morning, and I have  
22 heard the testimony relative to the report.

23 Q Do you agree with the findings that appear on pages  
24 -- I believe that's -- or the inclusions that appear on  
25 pages 19 and 20 of that report and the recommendations that

1 appear on pages 21 and 22 of that report?

2 A Yes, I do.

3 Q What is your opinion as to what is required to  
4 accomplish the work that is recommended?

5 A Well, as the consultant pointed out, we have to  
6 have a deck evaluation to know whether the deck has to be  
7 replaced in its entirety or whether it doesn't, and my own  
8 recommendation would be that some party be directed to make  
9 such a study and prepared after the study -- based on their  
10 findings, prepare a plan for rehabilitation for that deck,  
11 the whole superstructure of the bridge. When I say  
12 superstructure, I mean the deck portion of the  
13 superstructure.

14 Q Are plans necessary for any other repairs besides  
15 the deck portion?

16 A Well, this would probably be -- there maybe some  
17 superficial repairs that would be apparent when they made  
18 this inspection, yes.

19 Q Do you have any recommendation as to what would be  
20 a reasonable time; based on your experience with plans, what  
21 would be a reasonable length of time to prepare such plans  
22 and perform the deck on that?

23 A I think the actual work of the field work and the  
24 work preparing these plans and preparing an estimate would  
25 probably be done within a six-month period.

1 MR. HERSKOVITZ: I have no further questions. Thank  
2 you.

3 JUDGE GESOFF: Mr. Martin?

4 DIRECT EXAMINATION

5 BY MR. MARTIN:

6 Q Mr. Calder, from your inspection and review of this  
7 bridge, did you indicate that there was possible damage to  
8 the substructure?

9 A Substructure being the abutments carrying the  
10 superstructure. I think there is some superficial damage  
11 there and there is some repair work necessary to the  
12 backwalls, yes.

13 Q Do you think there is any possible loss of <sup>e</sup>suction  
14 in the substructure?

15 JUDGE GESOFF: Loss of what?

16 MR. MARTIN: <sup>e</sup>Suction.

17 A Not appreciable; not significant.

18 MR. MARTIN: I have nothing further, Your Honor.

19 JUDGE GESOFF: Mr. Gibboney?

20 MR. GIBBONEY: I have nothing, Your Honor.

21 MR. DONOVAN: I have nothing, Your Honor.

22 JUDGE GESOFF: Do you have any opinion about the  
23 drainage that we seem to be engaged in this proceeding, Mr.  
24 Calder?

25 THE WITNESS: Yes. In my own opinion, and speaking as

1 an engineer, the bridge is designed so that it is at a crest  
2 of vertical curve. Which means that any drainage on that  
3 bridge has got to be directed to the curblines, and then  
4 because of the grade of the bridge itself, it flows off  
5 either end of the bridge and anything that interrupts that  
6 flow is going to add to any deterioration that occurs on  
7 that paving structure. In this case, its true that the over  
8 -- the excessive overlay of macadam has contributed. It's  
9 not the entire reason but it contributes.

10 At the same time, any salt or cinders that normally  
11 find its way on the bridge is washed over to the curb. If  
12 the curbline is not kept clean, it cannot find its way to  
13 the end of the bridge. There is another reason in my mind  
14 why deterioration accelerates, and on top of that, all this  
15 structure we are talking about is four, five years old and  
16 there is normal deterioration in that length of time.

17 JUDGE GESOFF: Thank you. Anyone else have  
18 cross-examination based upon that?

19 MR. MARTIN: Yes, Your Honor.

20 RE-CROSS-EXAMINATION

21 BY MR. MARTIN:

22 Q Mr. Calder, you stated that the gutters should be  
23 kept clean and any interference with the drainage system as  
24 designed would cause a problem. Are you assuming -- if  
25 there was such a failure in the drainage as designed, is

1 there any other maintainence that could have been done to  
2 minimize the effect of that fill you would have?

3 A Well, I think in recent years there have been  
4 improvements in technology, but the bridge -- when it was  
5 designed, it was designed with the criteria that was  
6 prevalent at that time and it was proved by all of the  
7 principles. I don't think anyone objected to the design at  
8 the time. It's 45 years old and there certainly have been  
9 some improvements since that time. Our design today would  
10 be more sophisticated than it was then, this is true.

11 Q My question is directed more towards maintenance of  
12 those parts of the bridge which are damaged by the water  
13 that is misdirected. Is there something that could be done  
14 to those members or bridge parts that would minimize the  
15 effect even though that water is getting in there?

16 A Well, I think the fact that water is getting in  
17 there and has gotten over there, a percent is what  
18 necessitates the plans being prepared to remedy that  
19 situation.

20 Q So is the only remedy then to stop the water going  
21 in there? There is no treatment to the portions that get  
22 wet that shouldn't get wet?

23 A Well, there is always something that can be done.  
24 It's like putting a Band-Aid on a wet arm. It may help and  
25 it may not stay, that's all. But I think the real answer to

1 this is to recognize the fact the condition exists and  
2 prepare plans to rectify it.

3 MR. MARTIN: I have nothing further, Your Honor.

4 JUDGE GESOFF: Mr. Gibboney?

5 MR. GIBBONEY: Nothing, Your Honor.

6 JUDGE GESOFF: Mr. Donovan?

7 MR. DONOVAN: Nothing.

8 JUDGE GESOFF: You're excused.

9 (Witness excused.)

10 JUDGE GESOFF: Anything further gentlemen? All  
11 right. Let the Record reflect that there is still no one in  
12 the hearing room representing the County of Blair. Do  
13 parties wish to file briefs?

14 MR. HERSKOVITZ: Your Honor, as far as Commission  
15 Staff is concerned, our position is exactly as presented by  
16 my witness, Mr. Calder.

17 JUDGE GESOFF: Very well. Mr. Martin, Mr. Gibboney  
18 any desire to file briefs?

19 MR. GIBBONEY: I have no desire to file any brief in  
20 this matter.

21 MR. DONOVAN: I believe I am able to submit it on the  
22 Record, Your Honor.

23 JUDGE GESOFF: Mr. Martin?

24 MR. MARTIN: Your Honor, I would like to defer that  
25 decision. If you would like to establish a timetable in

1 which I may file a brief if I desire, I would appreciate it.

2 JUDGE GESOFF: Okay. Mr. Donovan?

3 MR. DONOVAN: Obviously I don't have to reserve that  
4 right too, Your Honor. I assume that Mr. Martin will let me  
5 know promptly whether he intends to file a brief, because I  
6 believe under the Commission's Rules, I would not be  
7 entitled to file a brief to his main brief unless I file a  
8 main brief myself.

9 JUDGE GESOFF: Yes, I could waive that rule if I so  
10 desire. However, why don't I send out the briefing letter  
11 when I receive the transcript. Were you going to say  
12 something, Mr. Martin?

13 MR. MARTIN: No, Your Honor. I think we were thinking  
14 along the same lines. Perhaps in your letter, ask for an  
15 immediate response from us, whether we intend to file a  
16 brief.

17 JUDGE GESOFF: All right. We will leave it at that.  
18 In my letter, I'll say something about reply briefs without  
19 main briefs. Very well. If there is nothing further. All  
20 right. Thank you very much.

21 (Whereupon, at 12:10 p.m., the hearing concluded.)

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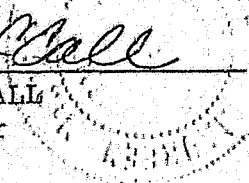
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I hereby certify that the proceedings and evidence are contained fully and accurately in the notes taken by me during the hearing of the within cause, and that this is a true and correct transcript of the same.

*Maureen T. McCall*  
MAUREEN T. McCALL  
Court Reporter



The foregoing certification does not apply to the reproduction of the same by any means unless under the direct control and/or supervision of the certifying reporter.

HOLBERT ASSOCIATES  
MAUREEN T. McCALL  
Suite 401, Kunkel Building  
301 Market Street  
Harrisburg, Pennsylvania 17101

Penn DOT EXHIBIT NO. 1  
P. U. C. NO: C849440  
DATE OF HEARING 6/26/85  
M. J. M. G. H.

JUL 13 1985

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DOCKETED  
JUL 13 1985

PENNSYLVANIA PUBLIC UTILITY COMMISSION

Complaint Docket No. 11005

DEPARTMENT OF HIGHWAYS OF THE  
COMMONWEALTH OF PENNSYLVANIA

DOCKETED  
DOCKET  
JUL 22 1985

v.  
THE PENNSYLVANIA RAILROAD COMPANY,  
ALTOONA & LOGAN VALLEY ELECTRIC  
RAILWAY COMPANY, COUNTY OF BLAIR  
and TOWNSHIP OF SNYDER, BLAIR COUNTY

ORDER

BY THE COMMISSION:

State Highway Route No. 55, which is part of the primary system of state highways, and is heavily traveled, is carried over and above the grade of four tracks of the Middle Division of The Pennsylvania Railroad Company at a point in Snyder Township, Blair County, in the vicinity of the Village of Grazierville, by means of a single span pin connected through steel truss bridge supported on stone abutments. The instant complaint alleges that this bridge is dangerous and inadequate and should be reconstructed.

The bridge is approximately 100 feet in length, and crosses the railroad company's right of way at an angle of about 90 degrees. The distance between the trusses is about 18 feet, and the clear width of the timber plank roadway between wheel guards is about 15 feet 2 inches. No sidewalks are provided on the bridge.

The highway in the vicinity of the overhead crossing is graded for a width of about 26 feet and improved with a concrete paving 16 feet in width, and extends in a general northeasterly direction, on irregular grade and alignment from State Highway Survey Station 980/92, located at a point in Antis Township, Blair County, about 5500 feet southwest of the overgrade crossing to a point in Snyder Township, about 150 feet southwest of the crossing, extends thence on a sharp curve to the north and on a 6% ascending grade to

the bridge and extends thence over the grade of the four main tracks of the railroad company. The highway continues northwestwardly and, on a 4% descending grade for about 150 feet, curves sharply to the east and extends northeastwardly through the Village of Grazierville toward Tyrone.

A general plan of the proposed improvement prepared by the Department of Highways and submitted at the hearing held July 29, 1938, as Department of Highways' Exhibit No. 1, provides for the construction of a new main highway approximately 7918 feet in length, beginning in the existing State Highway Route No. 55, at State Highway Survey Station 980/92 in Antis Township, Blair County, about 5500 feet west of the present overhead bridge and extending thence generally northeastwardly to a junction with the present highway at a point in the Village of Grazierville, Snyder Township, Blair County, at State Highway Survey Station 1060/10 located about 2400 feet east of the present overhead bridge, and for the construction of a new bridge to carry the new main highway over the grade of four main tracks of the railroad company at a point in the Village of Grazierville, Snyder Township, Blair County, about 300 feet east of the existing overgrade crossing.

The Commission has assumed jurisdiction over that portion of the improvement approximately 1773 feet in length located between State Highway Survey stations 1030/29 and 1048/02.8, located respectively about 813 feet southwest and 960 feet northeast of the center of the proposed new bridge. These stations are approximately the points at which the alignment and grade of the relocated main highway, as proposed, meet the alignment and grade of the existing approaches to the present overgrade crossing and represent the reasonable limits of the crossing improvement and of this Commission's jurisdiction.

The record shows that the cost of that portion of the proposed highway located beyond the portion included between State Highway Survey stations 1030/29 and 1048/02.8 will be assumed by the Department of Highways, and that all property therefor will be appropriated by the department by virtue of the provisions of the Act of Assembly of May 31, 1911, P.L. 468 and its supplements and amendments.

It is proposed to grade the highway approaches to the proposed overhead bridge to a width of 56 feet and to pave the center thereof with dual type paving 40 feet in width, consisting of two center macadam lanes each 10 feet in width, and two outside concrete lanes each 10 feet in width.

The proposed new bridge as shown on detail plans prepared by the Department of Highways and submitted of record as Department of Highways' Exhibit No. 4 at the hearing of July 29, 1938, consists of a single span through steel truss structure having a length of about 229 feet, extending across the tracks at an angle of about 30 degrees, accommodating a 14-foot roadway and a 5-foot sidewalk on the westerly side thereof, and providing a vertical clearance of 22.4 feet over the tracks of the railroad company.

The estimated cost of reconstructing the overgrade crossing in accordance with the plan submitted at the hearing of July 29, 1938, exclusive of the cost of altering the facilities of any public utility that may be required by reason of the improvement, and exclusive of property damages, is made up of the following principal items:

|                      |                  |
|----------------------|------------------|
| Bridge over Railroad | 3158,439.05      |
| Roadway              | 55,985.68        |
| Railroad Work        | <u>15,489.00</u> |
| Total                | 3207,913.75      |

The construction of the proposed improvement requires the appropriation of several parcels of private property, damages for

which have been estimated at approximately \$5000.

Testimony shows that the construction of the new highway in accordance with the general plan will necessitate the relocation of approximately 1600 feet of a 6.5/8-inch gas pipe line of The Peoples Natural Gas Company, about 1280 feet of which is now located upon a private right of way. The Peoples Natural Gas Company estimates the cost of relocating its facilities at \$2160, which amount does not include a substructure right of way. The proposed improvement will also necessitate certain alterations to the facilities of the Home Electric Light Steam and Heating Company, and the pole and wire lines of The Bell Telephone Company of Pennsylvania. The alterations to the facilities of the Home Electric Light Steam and Heating Company and of the poles and wire line of The Bell Telephone Company of Pennsylvania are estimated to cost \$3400 and \$300, respectively.

The respondent parties of record offered no objection to the plans of the proposed improvement. The record shows that the Department of Highways is willing to assume the total construction cost of reconstructing the overgrade crossing and the approaches thereto in accordance with the general plan submitted as Department of Highways' Exhibit No. 1 at the hearing of July 29, 1938, provided Federal funds appropriated under The Federal-Aid Highway Act, as amended June 16, 1936, Public No. 685-74th Congress, are allocated to said department for this project and provided further that the plans of the improvement are approved by the Bureau of Public Roads of the United States Department of Agriculture.

The record further shows that The Pennsylvania Railroad Company and the County of Blair are each willing to assume 50% of the amount of damages due for property taken, injured or destroyed by reason of the improvement.

The testimony shows that, subsequent to the filing of the instant complaint, Altoona & Logan Valley Electric Railway Company has been granted Commission approval to discontinue the operation of its street railway line extending over the existing overgrade crossing, and has removed its tracks in the vicinity thereof.

The matters and things involved having been fully considered, we find and determine that the crossing, above grade, where State Highway Route No. 55 crosses the four main tracks of the Middle Division of The Pennsylvania Railroad Company at a point in Snyder Township, Blair County, about 200 feet west of the Village of Grazierville is dangerous and its abolition by the construction of the new main highway and the construction of a bridge to carry the new main highway over the grade of the four tracks of the railroad company, at a point in the village, about 300 feet east of the present overgrade bridge, all in accordance with the general plan submitted at the hearing of July 29, 1938, as Department of Highways' Exhibit No. 1, is necessary or proper for the service, accommodation, convenience or safety of the public; THEREFORE,

NOW, to wit, March 20, 1939, IT IS ORDERED: That the complaint be and is hereby sustained.

IT IS FURTHER ORDERED: That the crossing above grade, where State Highway Route No. 55 crosses the four main tracks of the Middle Division of The Pennsylvania Railroad Company at a point in Snyder Township, Blair County, about 200 feet west of the Village of Grazierville, be abolished by the construction of a new main highway and the construction of a bridge to carry the new main highway over the grade of the tracks of the railroad company at a point in the village about 300 feet east of the present overgrade crossing, all in accordance with the general plan submitted at the hearing of July 29, 1938, as Department of Highways' Exhibit No. 1, which said plan is hereby approved, attached hereto and made part hereof.

IT IS FURTHER ORDERED: That the detail plan submitted at the hearing of July 29, 1938, as Department of Highways' Exhibit No. 4 be and is hereby approved.

IT IS FURTHER ORDERED: That the following public highway be and is hereby laid out and established, to wit:

A road or highway, approximately 1773 feet in length beginning in the existing State Highway Route No. 55 at a point in Snyder Township, Blair County, about 615 feet southwest of the center of the bridge carrying said highway over the grade of the four main tracks of the Middle Division of The Pennsylvania Railroad Company in the Village of Grazierville herein ordered abolished and at or about State Highway Survey Station 1030/29.5, and extending thence in a general northeasterly direction to a junction with the existing state highway at a point in the township about 1400 feet northeast of the present overgrade crossing and at about State Highway Survey Station 1048/02.8; alignment, width and grades to be as shown on the approved plan.

IT IS FURTHER ORDERED: That the following described properties, together with all buildings and structures or parts thereof of any description or use whatsoever, erected or located thereon, be and are hereby severally taken and appropriated for all purposes of the improvement in accordance with the approved plans other than for the purpose of slopes resulting from cuts or fills and for the purpose of a temporary road, to wit:

1. Fuoss Brothers, record owners  
William Fuoss,  
Lincoln Avenue & Tenth St.,  
Tyrone, Pa.

(Jacob Fuoss, R. D. No. 2, Tyrone, Pa.  
George Fuoss, R. D. No. 2., Tyrone, Pa.)

ALL THOSE CERTAIN tracts of land required for the purpose of right of way, situated in Snyder Township, Blair County, bounded and described as follows, to wit:

Tract #2

Beginning at a point in the northwesterly side of the proposed relocated State Highway Route No. 55, said point being distant thirty and seventeen hundredths (30.17) feet northwesterly from the center line of said State highway route, at or

about Station 1030/29.5 thereof, as shown on the plan; thence North forty-one degrees, forty-six minutes East (N 41° 46' E), a distance of seventy-seven and ninety-six hundredths (77.96) feet to a point; thence northeasterly by a curve to the left, having a radius of two thousand two hundred sixty-two and one hundredth (2262.01) feet, a distance of sixty-two and seven hundredths (62.07) feet to a point; thence North fifty-four degrees, fifty minutes East (N 54° 50' E), a distance of two hundred eight and forty-six hundredths (208.46) feet to a point; thence southwesterly by a curve to the right, having a radius of two thousand three hundred twenty-two and one hundredth (2322.01) feet, a distance of one hundred thirty-one and eighty hundredths (131.80) feet to a point; thence South fifty-five degrees, six minutes West (S 55° 06' W), a distance of two hundred sixteen and seventy-five hundredths (216.75) feet to a point; thence North fifty-four degrees, fourteen minutes West (N 54° 14' W), a distance of six and sixty-six hundredth (6.66) feet, more or less, to a point and the place of beginning and containing two hundred five thousandths (0.205) of an acre of land, more or less.

#### Tract #4

Beginning at a point in the southeasterly side of the proposed relocated State Highway Route No. 55, said point being distant thirty (30) feet southeasterly from the center line of said State Highway Route, at Station 1034/56.5 and measured at right angles thereto; thence northerly by a curve to the left, having a radius of one hundred twenty-one and five tenths (121.5) feet, a distance of sixty-seven and eighty-eight hundredths (67.88) feet to a point; thence South thirty degrees, forty-two minutes East (S 30° 42' E), a distance of forty-three and seventy-one hundredths (43.71) feet to a point; thence southwesterly by a curve to the right, having a radius of two thousand three hundred twenty-two and one hundredth (2322.01) feet, a distance of thirty-three and thirteen hundredths (33.13) feet, more or less, to a point and the place of beginning and containing eleven thousandths (0.011) of an acre of land, more or less.

#### Tract #5

Beginning at a point in the northwesterly side of the proposed relocated State Highway Route No. 55, said point being distant thirty and seventeen hundredths (30.17) feet northwesterly from the center line of said State highway route, at or about Station 1030/29.5 thereof, as shown on the plan; thence North fifty-five degrees, six minutes West (N 55° 06' W), a distance of two hundred sixteen and seventy-five hundredths (216.75) feet to a point; thence southwesterly by a curve to the right, having a radius of two thousand three hundred twenty-two and one hundredth (2322.01) feet, a distance of one hundred thirty-three and seventy-two hundredths (133.72) feet to a point; thence South forty-one degrees, forty-six minutes West (S 41° 46' W), a distance of seventy-one and sixty-six hundredths (71.66) feet to a point; thence North fifty-four degrees, fourteen minutes West (N 54° 14' W), a distance of fifty-three and sixty-seven hundredths (53.67)

feet, more or less, to a point and the place of beginning and containing one hundred twenty-nine thousandths (0.129) of an acre of land, more or less.

Tract #8

Beginning at a point in the northwesterly side of the proposed relocated State Highway Route No. 55, said point being distant forty-four and forty-four hundredths (44.44) feet northwesterly from the center line of said State highway route, at Station 1032/45.47 and measured at right angles thereto; thence northeasterly by a curve to the left, having a radius of two hundred eighty-seven and ninth-four hundredths (287.94) feet, a distance of ninety-four and ninety-two hundredths (94.92) feet to a point; thence northeasterly by a curve to the left, having a radius of two thousand two hundred sixty-two and one hundredth (2262.01) feet, a distance of one hundred nineteen and two hundredths (119.02) feet to a point; thence South eighteen degrees, fifty-two minutes, fifteen seconds East (S 18° 52' 15" E), a distance of twelve and forty-seven hundredths (12.47) feet to a point; thence southerly by a curve to the right, having a radius of sixty-three and twenty-eight hundredths (63.28) feet, a distance of eighty-one and forty hundredths (81.40) feet to a point; thence South fifty-four degrees, fifty minutes West (S 54° 50' W), a distance of one hundred forty-one and three hundredths (141.03) feet, more or less, to a point and the place of beginning and containing eighty thousandths (0.080) of an acre of land, more or less.

Tract #10

Beginning at a point in the northwesterly side of the proposed relocated State Highway Route No. 55, said point being distant thirty (30) feet northwesterly from the center line of said State highway route, at Station 1035/20.87 and measured at right angles thereto; thence northeasterly by a curve to the left, having a radius of two thousand two hundred sixty-two and one hundredth (2262.01) feet, a distance of ninety-seven and forty-eight hundredths (97.48) feet to a point; thence North twenty-eight degrees, fifty-three minutes East (N 28° 53' E), a distance of one hundred three and seventy hundredths (103.70) feet to a point; thence along lands of The Pennsylvania Railroad Company, North fifty-eight degrees, fifty-five minutes East (N 58° 55' E), a distance of one hundred nineteen and eighty-eight hundredths (119.88) feet to a point; thence South twenty-eight degrees, fifty-three minutes West (S 28° 53' W), a distance of two hundred seven and forty-eight hundredths (207.48) feet to a point; thence southwesterly by a curve to the right, having a radius of two thousand three hundred twenty-two and one hundredth (2322.01) feet, a distance of one hundred thirty-one and seventy-seven hundredths (131.77) feet to a point; thence North thirty degrees, forty-two minutes West (N 30° 42' W), a distance of sixty-seven and sixty-seven hundredths (67.67) feet, more or less, to a point

and the place of beginning and containing three hundred seventy-four thousandths (0.374) of an acre of land, more or less.

Tract #15

Beginning at a point in the northwesterly side of the proposed relocated State Highway Route No. 55, said point being distant thirty-three (33) feet northwesterly from the center line of said State highway route, at Station 1038/50 and measured at right angles thereto; thence North twenty-eight degrees, fifty-three minutes East ( $N 28^{\circ} 53' E$ ), a distance of one hundred ninety-two and twenty-four hundredths (192.24) feet to a point; thence northerly by a curve to the left, having a radius of fifty (50) feet, a distance of one hundred thirty and nine tenths (130.9) feet to a point; thence North fifty-eight degrees, fifty-three minutes East ( $N 58^{\circ} 53' E$ ), a distance of four hundred fifty-one and ninety-seven hundredths (451.97) feet to a point; thence southwesterly by a curve to the left, having a radius of one thousand twelve and fourteen hundredths (1012.14) feet, a distance of three hundred seventy-eight and ninety-two hundredths (378.92) feet to a point; thence South twenty-eight degrees, fifty-three minutes West ( $S 28^{\circ} 53' W$ ), a distance of one hundred thirty and thirty-four hundredths (130.34) feet to a point; thence along lands of The Pennsylvania Railroad Company, South fifty-eight degrees, fifty-five minutes West ( $S 58^{\circ} 55' W$ ), a distance of one hundred twenty-five and eighty-seven hundredths (125.87) feet, more or less, to a point and the place of beginning and containing seven hundred eighty-eight thousandths (0.788) of an acre of land, more or less.

Tract #17

Beginning at a point in the northwesterly side of the proposed relocated State Highway Route No. 55, said point being distant thirty (30) feet northwesterly from the center line of said State highway route, at Station 1044/57 and measured at right angles thereto; thence northeasterly by a curve to the right, having a radius of one thousand seventy-two and fourteen hundredths (1072.14) feet, a distance of two hundred eleven and ninety hundredths (211.90) feet to a point; thence North fifty-nine degrees, twenty minutes East ( $N 59^{\circ} 20' E$ ), a distance of ninety-nine and seventy hundredths (99.70) feet to a point; thence South thirty degrees, fifty-five minutes East ( $S 30^{\circ} 55' E$ ), a distance of nineteen and seventeen hundredths (19.17) feet to a point; thence South fifty-eight degrees, fifty-three minutes West ( $S 58^{\circ} 53' W$ ), a distance of three hundred ten and forty-one hundredths (310.41) feet, more or less, to a point and the place of beginning and containing one hundred ten thousandths (0.110) of an acre of land, more or less.

Tract #18

Beginning at a point in the northwesterly side of the proposed relocated State Highway Route No. 55, said point being distant one hundred ten (110) feet northwesterly from the center line of said State highway route, at or about Station 1041/84 thereof and measured at right angles thereto; thence North fifty-eight degrees, fifty-

three minutes East (N 58° 53' E), a distance of fifty (50) feet to a point; thence North thirty-one degrees, seven minutes West (N 31° 07' W), a distance of eight (8) feet to a point; thence North fifty-eight degrees, fifty-three minutes East (N 58° 53' E), a distance of two hundred thirty and forty-one hundredths (230.41) feet to a point; thence southwesterly by a curve to the left, having a radius of one thousand seventy-two and fourteen hundredths (1072.14) feet, a distance of twenty-seven and seventy-eight hundredths (27.78) feet to a point; thence southwesterly by a curve to the right, having a radius of five hundred seventy-three and sixty-nine hundredths (573.69) feet, a distance of one hundred forty-one and fifty-two hundredths (141.52) feet to a point; thence South fifty-eight degrees, fifty-three minutes West (S 58° 53' W), a distance of one hundred twelve and ninety-nine hundredths (112.99) feet to a point; thence North thirty-one degrees, seven minutes West (N 31° 07' W), a distance of fifteen (15) feet, more or less, to a point and the place of beginning and containing one hundred ten thousandths (0.110) of an acre of land, more or less.

Tract #19

Beginning at a point in the northwesterly side of the proposed relocated State Highway Route No. 55, said point being distant thirty (30) feet northwesterly from the center line of said State highway route, at Station 1044/37 and measured at right angles thereto; thence North fifty-eight degrees, fifty-three minutes East (N 58° 53' E), a distance of three hundred seventy and forty-one hundredths (370.41) feet to a point; thence South thirty degrees, fifty-five minutes East (S 30° 55' E), a distance of twenty-three (23) feet to a point; thence South fifty-eight degrees, fifty-three minutes West (S 58° 53' W), a distance of five hundred thirty-seven and seventy-five hundredths (537.75) feet to a point; thence northeasterly by a curve to the left, having a radius of five hundred seventy-three and sixty-nine hundredths (573.69) feet, a distance of one hundred forty-one and fifty-two hundredths (141.52) feet to a point; thence northeasterly by a curve to the right, having a radius of one thousand seventy-two and fourteen hundredths (1072.14) feet, a distance of twenty-seven and seventy-eight hundredths (27.78) feet, more or less, to a point and the place of beginning and containing two hundred twenty-one thousandths (0.221) of an acre of land, more or less.

The above nine (9) tracts of land are a portion of the real estate which became legally vested in the Fuoss Brothers by the deed of the Tyrone Brick and Tile Company, dated May 14, 1928, and recorded in Deed Book Volume 373, Page 234, in the Hollidaysburg Court House in the County of Blair.

2. Ira L. Graziop, record owner  
R. D. No. 2,  
Tyrone, Pa.

ALL THAT CERTAIN tract of land required for the purpose of right of way, situated in Snyder Township, Blair County, bounded and described as follows, to wit:

Tract #20

Beginning at a point in the northwesterly side of the proposed relocated State Highway Route No. 55, said point being distant thirty (30) feet northwesterly from the center line of said State highway route, at Station 1047/42.8 and measured at right angles thereto; thence North fifty-nine degrees, twenty minutes East (N 59° 20' E); a distance of sixty (60) feet to a point; thence South thirty degrees, fifty-five minutes East (S 30° 55' E), a distance of eighteen and seventy hundredths (18.70) feet to a point; thence South fifty-eight degrees, fifty-three minutes West (58° 53' W), a distance of sixty (60) feet to a point; thence North thirty degrees, fifty-five minutes West (N 30° 55' W), a distance of nineteen and seventeen hundredths (19.17) feet; more or less, to a point and the place of beginning and containing twenty-six thousandths (0.026) of an acre of land, more or less.

The above tract of land is a portion of the real estate which became legally vested in Ira L. Grazior by the deed of William M. Phillips, dated August 24, 1908, and recorded in Deed Book 183, Page 630, in the Hollidaysburg Court House in the County of Blair.

IT IS FURTHER ORDERED: That, in addition to the foregoing properties, the following described property, together with all buildings and structures or parts thereof of any description or use whatsoever, erected or located thereon, be and is hereby taken and appropriated for the purposes of the improvement in accordance with the approved plans, for the purpose of a temporary road.

1. Fuoss Brothers, record owners  
William Fuoss,  
Lincoln Avenue & Tenth St.,  
Tyrone, Pa.

Jacob Fuoss, R. D. No. 2, Tyrone, Pa.  
George Fuoss, R. D. No. 2, Tyrone, Pa.

ALL THAT CERTAIN tract of land required for the purpose of a temporary road during construction only, situated in Snyder Township, Blair County, bounded and described as follows, to wit:

Tract #15

Beginning at a point in the northwesterly side of the proposed relocated State Highway Route No. 55, said point being distant ninety-five (95) feet northwesterly from the center line of said State highway route, at Station 1042/32.59 and measured at right angles thereto; thence

northeasterly by a curve to the right, having a radius of one thousand one hundred thirty-seven and fourteen hundredths (1137.14) feet, a distance of nineteen (19) feet to a point; thence South fifty-two degrees, sixteen minutes, forty-eight seconds East ( $S 52^{\circ} 16' 48'' E$ ), a distance of five (5) feet to a point; thence northeasterly by a curve to the right, having a radius of one thousand one hundred thirty-two and fourteen hundredths (1132.14) feet, a distance of two hundred seventy-one and fifty-nine hundredths (271.59) feet to a point; thence South thirty-eight degrees, thirty-one minutes, forty-eight seconds East ( $S 38^{\circ} 31' 48'' E$ ), a distance of ten (10) feet to a point; thence northeasterly by a curve to the right, having a radius of one thousand one hundred twenty-two and fourteen hundredths (1122.14) feet, a distance of one hundred seven and sixty-eight hundredths (107.68) feet to a point; thence South thirty-three degrees, one minute, forty-eight seconds East ( $S 33^{\circ} 01' 48'' E$ ), a distance of fifteen (15) feet to a point; thence northeasterly by a curve to the right, having a radius of one thousand one hundred seven and fourteen hundredths (1107.14) feet, a distance of forty-five and sixty-five hundredths (45.65) feet to a point; thence South thirty degrees, forty minutes East ( $S 30^{\circ} 40' E$ ), a distance of twenty (20) feet to a point; thence North fifty-nine degrees, twenty minutes East ( $N 59^{\circ} 20' E$ ), a distance of fifty-seven and three hundredths (57.03) feet to a point; thence South thirty degrees, forty minutes East ( $S 30^{\circ} 40' E$ ), a distance of five (5) feet to a point; thence South fifty-nine degrees, twenty minutes West ( $S 59^{\circ} 20' W$ ), a distance of fifty-seven and three hundredths (57.03) feet to a point; thence southwesterly by a curve to the left, having a radius of one thousand eighty-two and fourteen hundredths (1082.14) feet, a distance of forty-four and sixty-two hundredths (44.62) feet to a point; thence North thirty-three degrees, one minute, forty-eight seconds West ( $N 33^{\circ} 01' 48'' W$ ), a distance of five (5) feet to a point; thence southwesterly by a curve to the left, having a radius of one thousand eighty-seven and fourteen hundredths (1087.14) feet, a distance of one hundred four and thirty-two hundredths (104.32) feet to a point; thence North thirty-eight degrees, thirty-one minutes, forty-eight seconds West ( $N 38^{\circ} 31' 48'' W$ ), a distance of five (5) feet to a point; thence southwesterly by a curve to the left, having a radius of one thousand ninety-two and fourteen hundredths (1092.14) feet, a distance of ninety-nine and fifty-six hundredths (99.56) feet to a point; thence South fifty-eight degrees, fifty-three minutes West ( $S 58^{\circ} 53' W$ ), a distance of ninety-four and sixteen hundredths (94.16) feet to a point; thence South thirty-one degrees, seven minutes East ( $S 31^{\circ} 07' E$ ), a distance of twelve (12) feet to a point; thence South fifty-eight degrees, fifty-three minutes West ( $S 58^{\circ} 53' W$ ), a distance of ninety-five and two tenths (95.2) feet, more or less, to a point and the place of beginning and containing three hundred fourteen thousandths (0.314) of an acre of land, more or less.

The above tract of land is a portion of the real estate which became legally vested in the Fuoss Brothers by the deed of the Tyrone Brick and Tile Company, dated May 14, 1928, and recorded in Deed Book Volume 373, Page 234, in the Hollidaysburg Court House in the County of Blair.

IT IS FURTHER ORDERED: That, in addition to the foregoing properties, the following described properties, together with all buildings and structures or parts thereof of any description or use whatsoever, erected or located thereon, be and are hereby severally taken and appropriated for the purpose of the improvement in accordance with the approved plans, for the purpose of slopes resulting from cuts or fills, to wit:

1. Fuoss Brothers, record owners  
William Fuoss,  
Lincoln Avenue & Tenth St.,  
Tyrone, Pa.

Jacob Fuoss, R. D. No. 2, Tyrone, Pa.  
George Fuoss, R. D. No. 2, Tyrone, Pa.

ALL THOSE CERTAIN tracts of land required for the purposes of slopes, situated in Snyder Township, Blair County, bounded and described as follows, to wit:

Tract #1

Beginning at a point in the northwesterly side of the proposed relocated State Highway Route No. 55, said point being distant forty-five and twenty-five hundredths (45.25) feet northwesterly from the center line of said State highway route, at or about Station 1030/29.5 thereof, as shown on the plan; thence North forty-one degrees, forty-six minutes East (N 41° 46' E), a distance of twenty-five and twenty-three hundredths (25.23) feet to a point; thence South forty-eight degrees, fourteen minutes East (S 48° 14' E), a distance of fifteen (15) feet to a point; thence South forty-one degrees, forty-six minutes West (S 41° 46' W), a distance of twenty-three and sixty-five hundredths (23.65) feet to a point; thence North fifty-four degrees, fourteen minutes West (N 54° 14' W), a distance of fifteen and eight hundredths (15.08) feet, more or less, to a point and the place of beginning and containing eight thousandths (0.008) of an acre of land, more or less.

Tract #3

Beginning at a point in the southeasterly side of the proposed relocated State Highway Route No. 55, said point being distant thirty (30) feet southeasterly from the center line of said State highway route, at Station 1032/36.3 and measured at right angles thereto; thence northeasterly by a curve to the left, having a radius of two thousand three hundred twenty-two and one hundredth (2322.01) feet, a distance of one hundred thirty-one and eighty hundredths (131.80) feet to a point; thence to the right, north fifty-four degrees, fifty minutes East (N 54° 50' E), a distance

of two and nine-tenths (2.9) feet to a point; thence northeasterly by a curve to the left, having a radius of one hundred twenty-one and five tenths (121.5) feet, a distance of ninety and five hundredths (90.05) feet to a point; thence northeasterly by a curve to the left, having a radius of two thousand three hundred twenty-two and one hundredth (2322.01) feet, a distance of thirty-three and thirteen hundredths (33.13) feet to a point; thence South thirty degrees, forty-two minutes East ( $S 30^{\circ} 42' E$ ), a distance of twenty-two and fifty-six hundredths (22.56) feet to a point; thence southwesterly by a curve to the right, having a radius of two thousand three hundred forty-two and one hundredth (2342.01) feet, a distance of one hundred eighty-five and forty-three hundredths (185.43) feet to a point; thence South fifty-five degrees, six minutes West ( $S 55^{\circ} 06' W$ ), a distance of sixty-five and ninety-four hundredths (65.94) feet, more or less, to a point and the place of beginning and containing eighty-nine thousandths (0.089) of an acre of land, more or less.

#### Tract #6

Beginning at a point in the southeasterly side of the proposed relocated State Highway Route No. 55, said point being distant thirty and seventeen hundredths (30.17) feet southeasterly from the center line of said State highway route, at or about Station 1030/29.5 thereof, as shown on the plan; thence North forty-one degrees, forty-six minutes East ( $N 41^{\circ} 46' E$ ), a distance of seventy-one and sixty-six hundredths (71.66) feet to a point; thence northeasterly by a curve to the left, having a radius of two thousand three hundred twenty-two and one hundredth (2322.01) feet, a distance of one hundred thirty-three and seventy-two hundredths (133.72) feet to a point; thence North fifty-five degrees, six minutes East ( $N 55^{\circ} 06' E$ ), a distance of sixty-five and ninety-four hundredths (65.94) feet to a point; thence southwesterly by a curve to the right, having a radius of two thousand three hundred forty-two and one hundredth (2342.01) feet, a distance of one hundred ninety-eight and two hundredths (198.02) feet to a point; thence South forty-eight degrees, fourteen minutes East ( $S 48^{\circ} 14' E$ ), a distance of five (5) feet to a point; thence South forty-one degrees, forty-six minutes West ( $S 41^{\circ} 46' W$ ), a distance of sixty-nine and three hundredths (69.03) feet to a point; thence North fifty-four degrees, fourteen minutes West ( $N 54^{\circ} 14' W$ ), a distance of twenty-five and fourteen hundredths (25.14) feet, more or less, to a point and the place of beginning and containing one hundred eighteen thousandths (0.118) of an acre of land, more or less.

#### Tract #7

Beginning at a point in the northwesterly side of the proposed relocated State Highway Route No. 55, said point being distant forty-five (45) feet northwesterly from the center line of said State highway route, at Station 1032/40.4 and measured at right angles thereto; thence northeasterly by a curve to the left, having a radius of two thousand two hundred forty-seven and one hundredth (2247.01) feet, a distance of two hundred twenty-eight and seventy-two hundredths (228.72) feet to a point; thence South eighteen degrees, fifty-two minutes, fifteen seconds East ( $S 18^{\circ} 52' 15" E$ ), a distance of

nineteen and sixteen hundredths (19.16) feet to a point; thence southwesterly by a curve to the right, having a radius of two thousand two hundred sixty-two and one hundredth (2262.01) feet, a distance of one hundred nineteen and two hundredths (119.02) feet to a point; thence southwesterly by a curve to the right, having a radius of two hundred eighty-seven and ninety-four hundredths (287.94) feet, a distance of ninety-four and ninety-two hundredths (94.92) feet to a point; thence South fifty-four degrees, fifty minutes West (S 54° 50' W), a distance of five (5) feet, more or less, to a point and the place of beginning and containing sixty-six thousandths (0.066) of an acre of land, more or less.

#### Tract #9

Beginning at a point in the northwesterly side of the proposed relocated State Highway Route No. 55, said point being distant forty-five (45) feet northwesterly from the center line of said State highway route, at Station 1055/88.96 and measured at right angles thereto; thence northeasterly by a curve to the left, having a radius of two thousand two hundred forty-seven and one hundredth (2247.01) feet, a distance of twenty and sixty-four hundredths (20.64) feet to a point; thence North fifty-nine degrees, twenty-two minutes, thirty-two seconds West (N 59° 22' 32" W), a distance of ten (10) feet to a point; thence northeasterly by a curve to the left, having a radius of two thousand two hundred thirty-seven and one hundredth (2237.01) feet, a distance of sixty-seven and ninety-seven hundredths (67.97) feet to a point; thence North sixty-one degrees, seven minutes West (N 61° 07' W), a distance of twenty (20) feet to a point; thence North twenty-eight degrees, fifty-three minutes East (N 28° 53' E), a distance of twenty-five and eighty-six hundredths (25.86) feet to a point; thence along lands of The Pennsylvania Railroad Company, North fifty-eight degrees, fifty-five minutes East (N 58° 55' E), a distance of eighty-nine and ninety-one hundredths (89.91) feet to a point; thence South twenty-eight degrees, fifty-three minutes West (S 28° 53' W), a distance of one hundred three and seventy hundredths (103.70) feet to a point; thence southwesterly by a curve to the right, having a radius of two thousand two hundred sixty-two and one hundredth (2262.01) feet, a distance of ninety-seven and forty-eight hundredths (97.48) feet to a point; thence North thirty degrees, forty-two minutes West (N 30° 42' W), a distance of sixteen and ninety-two hundredths (16.92) feet, more or less, to a point and the place of beginning; and containing one hundred seventeen thousandths (0.117) of an acre of land, more or less.

#### Tract #11

Beginning at a point in the southeasterly side of the proposed relocated State Highway Route No. 55, said point being distant thirty (30) feet southeasterly from the center line of said State highway route, at Station 1054/89.57 and measured at right angles thereto; thence northeasterly by a curve to the left, having a radius of two thousand three hundred twenty-two and one hundredth (2322.01) feet, a distance of one hundred thirty-one and seventy-seven hundredths (131.77) feet to a point; thence North twenty-eight degrees, fifty-three minutes East

(N 28° 53' E), a distance of two hundred seven and forty-eight hundredths (207.48) feet to a point; thence along lands of the Pennsylvania Railroad Company, North fifty-eight degrees, fifty-five minutes East (N 58° 55' E), a distance of fifty-nine and ninety-four hundredths (59.94) feet to a point; thence South twenty-eight degrees, fifty-three minutes West (S 28° 53' W), a distance of two hundred fifty-nine and thirty-seven hundredths (259.37) feet to a point; thence southwesterly by a curve to the right, having a radius of two thousand three hundred fifty-two and one hundredth (2352.01) feet, a distance of seventy-one and forty-six hundredths (71.46) feet to a point; thence North fifty-nine degrees, twenty-two minutes, thirty-two seconds West (N 59° 22' 32" W), a distance of ten (10) feet to a point; thence southwesterly by a curve to the right, having a radius of two thousand three hundred forty-two and one hundredth (2342.01) feet, a distance of seventy-one and ninety-seven hundredths (71.97) feet to a point; thence North thirty degrees, forty-two minutes West (N 30° 42' W), a distance of twenty-two and fifty-six hundredths (22.56) feet, more or less, to a point and the place of beginning and containing two hundred forty-seven thousandths (0.247) of an acre of land, more or less.

#### Tract #12

Beginning at a point in the northwesterly side of the proposed relocated State Highway Route No. 55, said point being distant sixty-five (65) feet northwesterly from the center line of said State highway route, at Station 1039 and measured at right angles thereto; thence North twenty-eight degrees, fifty-three minutes East (N 28° 53' E), a distance of one hundred (100) feet to a point; thence South sixty-one degrees, seven minutes East (S 61° 07' E), a distance of five (5) feet to a point; thence North twenty-eight degrees, fifty-three minutes East (N 28° 53' E), a distance of eighty-six and sixty-four hundredths (86.64) feet to a point; thence southerly by a curve to the right, having a radius of fifty (50) feet, a distance of fifty-four and sixty-four hundredths (54.64) feet to a point; thence South twenty-eight degrees, fifty-three minutes West (S 28° 53' W), a distance of one hundred forty-two and twenty-four hundredths (142.24) feet to a point; thence North Sixty-one degrees, seven minutes West (N 61° 07' W), a distance of thirty-two (32) feet, more or less, to a point and the place of beginning and containing one hundred twenty-five thousandths (0.125) of an acre of land, more or less.

#### Tract #14

Beginning at a point in the southeasterly side of the proposed relocated State Highway Route No. 55, said point being distant thirty (30) feet southeasterly from the center line of said State highway route, at Station 1039/52.99 and measured at right angles thereto; thence North twenty-eight degrees, fifty-three minutes East (N 28° 53' E), a distance of one hundred thirty and thirty-four hundredths (130.34) feet to a point; thence north-

easterly by a curve to the right, having a radius of one thousand twelve and fourteen hundredths (1012.14) feet, a distance of three hundred seventy-eight and ninety-two hundredths (378.92) feet to a point; thence North fifty-eight degrees, fifty-three minutes East (N 58° 53' E), a distance of sixty-eight and seventy hundredths (68.70) feet to a point; thence South thirty-five degrees, forty-six minutes, forty-eight seconds East (S 35° 46' 48" E), a distance of twelve and eleven hundredths (12.11) feet to a point; thence southwesterly by a curve to the left, having a radius of nine hundred ninety-two and fourteen hundredths (992.14) feet, a distance of ninety-five and twenty hundredths (95.20) feet to a point; thence South forty-one degrees, sixteen minutes, forty-eight seconds East (S 41° 16' 48" E), a distance of five (5) feet to a point; thence southwesterly by a curve to the left, having a radius of nine hundred eighty-seven and fourteen hundredths (987.14) feet, a distance of ninety-four and seventy-two hundredths (94.72) feet to a point; thence South forty-six degrees, forty-six minutes, forty-eight seconds East (S 46° 46' 48" E), a distance of five (5) feet to a point; thence southwesterly by a curve to the left, having a radius of nine hundred eighty-two and fourteen hundredths (982.14) feet, a distance of ninety-four and twenty-four hundredths (94.24) feet to a point; thence South fifty-two degrees, sixteen minutes, forty-eight seconds East (S 52° 16' 48" E), a distance of five (5) feet to a point; thence southwesterly by a curve to the left, having a radius of nine hundred seventy-seven and fourteen hundredths (977.14) feet, a distance of one hundred fifty and sixty-five hundredths (150.65) feet to a point; thence South twenty-eight degrees, fifty-three minutes West (S 28° 53' W), a distance of thirty-nine and thirty-three hundredths (39.33) feet to a point; thence South sixty-one degrees, seven minutes East (S 61° 07' E), a distance of ten (10) feet to a point; thence South twenty-eight degrees, fifty-three minutes West (S 28° 53' W), a distance of thirteen and seventeen hundredths (13.17) feet to a point; thence along lands of The Pennsylvania Railroad Company, South fifty-eight degrees, fifty-five minutes West (S 58° 55' W), a distance of eighty-nine and ninety-one hundredths (89.91) feet, more or less, to a point and the place of beginning and containing three hundred seventy-one thousandths (0.371) of an acre of land, more or less.

Tract #16

Beginning at a point in the northwesterly side of the proposed relocated State Highway Route No. 55, said point being distant seventy-three (73) feet northwesterly from the center line of said State highway route, at or about Survey Station 1043/20 thereof and measured at right angles thereto; thence North fifty-eight degrees, fifty-three minutes East (N 58° 53' E), a distance of ninety-four and sixteen hundredths (94.16) feet to a point; thence northeasterly by a curve to the right, having a radius of one thousand ninety-two and fourteen hundredths (1092.14) feet, a distance of ninety-nine and fifty-six hundredths (99.56) feet to a point; thence South thirty-

eight degrees, thirty-one minutes, forty-eight seconds East (S 38° 31' 48" E), a distance of five (5) feet to a point; thence northeasterly by a curve to the right, having a radius of one thousand eighty-seven and fourteen hundredths (1087.14) feet, a distance of one hundred four and thirty-two hundredths (104.32) feet to a point; thence South thirty-three degrees, one minute, forty-eight seconds East (S 33° 01' 48" E), a distance of five (5) feet to a point; thence northeasterly by a curve to the right, having a radius of one thousand eighty-two and fourteen hundredths (1082.14) feet, a distance of forty-four and sixty-two hundredths (44.62) feet to a point; thence North fifty-nine degrees, twenty minutes East (N 59° 20' E), a distance of fifty-seven and three hundredths (57.03) feet to a point; thence South thirty degrees, forty minutes East (S 30° 40' E), a distance of ten (10) feet to a point; thence South fifty-nine degrees, twenty minutes West (S 59° 20' W), a distance of fifty-seven and three hundredths (57.03) feet to a point; thence southwesterly by a curve to the left, having a radius of one thousand seventy-two and fourteen hundredths (1072.14) feet, a distance of two hundred eleven and ninety hundredths (211.90) feet to a point; thence South fifty-eight degrees, fifty-three minutes West (S 58° 53' W), a distance of one hundred thirty and forty-one hundredths (130.41) feet to a point; thence North thirty-one degrees, seven minutes West (N 31° 07' W), a distance of twelve (12) feet, more or less, to a point and the place of beginning and containing one hundred thirty thousandths (0.130) of an acre of land, more or less.

The above nine (9) tracts of land are a portion of the real estate which became legally vested in the Fuoss Brothers by the deed of the Tyrone Brick and Tile Company, dated May 14, 1928, and recorded in Deed Book Volume 273, Page 234, in the Hollidaysburg Court House in the County of Blair.

IT IS FURTHER ORDERED: That The Pennsylvania Railroad Company furnish all materials and do all work to complete any necessary temporary or permanent alterations or relocation of the facilities on its right of way required by reason of the improvement herein ordered and, in addition, maintain watchmen and flagmen to protect its train operation during the time said temporary or permanent alterations or relocations are being made and during the time the new bridge is being constructed over its tracks and the existing bridge is being removed; said material, work and watchman service being estimated to cost \$15,489.

IT IS FURTHER ORDERED: That the Department of Highways furnish all materials and do all work necessary to complete the remainder of the improvement in accordance with the approved plans,

including the removal of the existing bridge and the establishment and maintenance of any detour necessary for highway traffic during the construction of the improvement, the material recovered in the removal of the existing bridge to become the property of the Department of Highways; said materials and work being estimated to cost \$192,424.73.

IT IS FURTHER ORDERED: That all work herein ordered to be done be fully completed on or before June 1, 1940.

IT IS FURTHER ORDERED: That any relocation of, changes in, or removal of any structures, equipment or other facilities of any public utility, located within the limits of any highway, which may be required as incidental to the execution of this improvement, be made by said public utility, at its sole cost and expense, and in such manner as will not interfere with the construction of the improvement.

IT IS FURTHER ORDERED: That any relocation of, changes in, or removal of any structures, equipment or other facilities of any public utility, located beyond the limits of any highway, which may be required as incidental to the execution of this improvement, be made by said public utility in such manner as will not interfere with the construction of the improvement.

IT IS FURTHER ORDERED: That the Department of Highways cooperate with The Pennsylvania Railroad Company so that in the construction of the improvement the operation of the facilities of the railroad company will not be endangered or unnecessarily impeded.

IT IS FURTHER ORDERED: That The Pennsylvania Railroad Company cooperate with the Department of Highways in the construction of the improvement and operate its trains in the vicinity of the improvement in a safe manner, at a reduced speed and under control during the time the new bridge is being constructed over

its tracks, and during the time the existing bridge is being removed.

IT IS FURTHER ORDERED: That the Department of Highways pay The Pennsylvania Railroad Company, when and as certified by the Pennsylvania Public Utility Commission, a sum or sums of money equal to the actual cost of materials furnished and work done by said railroad company in accordance with this order, including the cost of watchman and flagman services rendered during the time the railroad company is making alterations to or relocating the facilities on its right of way, and during the time the new bridge is being constructed over its tracks and the present bridge is being removed in accordance with this order, but exclusive of services of watchmen, flagmen, linemen or other railroad employes required and placed by the railroad company at other periods; said reimbursement to the railroad company by the Department of Highways to be for only such items of work and expense incurred by the railroad company as are eligible for payment under the rules, regulations and administrative orders of the Bureau of Public Roads of the United States Department of Agriculture, covering highway-railroad crossing projects undertaken with Federal funds appropriated under The Federal-Aid Highway Act, as amended June 16, 1936, Public No. 686-74th Congress; the salvage value of materials recovered or temporarily used in the performance of the work by the railroad company to be deducted from the actual cost of that portion of the work to be done and materials to be furnished by the railroad company at the expense of the Department of Highways.

IT IS FURTHER ORDERED: That The Pennsylvania Railroad Company and Altoona & Logan Valley Electric Railway Company, each pay any money to which they severally may be entitled as compensation for damages for any of their respective properties taken, injured or destroyed by reason of the improvement.

IT IS FURTHER ORDERED: That the County of Blair be responsible for the vacation and the relocation, removal or demolition of non-utility structures, including occupied dwellings located upon property herein appropriated. Any reasonable costs actually

incurred by said county in connection with such vacation, relocation, removal or demolition shall be paid and borne by the same parties and in the same manner and proportion as herein provided for the payment of compensation for damages to the owners of property taken, injured or destroyed by reason of the improvement.

IT IS FURTHER ORDERED: That the County of Blair pay all compensation for damages due to the owners, exclusive of The Pennsylvania Railroad Company, for property taken, injured or destroyed in the execution of this improvement.

IT IS FURTHER ORDERED: That The Pennsylvania Railroad Company, having agreed so to do, pay the County of Blair, when and as ordered by the Pennsylvania Public Utility Commission, a sum or sums of money equal to 50% of the actual sum or sums of money paid by said county in accordance with this order as compensation for damages due to owners of property taken, injured or destroyed by reason of this improvement.

IT IS FURTHER ORDERED: That, effective upon the completion of the improvement and its opening to public use, The Pennsylvania Railroad Company, at its sole cost and expense, thereafter maintain its altered facilities and, in addition, thereafter maintain the substructure and superstructure of the new bridge herein ordered constructed over its tracks and right of way, exclusive of the roadway paving on the superstructure of the bridge.

IT IS FURTHER ORDERED: That, effective upon the completion of the improvement and its opening to public use, the Township of Snyder, at its sole cost and expense, thereafter maintain that portion of the ramp approach extending southwestwardly from State Highway Survey Station 1044/00 of the new main highway hereinabove laid out and established to the existing State Highway Route No. 55, located beyond the northwesterly edge of the pavement on the new main highway.

IT IS FURTHER ORDERED: That, effective upon the completion of the improvement and its opening to public use, the Department of Highways, at its sole cost and expense, thereafter maintain the remainder of the improvement, including the roadway paving on the new bridge herein ordered constructed over the tracks and right of way of The Pennsylvania Railroad Company.

IT IS FURTHER ORDERED: That, effective upon completion of the improvement and its opening to public use that portion of existing State Highway Route No. 55, about 310 feet in length, located between the southerly right-of-way line of Township Road No. 07513 at a point about 190 feet northwest of the center of the present overgrade crossing and the northerly right-of-way line of the new main highway hereinabove laid out and established at a point about 120 feet southeast of the center of the present overgrade crossing, be and is hereby vacated and closed to public use.

IT IS FURTHER ORDERED: That, effective upon the completion of the improvement and its opening to public use, the Department of Highways, at its sole cost and expense, effectively barricade against public travel that portion of existing State Highway Route No. 55 hereinabove vacated and closed to public use; the barricade at the southerly end of the vacated portion of the highway to be thereafter maintained by the Department of Highways, at its sole cost and expense, and the barricade at the northerly end of the vacated portion of the highway to be thereafter maintained by the Township of Snyder.

PENNSYLVANIA PUBLIC UTILITY COMMISSION

(signed) D. J. Driscoll

Chairman

ATTEST:

Secretary.

6334B

PENNSYLVANIA PUBLIC UTILITY COMMISSION

Complaint Docket No. 11005

DEPARTMENT OF HIGHWAYS OF THE  
COMMONWEALTH OF PENNSYLVANIA

v.

THE PENNSYLVANIA RAILROAD COMPANY,  
ALTOONA & LOGAN VALLEY ELECTRIC RAILWAY  
COMPANY, COUNTY OF BLAIR and TOWNSHIP OF SNYDER,  
Blair County.

ORDER

BY THE COMMISSION:

This matter is before us upon amended petition of the Commissioners of Blair County, dated December 4, 1942, and seeking an order directing The Pennsylvania Railroad Company to reimburse the County of Blair for compensation paid by the county for damages for property taken, injured or destroyed in accordance with the provisions of the order of the Pennsylvania Public Utility Commission issued March 20, 1939, at C. 11005.

Our order in that proceeding provides, inter alia, for the abolition of a crossing above grade, where State Highway Route No. 55 crosses four main tracks of the Middle Division of The Pennsylvania Railroad Company at a point in Snyder Township, Blair County, about 200 feet west of the Village of Grazierville, the relocation of a portion of State Highway Route No. 55 and the construction of a crossing above grade, to carry the relocated highway over the grade of the aforementioned tracks of The Pennsylvania Railroad Company.

The improvement as constructed by the Department of Highways of the Commonwealth of Pennsylvania covers a portion of State Highway Route No. 55, approximately 7918 feet in length, extending from State Highway Survey Station 980/02 in Antis Township, Blair County, to State Highway Survey Station 1060/10 in the Village of Grazierville, Snyder Township, Blair County. This Commission assumed jurisdiction over that portion of the improvement, approximately 1773 feet in length, located between State Highway Survey stations

1030/29 and 1048/02.8.

The Commission order issued March 20, 1939, at C. 11005, appropriates property from Fuoss Brothers and Ira L. Grazier, record owners, for the construction of the portion of the improvement over which this Commission assumed jurisdiction. The order further provides that the County of Blair pay all compensation for damages due to the owners, exclusive of The Pennsylvania Railroad Company, for property taken, injured or destroyed in the execution of the improvement, and that The Pennsylvania Railroad Company, having agreed so to do, pay the County of Blair, when and as ordered by the Pennsylvania Public Utility Commission, a sum or sums of money equal to 50% of the actual sum or sums of money paid by the County of Blair in accordance with the order as compensation for damages due to owners of property taken, injured or destroyed by reason of the improvement.

An application of the Commissioners of Blair County, seeking the determination of the amount of compensation for damages due owners of adjacent property taken, injured or destroyed by reason of the construction of the improvement ordered at C. 11005, was filed with this Commission June 10, 1940 and docketed at C. 11005, D. A. 1.

The names of the property owners as set forth in the petition are as follows:

Peoples Natural Gas Company  
The Bell Telephone Company of Pennsylvania  
\*Venetta Bailey, et al  
William Fuoss, et al  
Ira L. Grazier, et ux  
\*John F. Schaffer  
\*May Haupt  
\*Mark T. Kyler, et ux  
\*J. Egbert Holsopple  
\*Stansberry McFarland, et al  
\*William Martin, single  
\*Lyda H. Gault, widow  
\*Everett Shelow, et ux  
\*Harold Wertz, et ux  
\*Gladys Bailey, single  
\*Venetta Bailey, et al  
\*Martha Richardson, widow  
\*Charles H. McClellan, et al  
\*J. W. Hedding, et ux

\*Denotes property owners whose property is located beyond the limits of the portion of the improvement over which this Commission assumed jurisdiction.

Under the provisions of Section 411 of the Public Utility Law the determination of the damages as requested in the application was submitted on June 17, 1940 to the Courts of Blair County, wherein the properties are located.

The amended petition of the Commissioners of Blair County, dated December 4, 1942 and filed with this Commission December 11, 1942 shows that the amount of compensation for damages agreed to be paid, and which have been paid to the property owners and utility companies, is as follows:

|   |               |
|---|---------------|
| Peoples Natural Gas Company                     | \$ 107.22     |
| The Bell Telephone Company of Pennsylvania      | 175.65        |
| Venetta Bailey, et al                           | 719.66        |
| William Fuoss, et al                            | 1,618.40      |
| Ira L. Grazier, et ux                           | 600.00        |
| John P. Schaffer                                | 69.80         |
| Lyda H. Gault, widow                            | 143.74        |
| Stansberry McFarland, et al                     | 1,500.00      |
| William Martin, single                          | 225.00        |
| J. Egbert Holsopple                             | 61.25         |
| May Haupt                                       | 12.50         |
| Mark T. Kyler, et ux                            | 6.25          |
| Everett Shelow, et ux                           | 41.50         |
| Harold Wertz, et ux                             | 6.25          |
| Gladys Bailey, single                           | 3.75          |
| Venetta Bailey, et al                           | 12.00         |
| Martha Richardson, widow                        | 44.24         |
| Charles H. McClellan, et al                     | 73.25         |
| J. W. Hedding, et ux                            | 27.75         |
| Home Electric Light & Steam Heating Company     | 1,854.47      |
| Altoona & Logan Valley Electric Railway Company | <u>550.00</u> |
|   | \$8,258.68    |

The instant petition of the Commissioners of Blair County seeks an order directing The Pennsylvania Railroad Company to reimburse the county the sum of \$4129.84, which is 50% of the total amount of compensation paid as damages by the county by reason of the construction of the improvement.

An application for the determination of damages incurred by Home Electric Light & Steam Heating Company and Altoona & Logan Valley Electric Railway Company has not been filed with this Commission and therefore cannot be considered by the Commission at this time as requested in the amended petition.

A copy of an agreement, attached to the amended petition

and made part thereof, between The Pennsylvania Railroad Company and the Department of Highways of the Commonwealth of Pennsylvania relative to the relocation of State Highway Route No. 55 and the construction of the crossing above grade, involved in C. 11005, shows that The Pennsylvania Railroad Company agrees to contribute toward the improvement a sum equal to 50% of the cost of property and property damages which will be incurred between State Highway Survey stations 1028/00 and 1060/10. Commission approval of this agreement is not required.

This Commission can only require reimbursement for compensation paid for damages for property taken, injured or destroyed on that portion of the improvement over which it assumed jurisdiction. In the aforementioned list the only property owners whose property is located on that portion of the improvement over which this Commission assumed jurisdiction and from whom property was appropriated by the Commission order, and the utility companies entitled to compensation for damages, are as follows:

|  |           |
|--|-----------|
| The Peoples Natural Gas Company            | \$ 107.22 |
| The Bell Telephone Company of Pennsylvania | 221.88    |
| William Fuoss, et al                       | 2,807.00  |
| Ira L. Grazier, et ux                      | 500.00    |

The Court of Common Pleas of Blair County, by order dated July 9, 1940, copy of which is attached to the petition and made part thereof, determined, ordered and directed that the amount of damages to which the aforesaid property owners and utility companies are entitled is as set forth above. Of these amounts the petitioner avers that \$175.65 is the portion of \$221.88 paid as compensation for damages to the property of The Bell Telephone Company of Pennsylvania taken, injured or destroyed by reason of the construction of the portion of improvement over which the Commission assumed jurisdiction, and that \$1618.40 is the portion of \$2807 paid as compensation for damages to the property of William Fuoss, et al, taken, injured or destroyed by reason of the construction of the portion of the improvement over which the Commission assumed jurisdiction.

Accordingly, we are of the opinion that the County of Blair should be reimbursed in the sum of \$1250.63, which is 50% of the sum of \$2501.27, paid by the county as compensation for damages, in accordance with the terms of our order issued March 20, 1939, at C. 11005.

We are satisfied with the representations of the amended petition in so far as it relates to the properties which were appropriated by the Commission for the purpose of the improvement and to the public utility companies entitled to damages, and find that the County of Blair should now be reimbursed by The Pennsylvania Railroad Company for payments made by the county to the extent specified in our order at C. 11005; THEREFORE,

NOW, to wit, January 11, 1943, IT IS ORDERED:

1. That the prayer of the instant petition, in so far as it relates to that portion of the improvement over which this Commission assumes jurisdiction, be and is hereby granted.

2. That The Pennsylvania Railroad Company forthwith pay the County of Blair the sum of Twelve hundred fifty dollars and sixty-three cents (\$1250.63) to apply upon the sum of Twenty-five hundred one dollars and twenty-seven cents (\$2501.27) paid by the County of Blair in satisfaction of the aforementioned claims.

3. That in all other respects the prayer of the application for modification of our aforesaid order be and is hereby denied.

PENNSYLVANIA PUBLIC UTILITY COMMISSION

(signed) John Siggins, Jr.

Chairman

ATTEST:

Secretary

6334

PENNSYLVANIA PUBLIC UTILITY COMMISSION

---

Complaint Docket No. 11005

---

DEPARTMENT OF HIGHWAYS OF THE  
COMMONWEALTH OF PENNSYLVANIA

v.

THE PENNSYLVANIA RAILROAD COMPANY, ALTOONA  
& LOGAN VALLEY ELECTRIC RAILWAY COMPANY,  
COUNTY OF BLAIR, and TOWNSHIP OF SNYDER,  
Blair County

---

O R D E R

---

BY THE COMMISSION:

This matter is before us upon petition of the Commissioners of Blair County seeking an order directing The Pennsylvania Railroad Company to reimburse the County of Blair for compensation paid, by the County, for damages sustained by Home Electric Light & Steam Heating Company by reason of the construction of an improvement in accordance with the terms of the Commission order issued March 20, 1939 in the above entitled proceeding.

Our order in that proceeding provides, inter alia, for the abolition of a crossing above grade, where State Highway Route No. 55 crosses four main tracks of the Middle Division of The Pennsylvania Railroad Company at a point in Snyder Township, Blair County, about 200 feet west of the Village of Grazierville, the relocation of a portion of State Highway Route No. 55 and the construction of a crossing above grade, to carry the relocated highway over the grade of the aforementioned tracks of The Pennsylvania Railroad Company.

The improvement as constructed by the Department of Highways of the Commonwealth of Pennsylvania covers a portion of State Highway Route No. 55, approximately 7,918 feet in length, extending from State Highway Survey Station 980/92 in Antis Township, Blair County, to State Highway Survey Station 1060/10 in the Village of Grazierville, Snyder Township, Blair County. This Commission assumed jurisdiction over that portion of the improvement, approximately 1,773 feet in length, located between State Highway Survey stations 1030/29 and 1048/02.3.

A petition of the Commissioners of Blair County, dated December 4, 1942 and filed with the Commission December 11, 1942, seeking an order directing reimbursement to the county for damages sustained by property owners by reason of the construction of the improvement ordered at C. 11005, shows, inter alia, that the sum of \$1,354.47 is the amount of compensation agreed upon to be paid and was paid to Home Electric Light & Steam Heating Company, for damages sustained by said company in the relocation of facilities of the entire improvement. In the Commission order issued January 11, 1943 at C. 11005 the damages sustained by Home Electric Light & Steam Heating Company, as set forth above, were not considered, since an application for the determination of these damages had not been filed with this Commission.

An application of the Commissioners of Blair County seeking the determination of the damages sustained by Home Electric Light & Steam Heating Company because of the relocation of distribution and transmission lines on that portion of the improvement ordered at C. 11005 over which this Commission assumed jurisdiction was filed with this Commission on February 23, 1943 and docketed at C. 11005, D.A. 2.

Under the provisions of Section 411 of the Public Utility Law, determination of the damages as requested in the application was submitted on March 1, 1943 to the Courts of Blair County, wherein the property is located.

The Court of Common Pleas of Blair County, by order dated May 3, 1943 a copy of which is attached to the instant application of the Commissioners of Blair County, determined the damages to have been suffered by Home Electric Light & Steam Heating Company, as a result of the construction of the portion of the improvement over which the Pennsylvania Public Utility Commission assumed jurisdiction, amounts to \$976.44.

In the instant petition the Commissioners of Blair County request that The Pennsylvania Railroad Company be authorized to make reimbursement to the county 50% of the sum of \$976.44 or \$488.22 as required by the terms of the Commission order issued March 20, 1939 at C. 11005.

We are satisfied with the representation of the instant petition and find that the County of Blair should now be reimbursed by The Pennsylvania Railroad Company for payment claimed therein to the amount specified in our order issued March 20, 1939 at C. 11005; THEREFORE,

Now, to wit, June 23, 1943, IT IS ORDERED:

1. That the prayer of the instant petition of the Commissioners of Blair County be and is hereby granted.

2. That The Pennsylvania Railroad Company forthwith pay the County of Blair the sum of Four hundred eighty-eight dollars and twenty-two cents (\$488.22) to apply upon the sum of Nine hundred seventy-six dollars and forty-four cents (\$976.44) paid by the County of Blair in satisfaction of the aforementioned claim.

PENNSYLVANIA PUBLIC UTILITY COMMISSION

(signed) John Higgins, Jr.

Chairman

ATTEST:

Secretary

PENNSYLVANIA PUBLIC UTILITY COMMISSION

Complaint Docket No. 11005

DEPARTMENT OF HIGHWAYS OF THE  
COMMONWEALTH OF PENNSYLVANIA

v.

THE PENNSYLVANIA RAILROAD COMPANY, ALTOONA  
& LOGAN VALLEY ELECTRIC RAILWAY COMPANY,  
COUNTY OF BLAIR, and TOWNSHIP OF SNYDER,  
Blair County

ORDER

BY THE COMMISSION:

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Now, to wit, June 23, 1943, IT IS ORDERED:

1. That the prayer of the instant petition of the Commissioners of Blair County be and is hereby granted.
2. That The Pennsylvania Railroad Company forthwith pay the County of Blair the sum of Four hundred eighty-eight dollars and twenty-two cents (\$488.22) to apply upon the sum of Nine hundred seventy-six dollars and forty-four cents (\$976.44) paid by the County of Blair in satisfaction of the aforementioned claim.

PENNSYLVANIA PUBLIC UTILITY COMMISSION

(signed) John Siggins, Jr.

Chairman

ATTEST:

Secretary

Penn DOT EXHIBIT NO. 2  
P. U. C. NO. C-844440  
DATE OF HEARING 6-26-85  
M.T.M. G.H.

POOR ORIGINAL

COMMONWEALTH OF PENNSYLVANIA

DEPARTMENT OF TRANSPORTATION

IN-DEPTH INSPECTION & RATINGS OF BRIDGES

DISTRICT 9-0

L.R. NO. 55

(R.R. OH BRIDGE NO. 224.13)

OLD U.S. ROUTE 220

OVER

CONRAIL TRACKS

SNYDER TOWNSHIP, BLAIR COUNTY

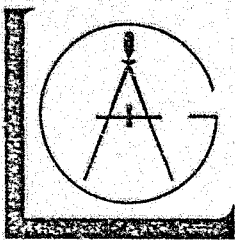
JULY 1982

DOCKETED  
DOCKET  
JUL 22 1985

DOCKETED  
JUL 22 1985

PREPARED BY:

A. G. LICHTENSTEIN & ASSOCIATES, INC.  
CONSULTING ENGINEERS  
LANGHORNE, PA



Fair Lawn, New Jersey  
New York, New York  
Langhorne, Pennsylvania

# A.G. LICHTENSTEIN & ASSOCIATES, INC.

CONSULTING ENGINEERS

One Oxford Valley  
Oxford Valley Mall  
2300 E. Lincoln Highway  
Langhorne, Pennsylvania 19047  
(215) 752-2206

Abba G. Lichtenstein, P.E.  
President

July 12, 1982

Mr. John Matthews, P.E.  
District Engineer  
District 9.0  
Commonwealth of Pennsylvania  
Department of Transportation  
North Juniata Street  
Hollidaysburg, Pennsylvania 16648

Attn: Mr. Walt Bagley

Re: Agreement No. 70449  
In-Depth Inspection and Inventory  
of Bridges over Railroads  
Blair, Cambria and Huntingdon Counties  
Our Project No. 787

Gentlemen:

As per instructions in your letter dated June 9, 1982, we are submitting herewith, five (5) copies each of the Final Reports covering the In-Depth Inspection and Rating of the following nine (9) bridges:

LR 352 State Route 305 - Alexandria  
LR 57 Old State Route 202 - Snyder Township  
LR 55 Old State Route 202 - Snyder Township  
LR 07046 Roots Road - Pinecroft  
LR 07016 Frankstown Township Road - Frankstown  
LR 54 US Route 422 - Blacklick Township  
LR 62 US Route 219 - Ebensburg Boro  
LR 53 State Route 53 - Galitzin Township  
LR 11113 Main Street - Galitzin Boro

We are also including one (1) copy each of our Field Notes and Rating Computations for each bridge for your files.

We trust you will find our submission satisfactory. We wish to thank Mr. Bagley, Mr. Allison and the Engineers from District 9.0 for the excellent cooperation accorded to us during the performance of this Project.

Very truly yours,

A. G. LICHTENSTEIN & ASSOCIATES, INC.

Charles M. Minervino, P.E.  
Project Manager

CM:gs

COMMONWEALTH OF PENNSYLVANIA

DEPARTMENT OF TRANSPORTATION

IN-DEPTH INSPECTION & RATINGS OF BRIDGES

DISTRICT 9-0

L.R. NO. 55

(R.R. OH BRIDGE NO. 224.13)

OLD U.S. ROUTE 220

OVER

CONRAIL TRACKS

SNYDER TOWNSHIP, BLAIR COUNTY

JULY 1982

PREPARED BY:

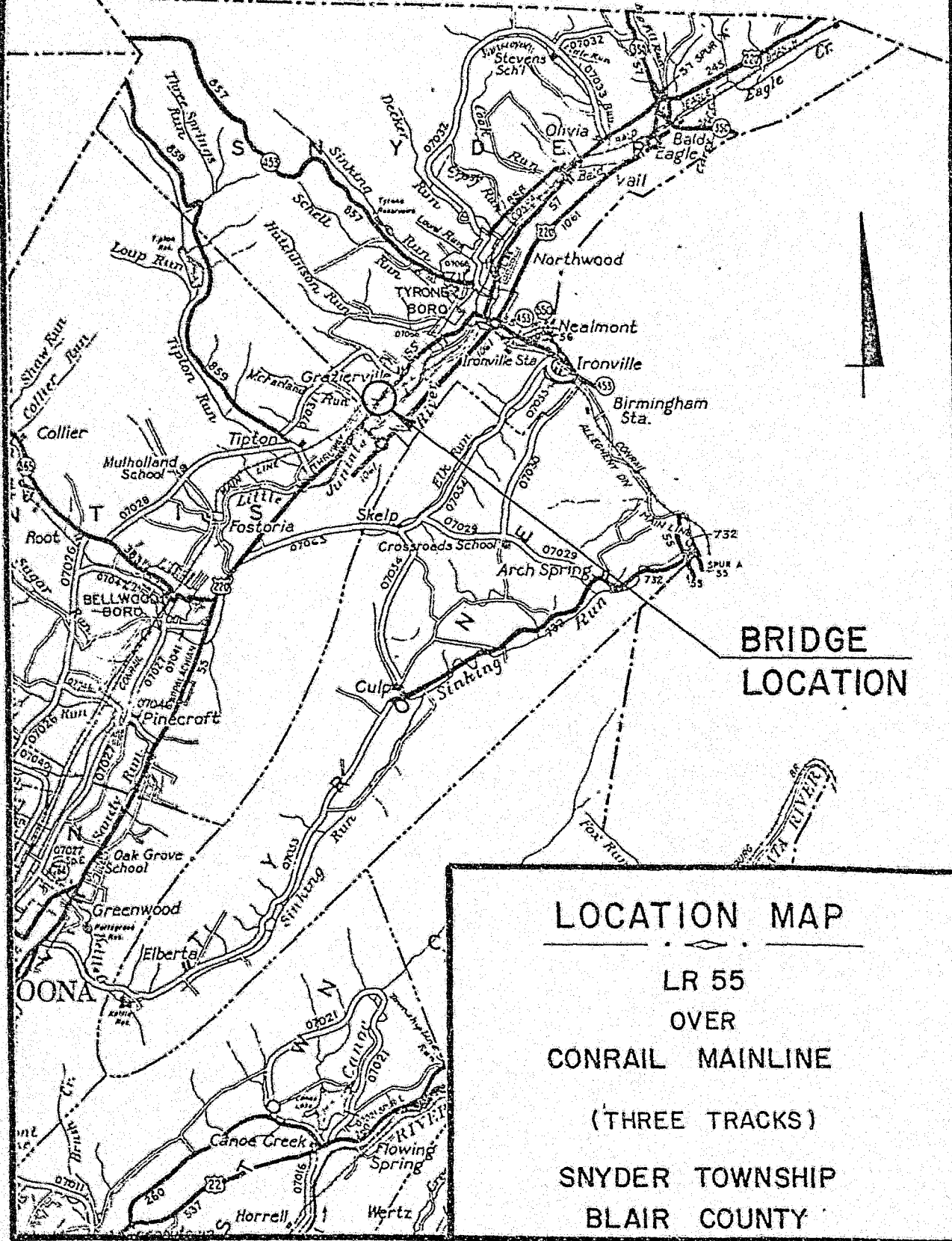
A. G. LICHTENSTEIN & ASSOCIATES, INC.  
CONSULTING ENGINEERS  
LANGHORNE, PA

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MAP

C E N T R E



BRIDGE  
LOCATION

**LOCATION MAP**

— ◆ —

LR 55  
OVER  
CONRAIL MAINLINE  
(THREE TRACKS)  
SNYDER TOWNSHIP  
BLAIR COUNTY

Pennsylvania State University, University Park, PA 16802  
 Department of Geology, University Park, PA 16802  
 Geology 100, University Park, PA 16802  
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 814/863/7200

SUMMARY OF FINDINGS

## SUMMARY OF FINDINGS

A bridge inspection crew from A. G. Lichtenstein & Associates, performed an in-depth inspection of the bridge in November 1981. Access for the inspection of the structural elements was by means of ladders and free-climbing. All components of the structure were inspected for deterioration. At several locations the concrete encasement was removed thereby, enabling a representative check of the member sizes for conformance to the construction drawings which were supplied by Penn Dot. Steel coupon samples were taken for testing purposes from a lateral bracing angle and a lacing bar (truss vertical), both of original construction.

The structure was found to be in overall fair condition. The curbs and sidewalks are severely spalled and there is a lack of an adequate drainage system for the bridge. There is a hole in the pavement at the east shoulder of the roadway at the north deck joint. Numerous areas of the exterior stringer and truss encasement are spalled off and the exposed steel is moderately rusted. The concrete abutments and wing-walls exhibit areas of large spalling. There is soil erosion at the southeast and northwest embankments.

The structural ratings of the bridge are controlled by the interior stringer with the following ratings:

| <u>Vehicle</u> | <u>Inventory Rating</u> | <u>Operating Rating</u> | <u>Safe Load Capacity</u> |
|----------------|-------------------------|-------------------------|---------------------------|
| H20            | 12 Tons                 | 22 Tons                 | -                         |
| HS20           | 20 Tons                 | 38 Tons                 | -                         |
| ML80           | -                       | -                       | 29 Tons                   |

## SUMMARY OF FINDINGS (cont'd)

The deck will require reconstruction from the first interior stringers to the fascias (including the pedestrian sidewalk). A permanent barrier should be added during this work to prohibit vehicle impact to the trusses.

The bituminous overlays have increased the dead load on the structure significantly ( $\pm 20\%$ ) and should be removed down to the top of the concrete deck. Concurrently, an extensive deck evaluation study should be performed to determine the feasibility of providing a new wearing surface vs. an entire new deck.

The following maintenance repairs are also recommended:

- 1) Repair the caved in pavement at the east shoulder of the north deck joint. Reconstruction of the abutment backwall at this location may be necessary.
- 2) Clean and paint all exposed structural steel.
- 3) Repair or replace the expansion bearings to insure proper functioning.
- 4) Areas of concrete spalling should be cleaned and repaired.
- 5) Stabilize the soil erosion at the southeast and northwest embankments.
- 6) Remove all debris from the curb and abutment seats.

DESCRIPTION OF STRUCTURE

DESCRIPTION OF STRUCTURE

The bridge carrying Old U.S. Route 220 (L.R. 55) across CONRAIL tracks in Snyder Township, Blair County on Railroad Overhead Bridge No. 224.13, was originally constructed by the Commonwealth of Pennsylvania Department of Highways in 1936.

The structure has a total length of 242'-1" out to out of backwalls. The bridge is generally aligned in a north-south direction. Presently there are utility lines and three CONRAIL tracks below the structure. At the center pair of rails the minimum vertical under clearance is 21'-3" (measured).

The 44'-0" curb to curb roadway was originally designed to accommodate four (4) - 10'-0" traffic lanes with a 2'-0" shoulder at each curblin. Presently there are 3 lanes on the structure, the 12'-0" center lane utilized as a left-turn-only lane for both southbound and northbound traffic. The shoulders are 3'-0" wide leaving one (1) 13'-0" lane for the north and south traffic flow.

The structure has no posted weight restrictions and vehicle speeds were observed to be approximately 50 MPH.

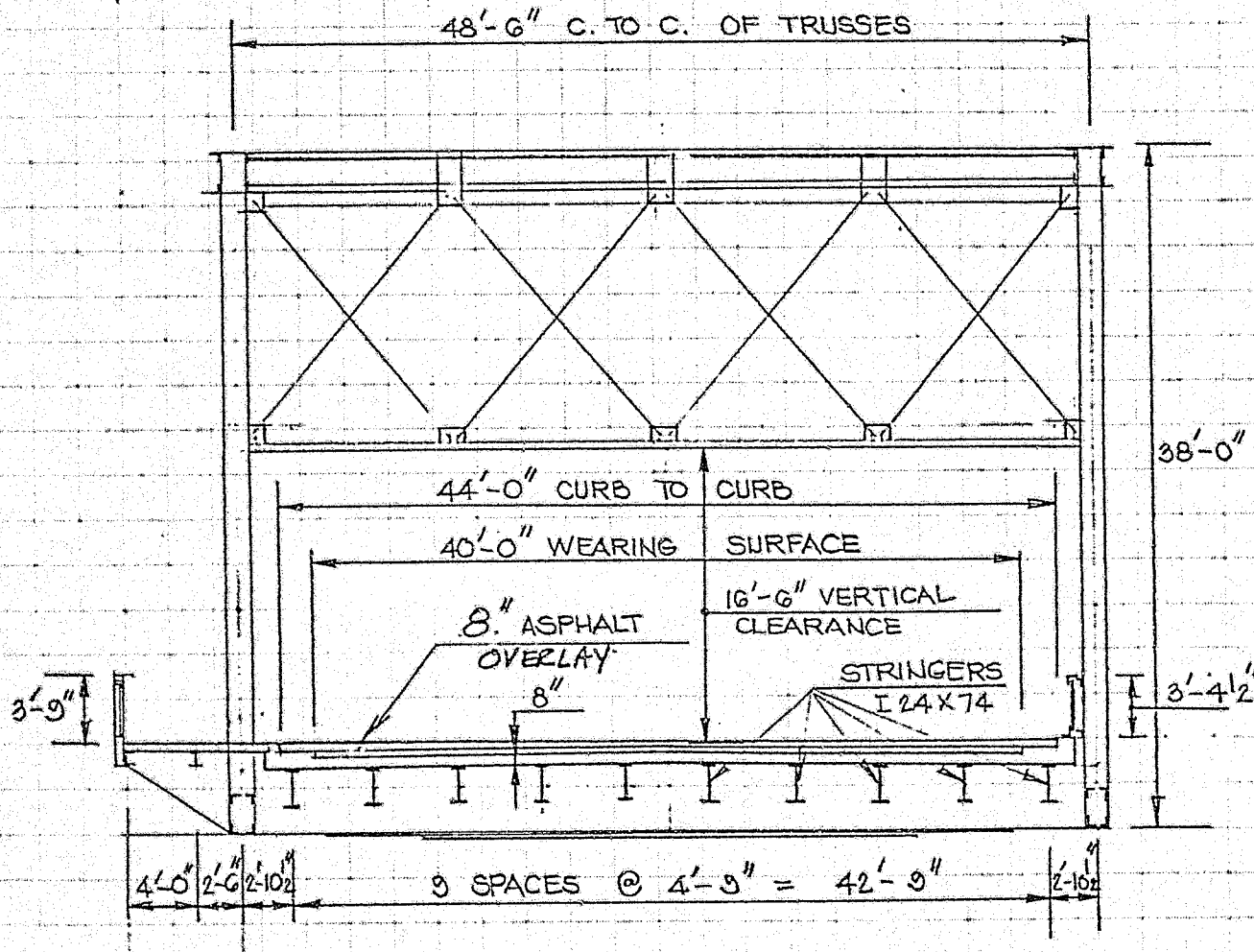
The deck consists of an 8" thick reinforced concrete slab and 8"± of asphalt overlay. The deck is supported by ten (10) 24" I rolled steel stringers at 4'-9" spacing.

DESCRIPTION OF STRUCTURE (cont'd)

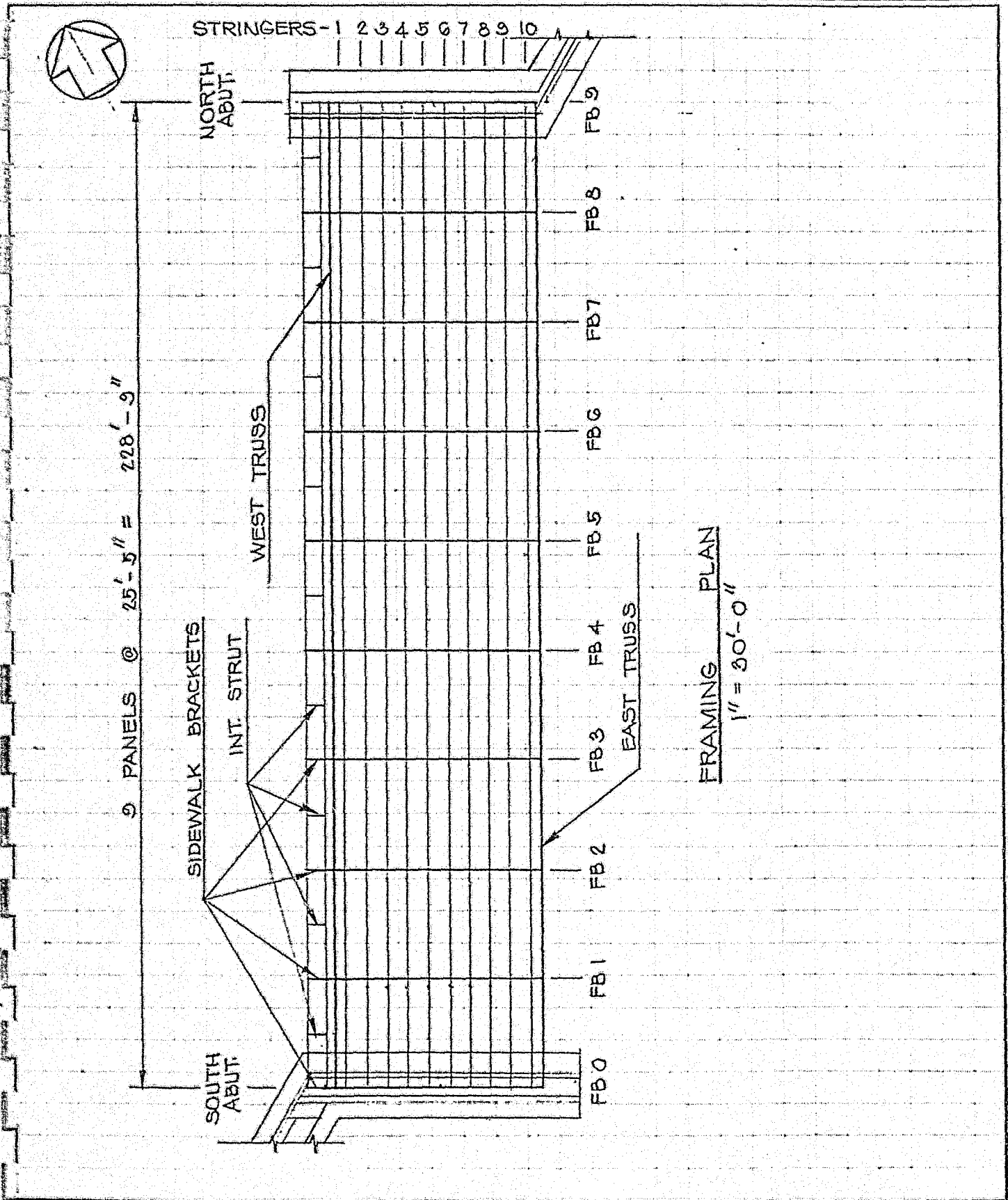
The main members of the superstructure consist of two (2) steel through trusses, 48'-6" on center, spanning 228'-9" between bearings. There is a 5'-9" clear width sidewalk cantilevered from the west truss. The floorbeams are typically spaced at 25'-5" on center. The floorbeams and truss members consist of riveted, built up steel sections with all the structural steel, excluding the bearings, below the deck and sidewalk encased in concrete.

The substructure consists of reinforced concrete abutments and wingwalls.





CROSS SECTION  
1" = 10'-0"



FRAMING PLAN  
1" = 30'-0"

INSPECTION FINDINGS

## INSPECTION FINDINGS

### Approach Roadway

Both approach roadways are aligned tangent to the bridge which is located at the top of a gradual vertical crest curve. (Photo 2) The sight distance appears adequate.

The asphalt wearing surface at each approach is in good condition. No settlement is evident and adequate natural drainage away from the roadway exists.

The approach beam guide rails are in good condition. The west guide rail terminates at the sidewalk parapet rendering the truss end posts exposed to possible vehicle impact. (Photo 3)

Slope erosion at the northwest and southeast embankments is evident. There are no wingwalls at these locations. Vegetation has grown beyond the guide rails.

### Bridge Deck

#### a. Railings, Sidewalk and Curbs

The bridge railing is in generally good condition, exhibiting surface rust throughout. The railing is attached directly to the east truss members and shows evidence of vehicle impact at several locations. A similar pedestrian railing is attached to steel posts at the west edge of the sidewalk which is cantilevered from the west truss. No protective system exists which would prevent vehicle impact to the members of the west truss. (Photo 2)

## INSPECTION FINDINGS (cont'd)

### Bridge Deck (cont'd)

#### a. Railings, Sidewalk and Curbs (cont'd)

The sidewalk is in poor condition. The top surface of the sidewalk is severely scaled throughout and exhibits large spalls at the pedestrian railing anchor posts. (Photo 4) Vegetation growth is common in the heavily accumulated debris. (Photo 5) The concrete has broken away and spalled through at the north sidewalk deck joint. (Photo 6)

The concrete curbs are in poor condition. They are spalled nearly to the level of the asphalt wearing surface.

#### b. Roadway Wearing Surface

The asphalt overlay is in good condition except at the curb-lines where its condition is marginal. No as-designed thru the deck drainage system exists. Excessive debris accumulation and vegetation growth is typical at the curblines. The deck has holed thru at several locations (Bays  $L_0L_1$  &  $L_8L_9$ ) along the west curbline exposing steel reinforcement. (Photo 7) The deck exhibits large spalls along the east fascia. Typically no cover remains on the deck reinforcing steel at these locations. There are medium to wide transverse cracks in the asphalt pavement at the deck joints. (Photo 3) There is an approximately 2' x 1' cave-in of the east shoulder pavement at the north deck joint. (Photo 8)

Bridge Deck (cont'd)

c. Underside of Deck

The underside of the reinforced concrete deck is in generally fair condition except for the areas below the curblines and at the east fascia. There are numerous fine cracks with leaching and efflorescence; however, hammer sounding determined the concrete to be sound. Below the curblines random heavy spalling is evident. (Photo 9) Several areas of the deck have holed-thru along the west curbline in bays  $L_0L_1$  &  $L_8L_9$ , resulting in exposed and deteriorated deck reinforcement. (Photo 10) Spalling of the concrete has extended to the underside of the deck along the east fascia. (Photo 11)

Superstructure

a. Encased Stringers, Floorbeams and Lower Chord Truss Members

The floorbeam and stringer encasements are in good condition and solid except for the exterior stringers. The exterior stringers are located below each curbline and the bottom flange encasement is "hollow" when sounded with a hammer. (Photo 12) The truss encasement of the bottom chord is typically "hollow" when sounded with a hammer. Areas of encasement in the vicinity of the connections are spalled and the wire mesh is exposed and rusted. The encasement is separated from the exterior gusset plates at several locations. (Photo 13)

Removal of the encasement by chipping hammer was performed in representative areas to check member sizes for conformance with the design drawings. The exposed steel exhibits red lead paint.

Superstructure (cont'd)

b. Non-Encased Structural Steel

The truss members above the level of the concrete deck are exposed. The truss members and their bracings are in good condition, with moderate surface rust throughout. Several vertical truss members have debris accumulations at the roadway level. (Photo 14)

The exposed fixed truss bearings are in generally good condition with moderate to severe rusting and debris build-up. The base plates have full bearing and anchorage. The expansion bearings which are moderately rusted appear to be frozen; however, adequate room for expansion exists. (Photo 15)

Substructure

The substructure is in generally fair condition. The bridge seats have accumulated much debris in the vicinity of the truss bearings. The abutments exhibit numerous large areas of leaching, hollow concrete, spalls, and rusted, exposed steel reinforcement. (Photo 16) Numerous spalls, areas of hollow concrete, and leaching occur throughout the bridge seats and backwalls, especially at the southwest and northeast truss bearings. Steel reinforcement is exposed and heavily rusted at these locations. (Photo 17) There are numerous large spalls on the wingwalls. There is a 1" gap at the vertical construction joint of the southwest wingwall, however, no differential settlement was noted.

STRUCTURAL RATING

## STRUCTURAL RATING

This structure was originally constructed in 1939 to support four traffic lanes according to the plans made available by Penn Dot. Presently the structure, over 3 main line CONRAIL tracks, supports approximately 5½" of additional bituminous material, evidence of successive overlays made since original construction, and is currently striped for three traffic lanes.

A structural analysis and rating was performed on each major component of the structure in accordance with the AASHTO Manual for Maintenance Inspection of Bridges, 1978, as modified by the Pennsylvania Department of Transportation. Accordingly, the vehicles used in computing ratings were AASHTO H20 and HS20 type trucks and a maximum legal vehicle as specified by Penn Dot and designated ML80. The ratings for the ML80 load type were computed for the operating level (Safe Load Capacity). The structural members were loaded with three traffic lanes, corresponding to present conditions for the ML80 loading. The H20 and HS20 load types were located for maximum stresses in four traffic lanes corresponding to the as-built condition. A comparison of 3 lanes vs. 4 lanes-loaded revealed a difference in live load moments of +3% (floor beams ratings) and -4% (truss member ratings). This slight difference is within the realm of assumptions and the ensuing ratings may be regarded as indicative of both a 3 or 4 lane condition per vehicle type.

STRUCTURAL RATING (cont'd)

Steel coupon samples, removed from a lateral bracing angle and a lacing bar (truss vertical), were tested for yield and ultimate stresses. Based on the results of these tests and guidelines set forth in the Maintenance Manual, the following allowable stresses for all structural steel components were chosen:

(1936 construction)

Inventory - 18,000 psi

Operating - 24,500 psi

Results of the steel coupon tests are included in this section of the report.

STRINGERS

Fascia Stringer (As Inspected)

| <u>Vehicle</u> | <u>Weight</u> | <u>Inventory</u> | <u>Operating</u> | <u>Safe Load Capacity</u> |
|----------------|---------------|------------------|------------------|---------------------------|
| H20            | 20 Tons       | 15 Tons          | 26 Tons          | -                         |
| HS20           | 36 Tons       | 25 Tons          | 44 Tons          | -                         |
| ML80           | 37.74 Tons    | -                | -                | 34 Tons                   |

Interior Stringer (As Inspected)

| <u>Vehicle</u> | <u>Weight</u> | <u>Inventory</u> | <u>Operating</u> | <u>Safe Load Capacity</u> |
|----------------|---------------|------------------|------------------|---------------------------|
| H20            | 20 Tons       | 12 Tons          | 22 Tons          | -                         |
| HS20           | 36 Tons       | 20 Tons          | 38 Tons          | -                         |
| ML80           | 37.74 Tons    | -                | -                | 29 Tons                   |

Interior Stringer (As Built-excess overlay loading removed)

| <u>Vehicle</u> | <u>Weight</u> | <u>Inventory</u> | <u>Operating</u> | <u>Safe Load Capacity</u> |
|----------------|---------------|------------------|------------------|---------------------------|
| H20            | 20 Tons       | 16 Tons          | 26 Tons          | -                         |
| HS20           | 36 Tons       | 28 Tons          | 45 Tons          | -                         |
| ML80           | 37.74 Tons    | -                | -                | 35 Tons                   |

STRUCTURAL RATING (cont'd)

FLOORBEAMS

Interior Floorbeams

| <u>Vehicle</u> | <u>Weight</u> | <u>Inventory</u> | <u>Operating</u> | <u>Safe Load Capacity</u> |
|----------------|---------------|------------------|------------------|---------------------------|
| H20            | 20 Tons       | 18 Tons          | 38 Tons          | -                         |
| HS20           | 36 Tons       | 23 Tons          | 49 Tons          | -                         |
| ML80           | 37.74 Tons    | -                | -                | 40 Tons                   |

End Floorbeams

| <u>Vehicle</u> | <u>Weight</u> | <u>Inventory</u> | <u>Operating</u> | <u>Safe Load Capacity</u> |
|----------------|---------------|------------------|------------------|---------------------------|
| H20            | 20 Tons       | 177 Tons         | 249 Tons         | -                         |
| HS20           | 36 Tons       | 245 Tons         | 344 Tons         | -                         |
| ML80           | 37.74 Tons    | -                | -                | 300 Tons                  |

TRUSS MEMBERS

Upper Chord U<sub>2</sub>U<sub>3</sub>, U<sub>6</sub>U<sub>7</sub>

| <u>Vehicle</u> | <u>Weight</u> | <u>Inventory</u> | <u>Operating</u> | <u>Safe Load Capacity</u> |
|----------------|---------------|------------------|------------------|---------------------------|
| H20            | 20 Tons       | 39 Tons          | 119 Tons         | -                         |
| HS20           | 36 Tons       | 41 Tons          | 124 Tons         | -                         |
| ML80           | 37.74 Tons    | -                | -                | 126 Tons                  |

Lower Chord L<sub>3</sub>L<sub>4</sub>, L<sub>5</sub>L<sub>6</sub>

| <u>Vehicle</u> | <u>Weight</u> | <u>Inventory</u> | <u>Operating</u> | <u>Safe Load Capacity</u> |
|----------------|---------------|------------------|------------------|---------------------------|
| H20            | 20 Tons       | 49 Tons          | 170 Tons         | -                         |
| HS20           | 36 Tons       | 52 Tons          | 178 Tons         | -                         |
| ML80           | 37.74 Tons    | -                | -                | 181 Tons                  |

STRUCTURAL RATING (cont'd)

TRUSS MEMBERS (cont'd)

Verticals U<sub>1</sub>L<sub>1</sub>, U<sub>8</sub>L<sub>8</sub>

| <u>Vehicle</u> | <u>Weight</u> | <u>Inventory</u> | <u>Operating</u> | <u>Safe Load Capacity</u> |
|----------------|---------------|------------------|------------------|---------------------------|
| H20            | 20 Tons       | 41 Tons          | 79 Tons          | -                         |
| HS20           | 36 Tons       | 53 Tons          | 101 Tons         | -                         |
| ML80           | 37.74 Tons    | -                | -                | 90 Tons                   |

Diagonals U<sub>1</sub>L<sub>2</sub>, L<sub>7</sub>U<sub>8</sub>

| <u>Vehicle</u> | <u>Weight</u> | <u>Inventory</u> | <u>Operating</u> | <u>Safe Load Capacity</u> |
|----------------|---------------|------------------|------------------|---------------------------|
| H20            | 20 Tons       | 37 Tons          | 132 Tons         | -                         |
| HS20           | 36 Tons       | 39 Tons          | 137 Tons         | -                         |
| ML80           | 37.74 Tons    | -                | -                | 141 Tons                  |

The remainder of the truss members rate in excess of those published above.

Reinforced Concrete Deck

The allowable stresses for the concrete and steel reinforcement (as per AASHTO Maintenance Manual, 1978) are:

|                   | <u>Inventory</u> | <u>Operating</u> |
|-------------------|------------------|------------------|
| Concrete          | 800 psi          | 1,100 psi        |
| Reinforcing Steel | 18,000 psi       | 25,000 psi       |

Reinforced Concrete Deck Slab

Total stresses due to 16<sup>k</sup> wheel load (DL+LL+I):

|                   |                   |
|-------------------|-------------------|
| Concrete          | $f_c = 11516$ psi |
| Reinforcing Steel | $f_s = 662$ psi   |

New York Division  
C. T. L. INC.  
2626 Halpenn Avenue Bronx 61, N.Y.  
(Area Code 212) 824-1616

South Jersey Division  
323 Black Horse Pike Pleasantville, N.J.  
(Area Code 609) 298-3255

New Jersey, Main Office  
CERTIFIED TESTING LABORATORIES, INC.  
155 U. S. Route 130, Bordentown, N. J.  
(Area Code 609) 298-3255

Pennsylvania Division  
ALLENTOWN TESTING LABORATORY, INC.  
754 East Fairview Street Bethlehem, Pa.  
(Area Code 215) 855-2674

Central Pennsylvania Division  
ASTROTECH INC.  
7801 Allentown Blvd. U.S. 22 Harrisburg, Pa.  
(Area Code 717) 652-1750

# CERTIFIED TESTING LABORATORIES, INC.

Inspection -- Testing -- Research -- Chemical Analysis -- Industrial Radiology -- Soils -- X-Ray -- Water -- Steel

CLIENT: A. G. Lichtenstein & Associates, Inc.  
PROJECT: #787  
SUBJECT: Steel Coupons  
TEST REQUIRED: Yield, Tensile & Elongation  
DATE: January 12, 1982  
LAB. NO. B-1213-SSS-42

| SPECIMEN | AREA  | YIELD STRENGTH LBS. | MAXIMUM LOAD LBS. | YIELD STRENGTH LBS. PER SQ. INCH. | TENSILE STRENGTH LBS SQ. INCH. | ELONGATION % |
|----------|-------|---------------------|-------------------|-----------------------------------|--------------------------------|--------------|
| *07046   | .4116 | 13,000              | 17,000            | 31,584                            | 41,302                         | 3%           |
| 07016S   | .3880 | 12,000              | 15,000            | 30,927                            | 38,659                         | 30%          |
| 11113T   | .4806 | 17,000              | 31,500            | 35,372                            | 65,543                         | 30%          |
| LR11113B | .4989 | 18,000              | 27,000            | 36,079                            | 54,119                         | 40%          |
| 352S     | .4113 | 12,000              | 25,000            | 29,175                            | 60,782                         | 30%          |
| 352      | .5439 | 24,000              | 33,000            | 44,125                            | 60,672                         | 30%          |
| LR55Bx   | .5018 | 22,000              | 27,000            | 43,842                            | 53,806                         | 29%          |
| LR53     | .4220 | 20,000              | 28,000            | 47,393                            | 66,350                         | 38%          |
| 720G     | .3154 | 13,000              | 20,000            | 41,217                            | 63,411                         | 32%          |
| 07016F   | .9502 | 37,000              | 56,000            | 38,939                            | 58,934                         | 44%          |
| LR57     | .5145 | 26,000              | 33,000            | 50,534                            | 64,139                         | 30%          |
| LR720B   | .3949 | 11,000              | 21,500            | 27,855                            | 54,444                         | 38%          |
| LR55P    | .4436 | 20,000              | 31,000            | 45,085                            | 69,882                         | 34%          |

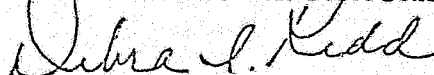
\*Coupon did not break in cut-out area.

NOTE: Coupons have been retained for pick-up at Certified Testing Labs., Inc. office.

Reported to: A.G. Lichtenstein & Associates

/ksr

Respectfully Submitted,  
CERTIFIED TESTING LABORATORIES, INC.



Debra I. Kidd

CONCLUSIONS

## CONCLUSIONS

The structure is in fair condition. The load carrying capacities of the structural components have been reduced from those of original construction with the addition of successive bituminous overlays to the deck. The lack of preventive maintenance has created localized deteriorations. The computed ratings for the critical member, the interior stringers, in the as inspected condition are as follows:

|      | <u>Inventory</u> | <u>Operating</u> | <u>Safe Load Condition</u> |
|------|------------------|------------------|----------------------------|
| H    | 12 Tons          | 22 Tons          | -                          |
| HS   | 20 Tons          | 38 Tons          | -                          |
| ML30 | -                | -                | 29 Tons                    |

The following items require attention:

1. The underside of the deck exhibits fine cracks and leaching throughout, with evidence of moisture seepage thru the deck. This situation is more prominent below each curbline where numerous large spalls in the underside of the deck occur. The structure has no as-built thru-the-deck drainage system. Runoff is intended to follow the roadway profile to the curblines and in turn flow towards the ends of the span.
- 2.. The fascia stringers and floorbeam/truss connections typically exhibit hollow gunite encasement. These members are further deteriorated by the effects of runoff seepage thru the deck at the curblines.
- 3.. The deck at the ends of the span at the west curbline has spalled, and has holed thru at numerous locations. The east curbline at the north deck joint has caved in at one location. Transverse cracks occur at the deck joints. Runoff is directed to the sub-structure thereby accelerating deteriorations to the bearings, backwalls, bridge seats and abutments.

CONCLUSIONS (cont'd)

4. The  $\pm 5\frac{1}{2}$ " of bituminous overlays has reduced the height of the curbs to  $\pm 3\frac{1}{2}$ ". In addition, the curbs are heavily spalled which further reduces their effective height. As a result, the curbs are highly inadequate to confine traffic flow to within the lanes. Debris hinders runoff towards the ends of the span.
5. No protective system exists which will prevent vehicle impact to the members of the west truss. The bridge railing, directly attached to the east truss does not conform to current safety specifications.
6. The sidewalk exhibits severe deterioration aided by freeze-thaw action. A possible hazard to pedestrian crossings may exist.
7. The superstructure steel is moderately rusted but no reductions in sections have occurred.
8. The vertical construction joint in the southwest wingwall is 1" wide indicating possible lateral movement (as-built  $\frac{1}{8}$ ").

RECOMMENDATIONS

## RECOMMENDATIONS

Based on the results of the field inspection, condition of the structural components, and the computed Inventory and Operating Ratings, we recommend the following:

1. The computed load capacities for the controlling member, the interior stringers, are:

|      | <u>Inventory</u> | <u>Operating</u> | <u>Safe Load Capacity</u> |
|------|------------------|------------------|---------------------------|
| H    | 12 Tons          | 22 Tons          | -                         |
| HS   | 20 Tons          | 38 Tons          | -                         |
| ML80 | -                | -                | 29 Tons                   |

These ratings indicate that a restrictive load posting should be considered by the DOT.

2. The curblines are in need of repairs. This would involve reconstruction of the concrete deck from the first interior stringer to the fascia at the east truss and similarly include the pedestrian sidewalk at the west truss. A "Jersey-Type" safety shape barrier should be incorporated in the design to safeguard against vehicle impact to the trusses.
3. The bituminous material should be removed down to the top of the concrete deck thereby reducing the dead load on the structure. This would increase the load capacity of the interior stringers to 16 tons for H and 28 tons for HS trucks based on the Inventory levels. Concurrently, a complete deck evaluation study should be performed to determine the feasibility of providing a new wearing surface (i.e. L.M.C.) vs. an entire new deck to the area

RECOMMENDATIONS (cont'd)

of the deck between curbline repairs. The extent to which encasements will require removal should be given consideration at this time. Reconstruction of the expansion deck joints should be included in the final schedule of repairs.

4. The following items are in need of maintenance work:
  - a. Repair the hole in the pavement at the northeast corner of the bridge. This will include repairs to the concrete backwall at this location.
  - b. Clean and paint all exposed structural steel.
  - c. Repair or replace the expansion bearings to insure their proper function.
  - d. All concrete elements of the substructure should be cleaned of loose material and an epoxy mortar applied to prevent further deterioration. Special attention should be given to the bridge seats where large spalls have exposed the steel reinforcement.
  - e. Monitor the expansion joint in the southwest wingwall for further lateral displacement.
  - f. Stabilize the erosion at the southeast and northwest embankments.
  - g. Remove vegetation which has grown beyond the approach shoulders.

APPENDIX 1 - STRUCTURE INVENTORY & APPRAISAL SHEET



All items contained in FHWA Structure Inventory & Appraisal Sheet (Short Form) dated Jan. '79 are indicated thus O.

### STRUCTURE INVENTORY & APPRAISAL SHEET

#### IDENTIFICATION

1 State Pennsylvania  
 2 Hwy District 9-0  
 3 County BLAIR 07 4 City/Town SNYDERTOWN 7866  
 5 Inventory Route 11211101012121010 On  Under   
 6 Features Intersected CORAIL

#### CLASSIFICATION

24 Fed. Aid System \_\_\_\_\_  
 25 Administrative \_\_\_\_\_  
 26 Functional \_\_\_\_\_

|                    | By | Date |
|--------------------|----|------|
| Transfer of Data   |    |      |
| Maintenance Insp.  |    |      |
| Condition Analysis |    |      |
| Appraisal          |    |      |
| Cost Estimate      |    |      |
| General Review     |    |      |

7 Facility Carried by Structure OLD U.S. ROUTE 220  
 8 Structure No. 10171010101531111111  
 9 Location 4 MILES WEST OF LITTLE JUNITA RIVER  
 10 Min. Vert. Clearance, Inv. Rts. 9999  
 11 Milepoint \_\_\_\_\_  
 12 Road Section No. \_\_\_\_\_  
 13 Defense Bridge Description \_\_\_\_\_  
 14 Defense Milepoint \_\_\_\_\_  
 15 Defense Section Length \_\_\_\_\_  
 16 Latitude 40° 38' 59"  
 17 Longitude 78° 16' 11"  
 18 Physical Vulnerability   
 19 By pass, Detour Length STRUCTURE OVER RR 5.5 MI.  
 20 Toll Bridge  On Toll Road  On Free Road   
 21 Custodian \_\_\_\_\_  
 22 Owner \_\_\_\_\_  
 23 F.A.P. No. \_\_\_\_\_

#### STRUCTURE DATA

27 Year Built 3900  
 28 Lanes on Str. 03 under 00  
 29 ADT \_\_\_\_\_ 30 Year \_\_\_\_\_  
 31 Design Load 0  
 32 Appr. Rdwy. Width w/Sh'd 052  
 33 Br. Median  None  Open  Closed  
 34 Skew 00  
 35 Structure Flared  Yes  No  
 36 Traffic Safety Features 100119  
 37 \_\_\_\_\_  
 38 Navigation Control  Yes  No  
 39 -Vertical \_\_\_\_\_ ft  
 40 -Horizontal \_\_\_\_\_ ft  
 41 Struc.  OPEN  CL  ROSTLD

42 Type Service \_\_\_\_\_ 1512  
 43 Structure Type-Main 131110  
 44 -Approach 101010  
 45 No. of Sp. to Main 001  
 46 -Approach 0000  
 47 Total Horiz. Clear. 46 0 ft  
 48 Max. Span Length 00227 ft  
 49 Structure Length 000242 ft  
 50 Sidewalk: Ft. 00 0 ft, Lt. 05 6 ft  
 51 Br. Rdwy. Width (curb-curb) 46 0 ft  
 52 Deck Width (out-out) 046 0 ft  
 53 Vert. Clear. over Deck 16.06 ft  
 54 Underclearance-Vert. 21.3 ft  
 55 -Lateral-Right 10.8 ft  
 56 -Left 24.6 ft  
 57 Wearing'g' mts ASPHALT

#### CONDITION

| Item                            | Material                               | Condition Analysis                                  | Rating (9-0) |
|---------------------------------|--|---|--------------|
| 58 Deck                         | <u>R. CONC. SLAB, ASPHALT W.S.</u>     | <u>AREAS OF LARGE SPALLING &amp; EXPOSED REBARS</u> | <u>3</u>     |
| 59 Superstructure               | <u>STEEL THRU TRUSS</u>                |   | <u>6</u>     |
| 60 Substructure                 | <u>R. CONC. ABUTS. &amp; WINGWALLS</u> | <u>AREAS</u>  | <u>5</u>     |
| 61 Channel & Channel Protection |  |   | <u>NA</u>    |
| 62 Culvert & Retaining Walls    |  |   | <u>NA</u>    |
| 63 Estimated Remaining Life     |  |   | <u>7</u>     |
| 64 Operating Rating             | <u>38 TONS - HS</u>                    | 65 Approach Roadway Alignment <u>ADEQUATE</u>       |              |
|                                 |  | 66 Inventory Rating <u>20 TONS - HS</u>             |              |

#### APPRAISAL

| Item                                  | Deficiencies                                    | Rating (9-0) |
|---------------------------------------|---|--------------|
| 67 Structural Condition               | <u>DECK DETEIORATION LOW STRUCTURAL RATINGS</u> | <u>4</u>     |
| 68 Deck Geometry                      |   | <u>4</u>     |
| 69 Underclearances-Vertical & Lateral | <u>ADEQUATE</u>                                 | <u>5</u>     |
| 70 Safe Load Capacity                 | <u>29 T (MLBO - OPERATING RATING)</u>           | <u>4</u>     |
| 71 Waterway Adequacy                  |   | <u>NA</u>    |
| 72 Approach Roadway Alignment         |   | <u>7</u>     |

#### PROPOSED IMPROVEMENTS

73 Year Needed \_\_\_\_\_ Completed \_\_\_\_\_ Describe (Item 75) \_\_\_\_\_  
 74 Type of Service \_\_\_\_\_  
 75 Type of Work \_\_\_\_\_  
 76 Improvement Length \_\_\_\_\_ ft  
 77 Design Loading \_\_\_\_\_  
 78 Roadway Width \_\_\_\_\_ ft  
 79 Number of Lanes \_\_\_\_\_ 82 Prop. Rdwy. Improvement Year \_\_\_\_\_  
 80 ADT \_\_\_\_\_ 81 Year \_\_\_\_\_ 83 \_\_\_\_\_ -Type \_\_\_\_\_

#### COST OF IMPROVEMENTS

84 \$ \_\_\_\_\_, \_\_\_\_\_, 000.  
 85 Prelim. Eng. Cost \$ \_\_\_\_\_  
 86 Demolition Cost \$ \_\_\_\_\_  
 87 Substructure Cost \$ \_\_\_\_\_  
 88 Superstructure Cost \$ \_\_\_\_\_  
 89 Inspection Date \_\_\_\_\_

#### REMARKS:

\_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_

APPENDIX 2 - PHOTOGRAPHS

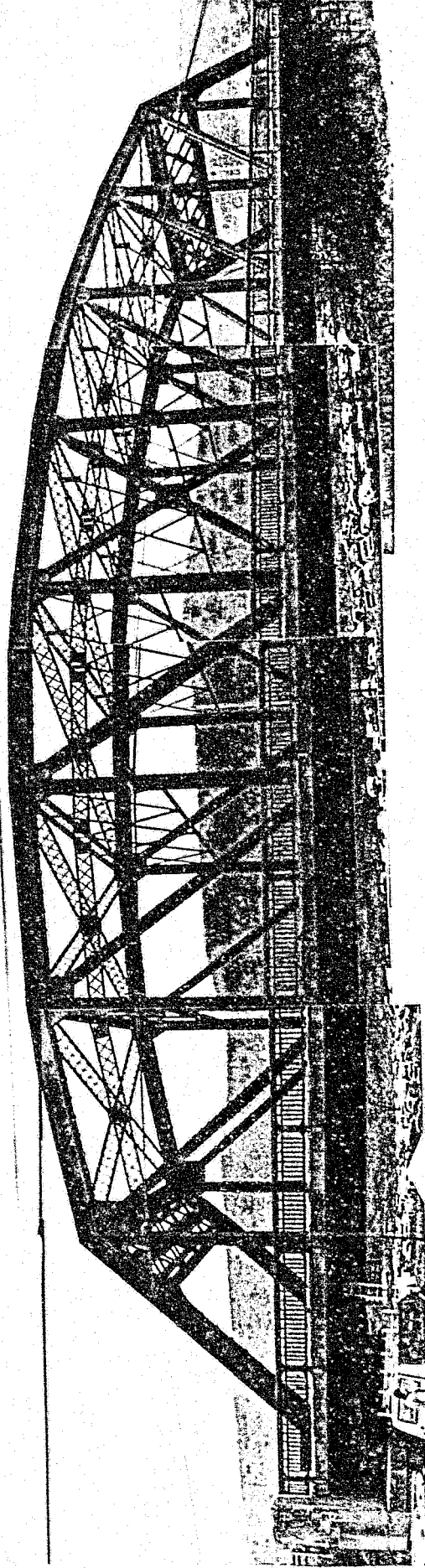


PHOTO 1 WEST ELEVATION

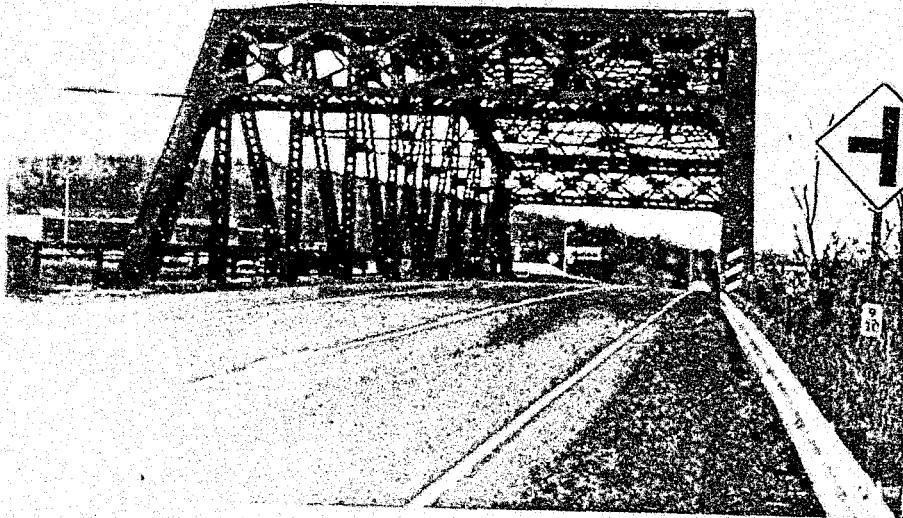


PHOTO 2

ROADWAY PLAN LOOKING NORTH



PHOTO 3

GUIDE RAIL  
TERMINAL AND  
TRANSVERSE CRACK  
AT NORTH DECK  
JOINT PAVEMENT

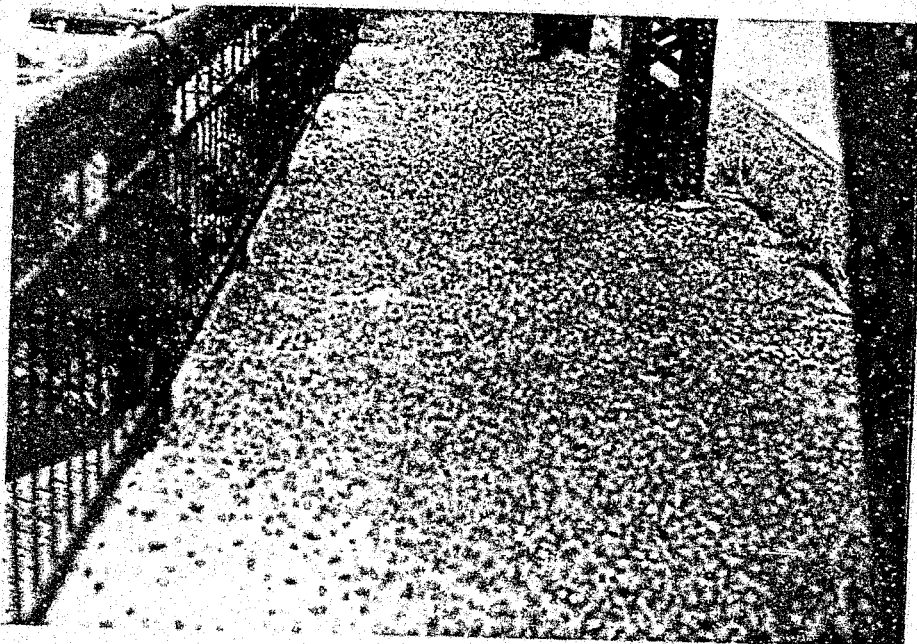


PHOTO 4

SEVERELY SCALED SIDEWALK TOP SURFACE

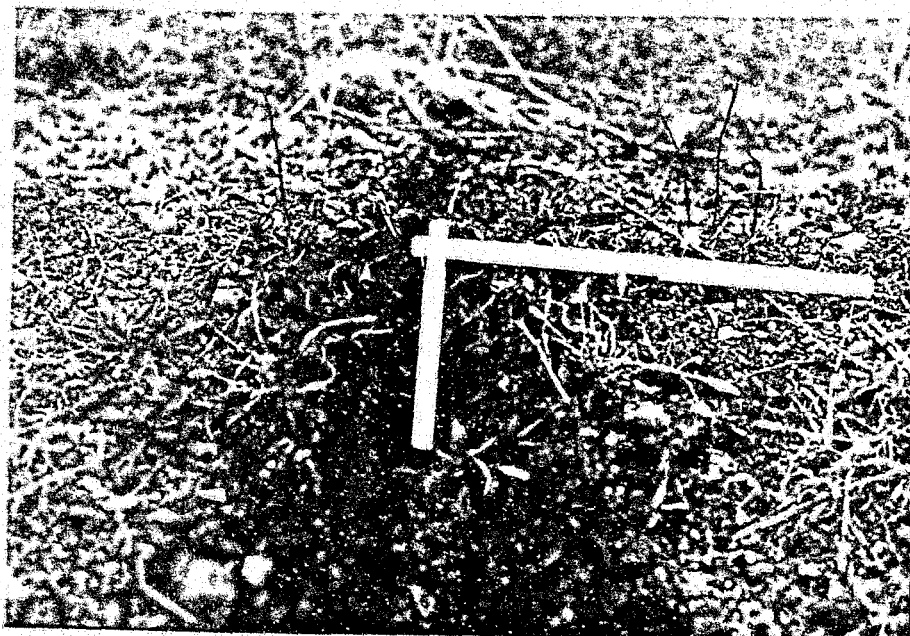


PHOTO 5

LARGE DEBRIS ACCUMULATION AT WEST TRUSS

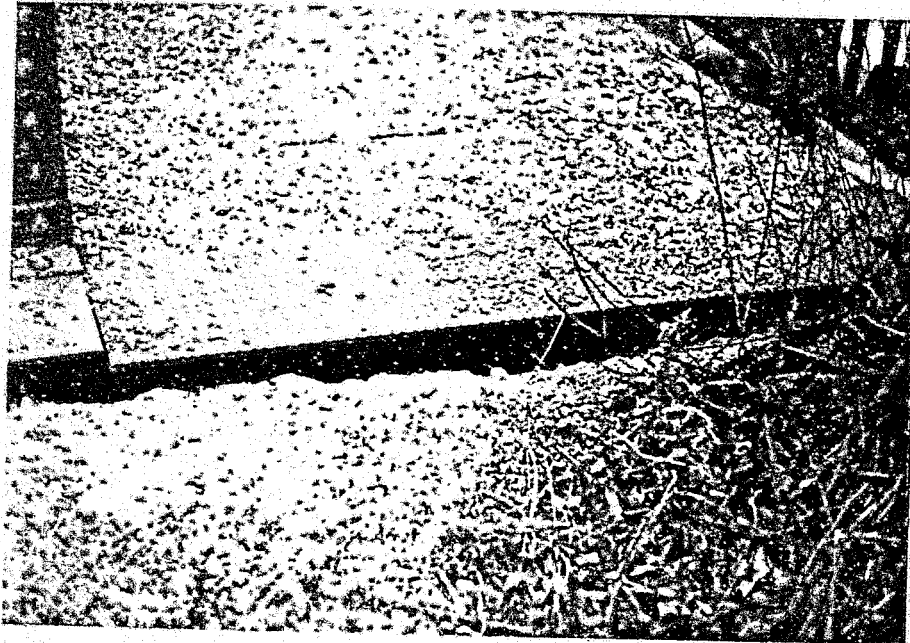


PHOTO 6

LARGE SPALLING AT NORTH DECK JOINT AT SIDEWALK

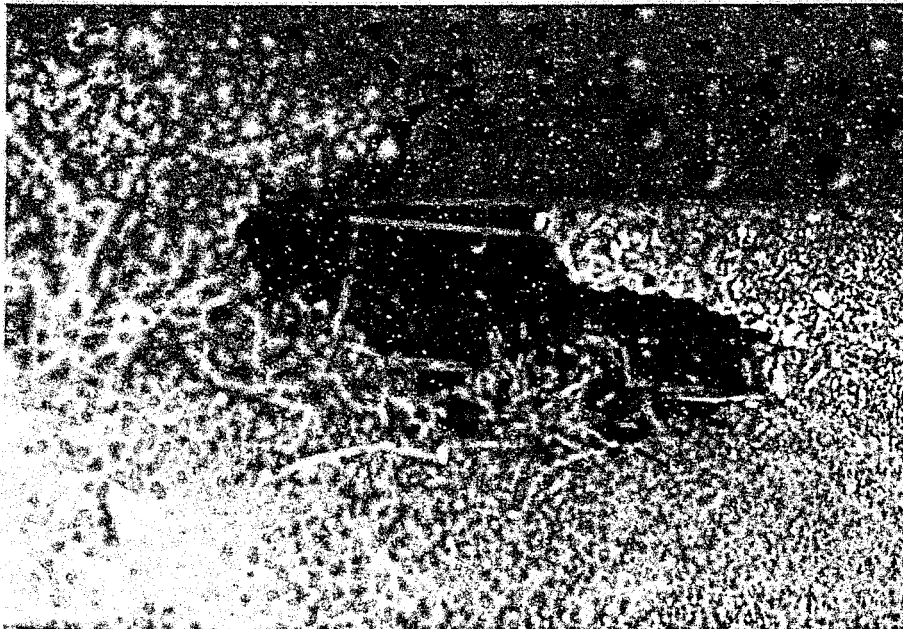


PHOTO 7

LARGE SPALLING  
OF WEST CURB WITH  
EXPOSED REBARS

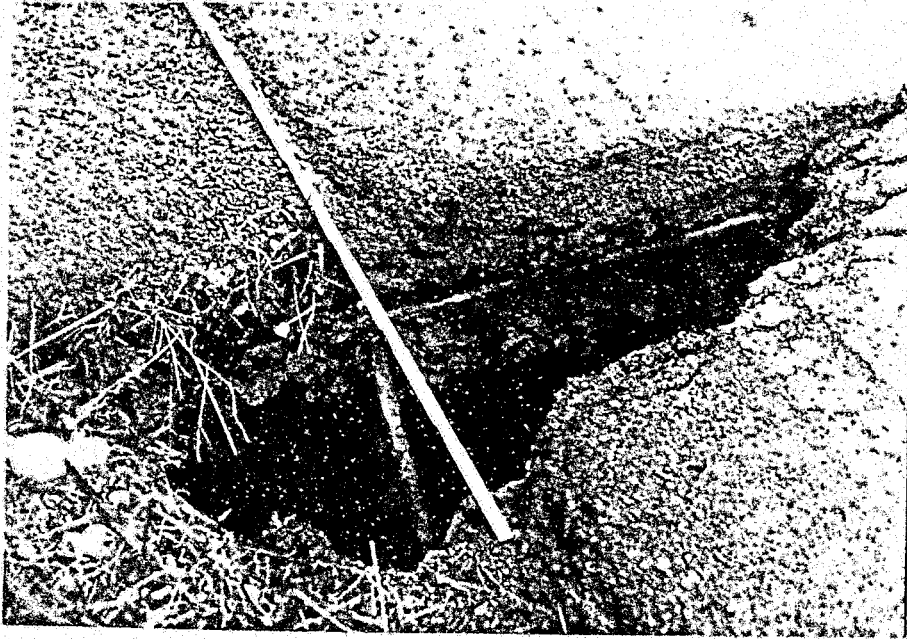


PHOTO 8

HOLE AT EAST SHOULDER AT NORTH DECK JOINT



PHOTO 9

TYPICAL SPALLED UNDERSIDE OF DECK BELOW EAST CURB



PHOTO 10

DECK UNDERSIDE AT WEST CURBLINE (VIEW FROM BELOW SIDEWALK)

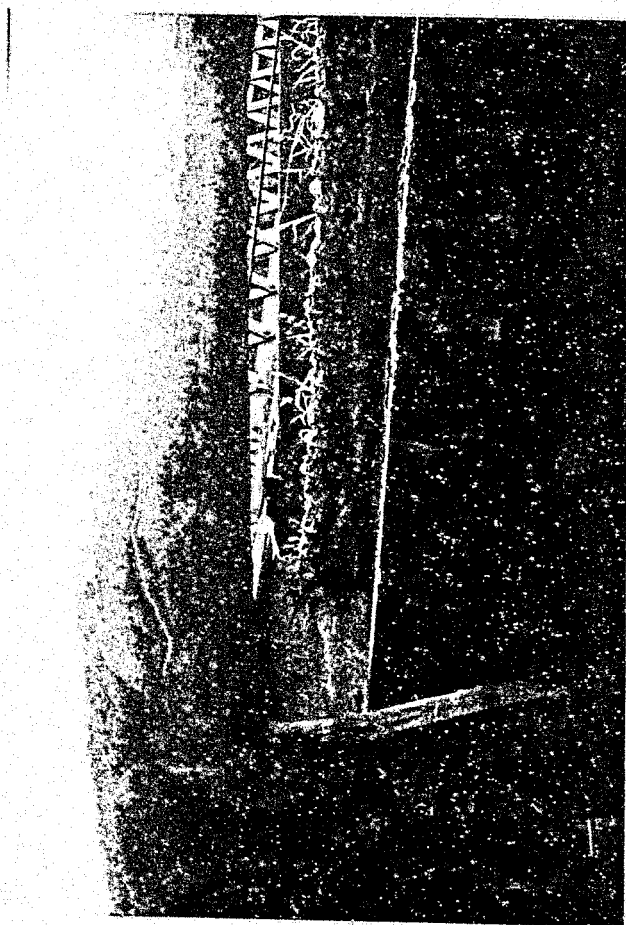


PHOTO 11  
SPALLED UNDER-  
SIDE OF DECK  
ALONG EAST FASC

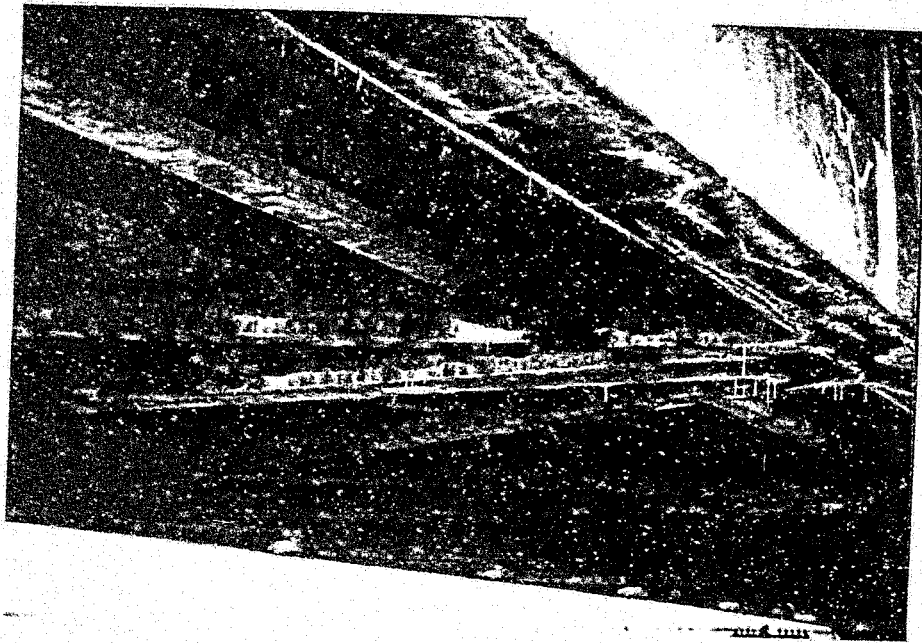


PHOTO 12

TYPICAL SUPERSTRUCTURE ENCASEMENT CONDITION

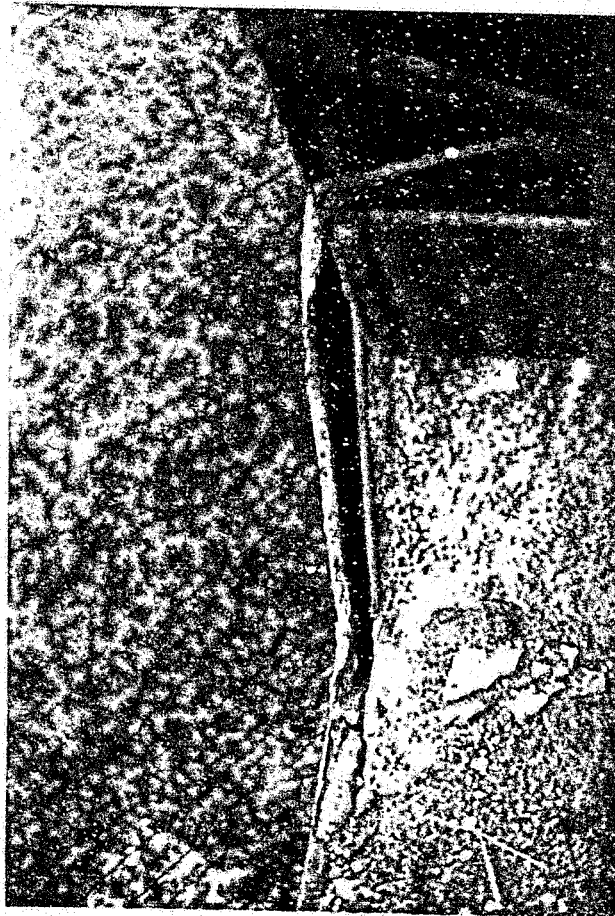


PHOTO 13

SEPARATION OF  
ENCASEMENT FROM  
GUSSET PLATE  
(L<sub>4</sub> EAST TRUSS)

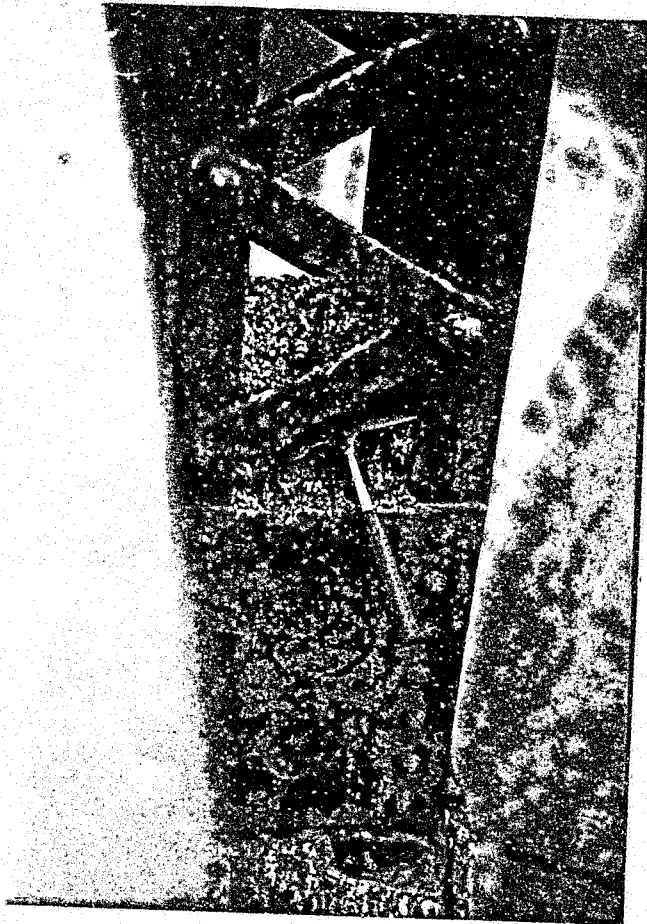


PHOTO 14

TYPICAL DEBRIS  
ACCUMULATION AT  
TRUSS VERTICAL

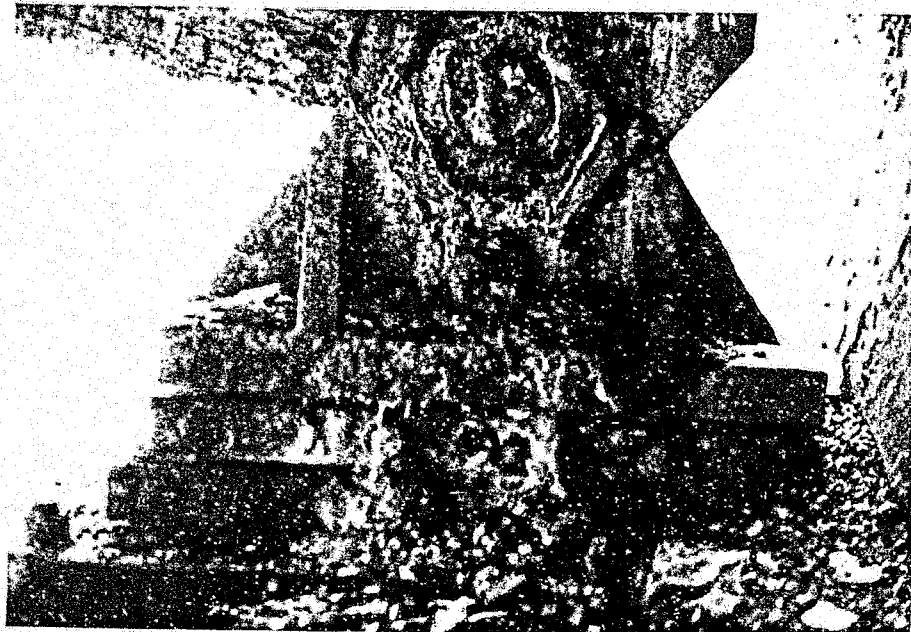


PHOTO 15

SOUTHEAST TRUSS EXPANSION BEARING AND BRIDGE SEAT

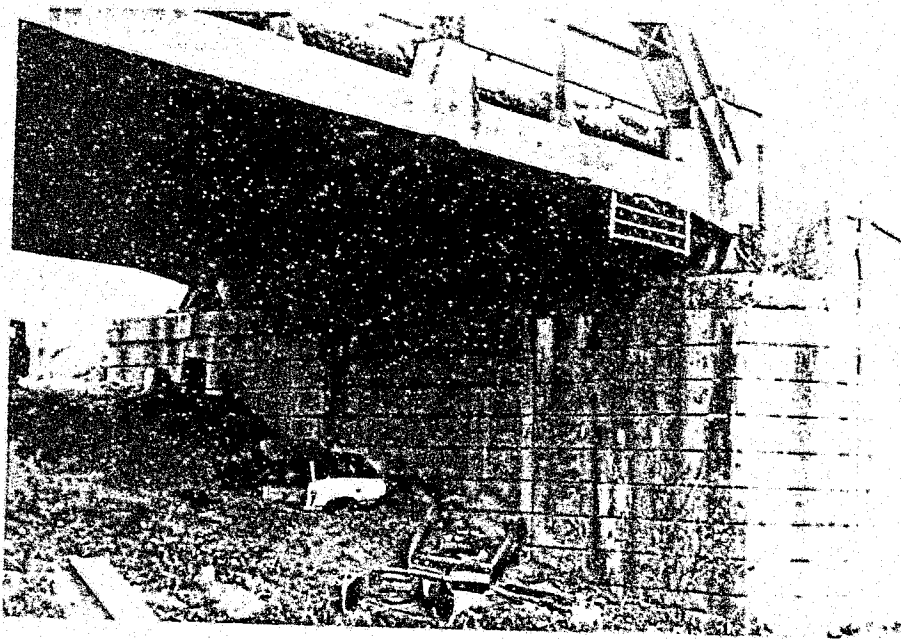


PHOTO 16

NORTH ABUTMENT



PHOTO 17

EAST CORNER SPALLING AT NORTH ABUTMENT

Exhibit 3

COMMONWEALTH OF PENNSYLVANIA  
DEPARTMENT OF TRANSPORTATION  
Hollidaysburg, PA 16648  
December 29, 1983



IN REPLY REFER TO

DOCUMENT  
FOLDER

BR 224.13  
SCW  
pls compare  
Sub 1/5

Blair County  
Snyder Township, Near the Village of Grazierville  
L.R. 55, Station 931+54

Mr. J. T. Sullivan, Chief Engineer  
Design and Construction  
Consolidated Rail Corporation  
15 North 32nd Street, 12th Floor  
Philadelphia, PA 19104

RECEIVED

JUL 15 1985

Dear Mr. Sullivan:

SECRETARY'S OFFICE  
Public Utility Commission

Reference is made to your letter of December 5, 1983 concerning the subject structure.

Relative to the cause of the structure's deterioration, the use of salt by the Department of Transportation in order to remove snow and ice may have attributed to some of the deterioration, but the structure is 44 years old and the deterioration could solely be based on age alone. For design and economic evaluation purposes, the life expectancy of a concrete deck ranges from 40 to 50 years and the life expectancy of an overall structure (with proper maintenance) ranges from 50 to 100 years. Furthermore, our records do not indicate that the concrete deck or any part of the structure has ever had repair work done, other than for the Department's repairs to the roadway paving on the structure.

As per the PUC Order of Complaint Docket #11005, the maintenance of the substructure and superstructure of the bridge, excluding the roadway paving thereon, was assigned to the railroad. The Department is willing to make paving repairs following the necessary substructure and superstructure repairs by the railroad.

Therefore, in order to maintain the structural integrity of the structure and to avoid the possible need for an early total replacement of the structure due to lack of maintenance, we are requesting that you make the repairs that were requested in our letter of September 6, 1983 as well as

RECEIVED

CURA  
JAN 8 1984

ASST. CHIEF  
ENGINEER - STAFF

WHAT PROPOSAL  
THIS?  
See our letter of 12/5/83  
And file (A. H. H. H.)  
WPA 1/4

PA. DEPT. OF TRANSPORTATION  
EXHIBIT NO. 3  
P.U.C. NO. C-84444/0  
DATE OF REPLY 6/20/85

Blair County  
Snyder Township, Near the Village of Grazierville  
L.R. 55, Station 931+54

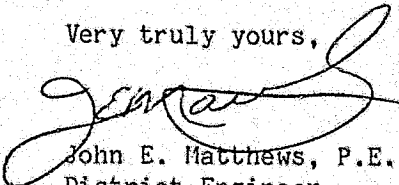
-2-

Hollidaysburg, PA  
December 29, 1983

those maintenance repairs recommended in the in-depth inspection report prepared by A. G. Lichtenstein & Associates, Inc. A second copy of the report was sent to Mr. W. R. Addison of your office on October 3, 1983.

May we have a reply to this request by January 31, 1984?

Very truly yours,



John E. Matthews, P.E.  
District Engineer  
Engineering District 9-0

CC: Mr. J. R. Clark, Chief Engineer, Maintenance of Way, Conrail  
Room 1640 - 6 Penn Center Plaza, Philadelphia, PA 19104  
Mr. R. A. Peteritas, Director, Bur. of Rail Transportation,  
PA Public Utility Commission, 4th Fl. T&S Bldg., Harrisburg, PA

1-4-84

PennDOT Ex. No 4  
PUC No. 844440  
LSR  
6-26-85 M.T.M.  
HOIBERT Associates

Hollidaysburg, PA 16648  
December 29, 1983

POOR ORIGINAL

DOCUMENT  
FOLDER

Elair County  
Snyder Township, Near the Village of Grazierville  
L.R. 55, Station 931+54

RECEIVED

Mr. J. T. Sullivan, Chief Engineer  
Design and Construction  
Consolidated Rail Corporation  
15 North 32nd Street, 12th Floor  
Philadelphia, PA 19104

DEC 15 1983  
OFFICE  
Commission

Dear Mr. Sullivan:

Reference is made to your letter of December 5, 1983 concerning the subject structure.

Relative to the cause of the structure's deterioration, the use of salt by the Department of Transportation in order to remove snow and ice may have attributed to some of the deterioration, but the structure is 44 years old and the deterioration could solely be based on age alone. For design and economic evaluation purposes, the life expectancy of a concrete deck ranges from 40 to 50 years and the life expectancy of an overall structure (with proper maintenance) ranges from 50 to 100 years. Furthermore, our records do not indicate that the concrete deck or any part of the structure has ever had repair work done, other than for the Department's repairs to the roadway paving on the structure.

As per the PUC Order of Complaint Docket #11005, the maintenance of the substructure and superstructure of the bridge, excluding the roadway paving thereon, was assigned to the railroad. The Department is willing to make paving repairs following the necessary substructure and superstructure repairs by the railroad.

Therefore, in order to maintain the structural integrity of the structure and to avoid the possible need for an early total replacement of the structure due to lack of maintenance, we are requesting that you make the repairs that were requested in our letter of September 6, 1983 as well as

1-5-84 Tex Walker called relative to this letter - got Kunselman on a 3 way confer.  
Tex suggested Dept. contribution to help w/repair even though exist. P.U.C. order gives RR the maint responsibility for struc. than rdwy surf., I rejected the request.  
Tex is going to P.U.C. w/in formal complaint to try to get Dept. to contribute to repairs.

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those maintenance repairs recommended in the in-depth inspection report prepared by A. G. Lichtenstein & Associates, Inc. A second copy of the report was sent to Mr. W. R. Addison of your office on October 3, 1983.

May we have a reply to this request by January 31, 1984?

Very truly yours,

J. E. MATTHEWS P.E. DIST. ENGR.

John E. Matthews, P.E.  
District Engineer  
Engineering District 9-0

CC: Mr. J. R. Clark, Chief Engineer, Maintenance of Way, Conrail  
Room 1640 - 5 Penn Center Plaza, Philadelphia, PA 19104  
Mr. R. A. Peteritas, Director, Bur. of Rail Transportation,  
PA Public Utility Commission, 4th Fl. T&S Bldg., Harrisburg, PA  
F. W. Bowser, P.E., Director, Bur. of Highway Design  
ATTN: K. W. Walker, P.E., Chief, R/W & Utilities Division  
J. E. Matthews, P.E., District Engineer 9-0  
R. E. Fleagle, P.E., Asst. District Engineer - Maintenance  
R. J. Kunselman, P.E., District Bridge Engineer  
E. S. Reese, District Grade Crossing Liaison

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