



February 17, 2021

Ms. Rosemary Chiavetta, Secretary
Pennsylvania Public Utility Commission
400 North Street
Harrisburg, PA 17120

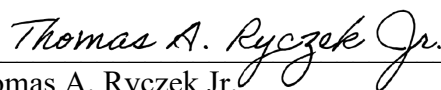
Re: Application of the Department of Transportation of the Commonwealth of Pennsylvania for approval to remove and replace on a new alignment the existing overhead bridge where State Route 1060, Section A20 crosses over the tracks of Norfolk Southern Railway Company (DOT No. 545 164 X) in Bell Township, Westmoreland County and Kiskiminetas Township, Armstrong County and the allocation of costs incident thereto.

Dear SecretaryChiavetta:

Enclosed for filing please find the Application of the Department of Transportation.

A copy of this Application and Exhibits has been served upon the parties in the Certificate of Service to the Application by email.

Sincerely,



Thomas A. Ryczek Jr.
District Grade Crossing Administrator
Engineering District 12-0
Department of Transportation

Attachments

cc: Parties of Record
Mark J. Chappell, P.E., Chief, Right-of-Way and Utilities Section, 7th Floor, CKB
Gina M. D'Alfonso, Senior Counsel in Charge, Office of Chief Counsel, 9th Floor, CKB
Ronald J. Hull, P.E., Supervisor, Rail Safety Engineering Section PUC, 3rd Floor, CKB

**BEFORE THE
PENNSYLVANIA PUBLIC UTILITY COMMISSION**

Application of the Department of Transportation of the Commonwealth of Pennsylvania for approval to remove and replace on a new alignment the existing overhead bridge where State Route 1060, Section A20 crosses over the tracks of Norfolk Southern Railway Company (DOT No. 545 164 X) in Bell Township, Westmoreland County and Kiskiminetas Township, Armstrong County and the allocation of costs incident thereto.

Application
Docket No. _____

Electronically Filed

To the Pennsylvania Public Utility Commission:

1. The name and address of Applicant Commonwealth of Pennsylvania, Department of Transportation, Rachel D. Duda, P.E., Assistant District Executive, Engineering District 12-0, PO Box 459, 825 North Gallatin Avenue, Uniontown, PA 15401.
2. The name and address of attorney for the Applicant are Jason D. Sharp, Chief Counsel, Commonwealth of Pennsylvania, Department of Transportation, Office of Chief Counsel, PO Box 8212, Harrisburg, PA 17105-8212.
3. The Applicant is an agency of Commonwealth of Pennsylvania, organized and existing under the Administrative Code of 1929, 71 P.S. § 511, et seq. and generally 36 P.S. § 670 - 401 et seq.
4. The names and addresses of the persons, parties and entities concerned in, or affected by the proposed construction, to the best of the Applicant's knowledge, are shown in the certificate of service. In addition to those served, the Applicant requests that the following also receive service of all documents in this matter:

Mark J. Chappell P.E., Chief
Utilities and Right of Way Section
Pennsylvania Department of Transportation
PO Box 3362
Harrisburg, PA 17105-3362
marchappell@pa.gov

Gina M. D'Alfonso, Senior Counsel in Charge
Office of Chief Counsel
Pennsylvania Department of Transportation
PO Box 8212
Harrisburg, PA 17105-8212
gdalfonso@pa.gov

5. It is desirable as part of a bridge replacement project to remove and replace the existing overhead bridge on a new alignment where State Route 1060 crosses over the single track of Norfolk Southern Railway Company. The existing bridge is a three-span Parker through-truss with a length of 669.0 feet and a width of 22.67 feet. The proposed bridge is a three-span continuous composite weathering steel plate girder with a length of 650.0 feet and width of 35.0

feet and will meet the current clearance requirements. Preliminary plans are attached hereto as Exhibit “A” included; a location map is attached hereto as Exhibit “B” .

6. The average daily traffic for State Route 1060 is 630 vehicles with 5.3% trucks.
7. The estimated total cost for the bridge replacement is \$11.1 million. The funding for the project will be 80% federal funds and 20% state funds.
8. This project is necessary and proper for the safety and convenience of the public.
9. A conference of all parties of interest should be held to discuss the proposed improvement.

Wherefore, Applicant respectfully requests that the Public Utility Commission approve this application:

Respectfully Submitted:

Rachel D Duda

Digitally signed by Rachel D

Duda

Date: 2021.02.17 13:37:20 -05'00'

Rachel D. Duda, P.E.
Assistant District Executive – Design
Engineering District 12-0
Department of Transportation
825 N. Gallatin Ave.
Uniontown, PA 15401
Phone: (724) 429-7259
Fax: (724) 430-4401
Email: rduda@pa.gov

Dated: _____

**BEFORE THE
PENNSYLVANIA PUBLIC UTILITY COMMISSION**

Application of the Department of Transportation of the Commonwealth of Pennsylvania for approval to remove and replace on a new alignment the existing overhead bridge where State Route 1060, Section A20 crosses over the tracks of Norfolk Southern Railway Company (DOT No. 545 164 X) in Bell Township, Westmoreland County and Kiskiminetas Township, Armstrong County and the allocation of costs incident thereto.

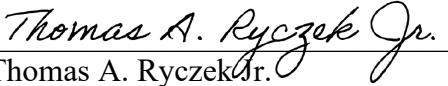
Application
Docket No. _____

Electronically Filed

VERIFICATION

I, Thomas A. Ryczek Jr., hereby state that the facts above set forth are true and correct to the best of my knowledge, information and belief and that I expect to be able to prove the same at a hearing held in this matter. I understand that the statements herein are made subject to the penalties of 18 Pa.C.S. §4904 (relating to unsworn falsification to authorities).

Date: 02-17-2021



Thomas A. Ryczek Jr.
District Grade Crossing Administrator

**BEFORE THE
PENNSYLVANIA PUBLIC UTILITY COMMISSION**

Application of the Department of Transportation of the Commonwealth of Pennsylvania for approval to remove and replace on a new alignment the existing overhead bridge where State Route 1060, Section A20 crosses over the tracks of Norfolk Southern Railway Company (DOT No. 545 164 X) in Bell Township, Westmoreland County and Kiskiminetas Township, Armstrong County and the allocation of costs incident thereto.

Application
Docket No. _____

Electronically Filed

CERTIFICATE OF SERVICE

I hereby certify that I have this day served a true copy of the foregoing document upon the participants listed below by electronic mail pursuant to the Emergency Order at Docket Number M-2020-3019262:

D. Shawn Starling, P.E.
Senior Engineer – Public Improvements
Norfolk Southern Corporation
1200 Peachtree Street
Atlanta, GA 30309
Douglas.Starling@nscorp.com

Julianne Freeman, Esq.
Norfolk Southern Corporation
Three Commercial Place
Norfolk, VA 23510
Julianne.Freeman@nscorp.com

Vera Spina, Chief Clerk
Westmoreland County
101 Courthouse Square
2 North Main Street
Greensburg, PA 15601
vspina@co.westmoreland.pa.us

James Webb
Armstrong County
Courthouse Administration Bldg.
Attn: Board of Commissioners
450 East Marker Street
Kittanning, PA 16201
secretary@co.armstrong.pa.us

Angela Duffner
Bell Township
P.O. Box D
Intersection of SR 819 and SR 380
Salina, PA 15680
belltwpsup@yahoo.com

Patrick Bono
Kiskiminetas Township
1222A Old State Road
Apollo, PA 15613
pbono@kiskitownship-pa.gov

Rob Holman
West Penn Power
143 West Penn Drive
Jeannette, PA 15644
rholman@firstenergycorp.com

John Radeshak
Comcast
1250 Greensburg Road
Lower Burrell, PA 15068
John_Radeshak@cable.comcast.com

Mike Fowkes
Verizon (GTE Fiber)
508 Old Frankstown Road
Monroeville, PA 15146
michael.g.fowkes@verizon.com

Larry Goglin
Windstream (Inside T-Cubed Duct)
1003 Treasure Lake
Dubois, PA 15801
larry.goglin@windstream.com

Dated this 17 Day of February 2021

Thomas A. Ryczek Jr.
Thomas A Ryczek Jr.
Grade Crossing Administrator

| FEDERAL PROJECT NO. | | | | | | |
|---------------------|--------------|--------------|---------|-------|---------|--------------|
| DISTRICT | COUNTY | TOWNSHIP | BOROUGH | ROUTE | SECTION | TOTAL SHEETS |
| 12-0 | WESTMORELAND | BELL | | 1060 | A20 | 21 |
| | ARMSTRONG | KISKIMINETAS | | 2047 | A20 | |
| | | | | | | |

COMMONWEALTH OF PENNSYLVANIA



DEPARTMENT OF TRANSPORTATION

DRAWINGS FOR CONSTRUCTION

ALSO INCLUDED:

| | |
|---|-----------|
| TRAFFIC CONTROL PLAN | 23 SHEETS |
| SIGNING AND PAVEMENT MARKING PLAN | 8 SHEETS |
| EROSION AND SEDIMENT POLLUTION CONTROL PLAN | 39 SHEETS |
| STRUCTURE PLAN S-39414 | 3 SHEETS |
| CROSS SECTIONS | 62 SHEETS |

STATE ROUTE 1060 SECTION A20

IN WESTMORELAND COUNTY

FROM STA 92+15.00 TO STA 108+90.00 LENGTH 1575.00 FT 0.298 MI
FROM SEGMENT 0006 OFFSET 1390 TO SEGMENT 0010 OFFSET 0772

AND

STATE ROUTE 2047 SECTION A20

IN ARMSTRONG COUNTY

FROM STA 108+90.00 TO STA 114+75.00 LENGTH 485.00 FT 0.092 MI
FROM SEGMENT 0010 OFFSET 0066 TO SEGMENT 0010 OFFSET 0651

ALSO

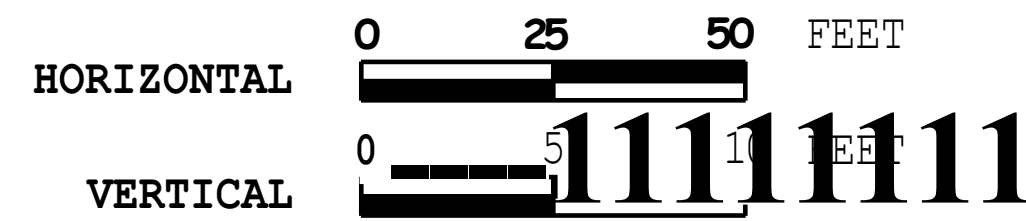
STATE ROUTE 1069

FROM STA 10+57.34 TO STA 18+50.00

PUC APPLICATION DOCKET NUMBER _____

**DESIGN FIELD VIEW SUBMISSION
OCTOBER 2020**

SCALE



DESIGN DESIGNATION

FUNCTIONAL CLASSIFICATION - MAJOR COLLECTOR
ROADWAY TYPOLOGY - COMMUNITY COLLECTOR, SUBURBAN NEIGHBORHOOD
DESIGN SPEED - 25 MPH
PAVEMENT WIDTH - 2 - 11'-0" LANES
SHOULDER WIDTH - 5'-0"
MEDIAN WIDTH - NONE

TRAFFIC DATA

CURRENT YEAR ADT - 630 <2021.1
DESIGN YEAR ADT - 710 (2041)
DHV - 71
D - 50%
T - 5.3%

Exhibit "A"
Sheet 1 of 17

| | | | |
|--|----------------------------------|----------------------------------|--|
| <p>Michael Baker INTERNATIONAL AIRSIDE BUSINESS PARK 100 AIRSIDE DRIVE MOON TOWNSHIP, PA 15108 Phone: (412) 269-6300</p> | ENGINEER _____ DATE _____ | ENGINEER _____ DATE _____ | RECOMMENDED _____ DATE: ____-____-20____ DISTRICT EXECUTIVE |
| | REGISTERED PROFESSIONAL ENGINEER | REGISTERED PROFESSIONAL ENGINEER | RECOMMENDED _____ DATE: ____-____-20____ DEPUTY SECRETARY |
| | | | APPROVED _____ DATE: ____-____-20____ SECRETARY OF TRANSPORTATION |
| | | | (ON BEHALF OF THE GOVERNOR AS WELL AS THE SECRETARY) |

SHEET INDEX BLOCK

| DESCRIPTION | SHEET |
|--------------------------------|----------|
| TITLE SHEET | 1 |
| INDEX MAP | 2 |
| GENERAL NOTES AND LOCATION MAP | 3 |
| PROJECT COORDINATES | 4 |
| TYPICAL SECTIONS | 5 TO 7 |
| SPECIAL DETAILS | 8 |
| PLAN SHEETS | 9 TO 14 |
| PROFILE SHEETS | 15 TO 21 |

TABULATION OF SEGMENT EQUALITIES

SEGMENT 0006 OFFSET 1530 = STA 93+55 SR 1060
 SEGMENT 0006 OFFSET 2293 = STA 101+18 SR 1060 =
 SEGMENT 0010 OFFSET 0000 = STA 101+18 SR 1060
 SEGMENT 0006 OFFSET 2375 = STA 11+96 SR 1069 =
 SEGMENT 0032 OFFSET 1263 = STA 11+96 SR 1069
 SEGMENT 0032 OFFSET 1023 = STA 14+36 SR 1069
 SEGMENT 0010 OFFSET 0772 = STA 108+90 SR 1060 WESTMORELAND COUNTY=
 SEGMENT 0010 OFFSET 0066 = STA 108+90 SR 2047 ARMSTRONG COUNTY
 SEGMENT 0010 OFFSET 0304 = STA 111+28 SR 2047

* WESTMORELAND COUNTY
 AND ARMSTRONG COUNTY

| DISTRICT | COUNTY | ROUTE | SECTION | SHEET |
|----------|--------|-------|---------|---------|
| 12-0 | * | 1060 | A20 | 2 OF 21 |

BELL TOWNSHIP AND KISKIMINETAS TOWNSHIP

| REVISION NUMBER | REVISIONS | DATE | BY |
|-----------------|-----------|------|----|
| | | | |

PROPERTY OWNERS

- & PATRICK CALANDRELLA
- ② BIG VISIONS CORPORATION, A CORPORATION EXISTING UNDER THE LAWS OF THE COMMONWEALTH OF PENNSYLVANIA
- ③ PENNSYLVANIA LINES LLC, A DELAWARE LIMITED LIABILITY COMPANY AND WHOLLY OWNED SUBSIDIARY
- ④ ELDEN L. KING, JR. AND CINDY L. KING, HUSBAND AND WIFE
- ⑤ ANTONIO CALL/PARE AND MARYE. CALL/PARE, HUSBAND AND WIFE
- ⑥ DAVID A. CARNEY, A SINGLE MAN AND DEBORAH SENGE, A SINGLE WOMAN
- ⑦ JOHN O. GARTLEY AND ROSANNA GARTLEY, HIS WIFE
- ⑧ EDWARD USKO, JR. AND KAREN M. USKO, HUSBAND AND WIFE
- & WILLIAM L. FRAIN AND KATHLEEN M. FRAIN, HIS WIFE
- A DENNIS J. KERCHENSKY
- A SHANE M. CLARK
- @ BUHL D. GARTLEY AND MARY M. GARTLEY, HIS WIFE (5/9 INTEREST)
 THOMAS C. McMAHON AND DORIS J. McMAHON (1/9 INTEREST)
 JOHN O. GARTLEY AND MELANIE SUE GARTLEY, HIS WIFE (2/9 INTEREST)
 LUCINDA K. GARTLEY (1/9 INTEREST)

RECORD OF EXISTING ROAD TYPES

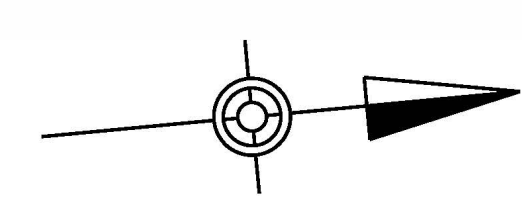
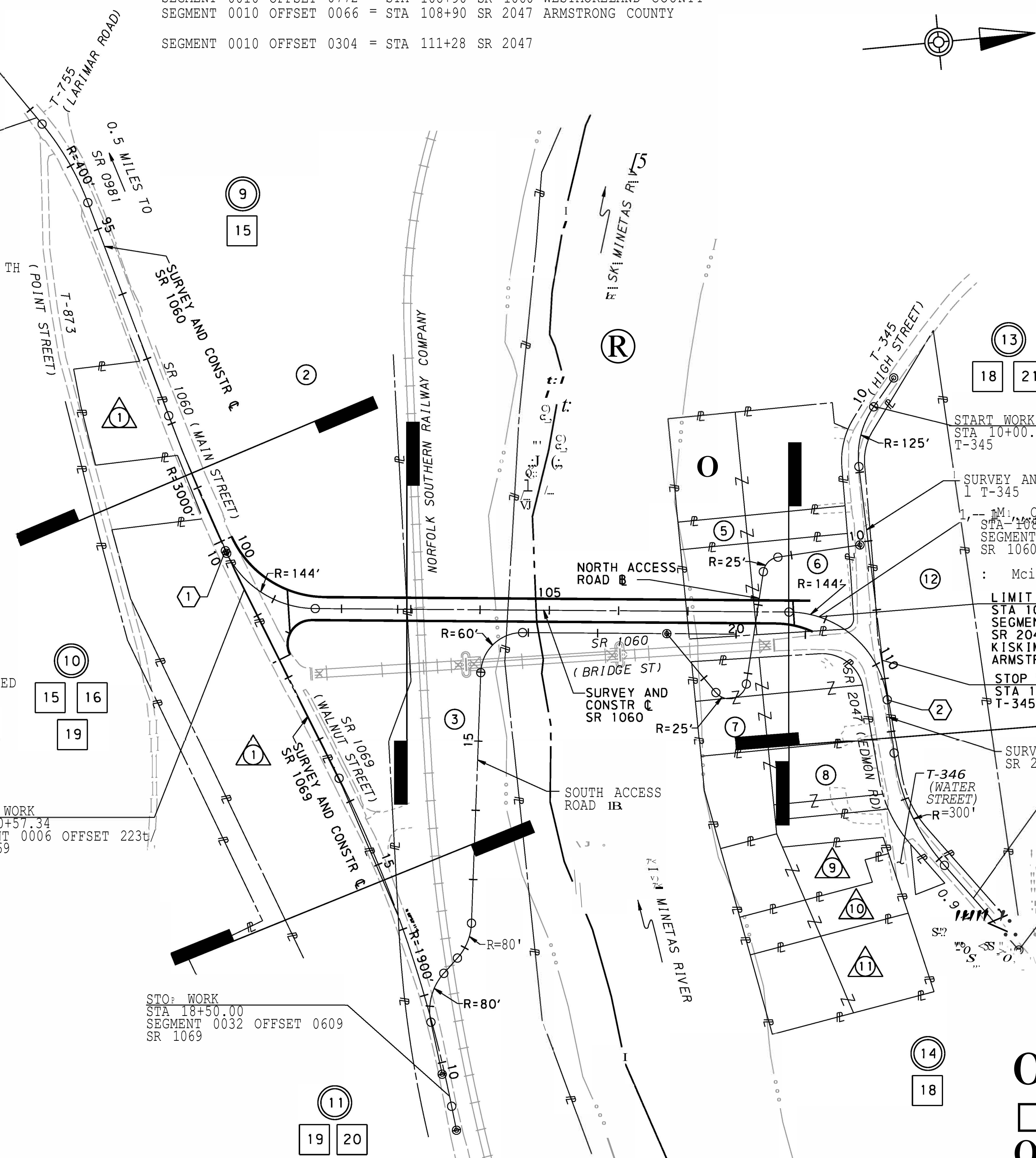
| | | |
|--------------------|------|--|
| SR 1060 CMAIN ST | 2010 | SURFACE TREATMENT - SEAL COAT .25" DEPTH |
| LIMIT OF WORK | 2006 | SURFACE TREATMENT - SEAL COAT .3" DEPTH |
| SEGMENT 0006 | 1998 | BIT WEARING CRSE ID-2 BINDER CRSE 1.5" DEPTH |
| OFFSET 1390 | 1998 | BIT WEARING CRSE ID-2 1" DEPTH |
| TO OFFSET 2293 | 1900 | UNKNOWN BIT WEARING CRSE 1" DEPTH |
| SR 1060 CBRIDGE ST | 2010 | SURFACE TREATMENT - SEAL COAT .25" DEPTH |
| SEGMENT 0010 | 1969 | SURFACE TREATMENT - SEAL COAT .25" DEPTH |
| OFFSET 0000 TO | 1968 | BIT WEARING CRSE SCRATCH ID-2 0.5" DEPTH |
| OFFSET 0037 | 1968 | BIT WEARING CRSE ID-2 2.5" DEPTH |
| | 1961 | BIT WEARING CRSE ID-2 2.5" DEPTH |
| | 1961 | CRUSHED AGGREGATE BASE CRSE 12" DEPTH |
| | 1961 | SPECIAL SUBBASE 6" DEPTH |
| SR 1069 | 1969 | SURFACE TREATMENT - SEAL COAT .25" DEPTH |
| SEGMENT 0032 | 1968 | BITUMINOUS WEARING CRSE ID-2 0.5" DEPTH |
| OFFSET 0909 | 1968 | BITUMINOUS WEARING CRSE ID-2 2.5" DEPTH |
| TO OFFSET 1263 | 1961 | BITUMINOUS WEARING CRSE ID-2 2.5" DEPTH |
| | 1961 | CRUSHED AGGREGATE BASE CRSE 12" DEPTH |
| | 1961 | SPECIAL SUBBASE 6" DEPTH |
| SR 2047 | 2016 | SURFACE TREATMENT - SEAL COAT .25" DEPTH |
| SEGMENT 0010 | 2012 | SURFACE TREATMENT - SEAL COAT .25" DEPTH |
| OFFSET 0000 TO | 2008 | SURFACE TREATMENT - SEAL COAT .25" DEPTH |
| OFFSET 0651 | 2003 | SURFACE TREATMENT - SEAL COAT .25" DEPTH |
| | 1997 | SURFACE TREATMENT - SEAL COAT .25" DEPTH |
| | 1987 | SURFACE TREATMENT - SEAL COAT .25" DEPTH |
| | 1900 | LCC PAVEMENT 8" DEPTH |

NOTE: THE DEPTHS OF MATERIAL SHOWN ARE FOR DESIGN PURPOSES ONLY. ANY RISK OF UNANTICIPATED COSTS ASSOCIATED WITH DIFFERENCES BETWEEN THE LISTED DEPTHS AND THE ACTUAL DEPTHS SHALL BE ACCEPTED BY THE CONTRACTOR.

- STA 100+03.52 SURVEY AND CONSTR 1 SR 1060 =
 STA 10+00.00 SURVEY AND CONSTR 1 SR 1069
- STA 110+53.71 SURVEY AND CONSTR SR 2047 =
 STA 14+30.65 SURVEY AND CONSTR 1 T-345

START WORK
 STA 10+57.34
 SEGMENT 0006 OFFSET 2293
 SR 1069

STOP WORK
 STA 18+50.00
 SEGMENT 0032 OFFSET 0609
 SR 1069

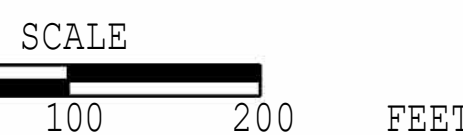


LEGEND

- PLAN
- PROFILE
- ARCEL IDENTIFICATION NUMBER
- ARCEL IDENTIFICATION NUMBER - NO TAKE
- SHEET LIMITS

Exhibit "A"
 Sheet 2 of 17

INDEX MAP



PLANS PREPARED BY
 MICHAEL BAKER INTERNATIONAL
 MOON TOWNSHIP, PA

P: ZT: \$ AU

P: ZT: 7520

* WESTMORELAND COUNTY
AND ARMSTRONG COUNTY

| DISTRICT | COUNTY | ROUTE | SECTION | SHEET |
|---|-----------|-------|---------|---------|
| 12-0 | * | 1060 | A20 | 3 OF 21 |
| BELL TOWNSHIP AND KISKIMINETAS TOWNSHIP | | | | |
| REVISION NUMBER | REVISIONS | DATE | BY | |
| | | | | |
| | | | | |
| | | | | |

AERIAL EASEMENT NOTES

WHERE AN AERIAL EASEMENT IS ACQUIRED, IT SHALL INCLUDE AN EASEMENT IN THE AIR FOR THE ACCOMMODATION OF THE ELEVATED HIGHWAY STRUCTURE UNLIMITED IN VERTICAL DIMENSION ABOVE THE STRUCTURE, A SURFACE EASEMENT UNLIMITED IN VERTICAL DIMENSION FOR THE ACCOMMODATION OF PIERS AND OTHER APPURTENANCES AND A TEMPORARY EASEMENT FOR CONSTRUCTION PURPOSES INCLUDING THE STORAGE OF MATERIALS DURING CONSTRUCTION FOR THE ENTIRE AREA. THE FOLLOWING LIMITATIONS SHALL BE IMPOSED ON THE PROPERTY BENEATH THE AREA AFFECTED BY THE AERIAL EASEMENT.

NO USE SHALL BE MADE OF THE PROPERTY WHICH SHALL ENDANGER THE STRUCTURE OR THE HEALTH, SAFETY OR WELFARE OF THE TRAVELING PUBLIC.

NO FLAMMABLE, EXPLOSIVE, DANGEROUS OR HAZARDOUS MATERIAL SHALL BE USED, PLACED OR STORED ON THE PROPERTY.

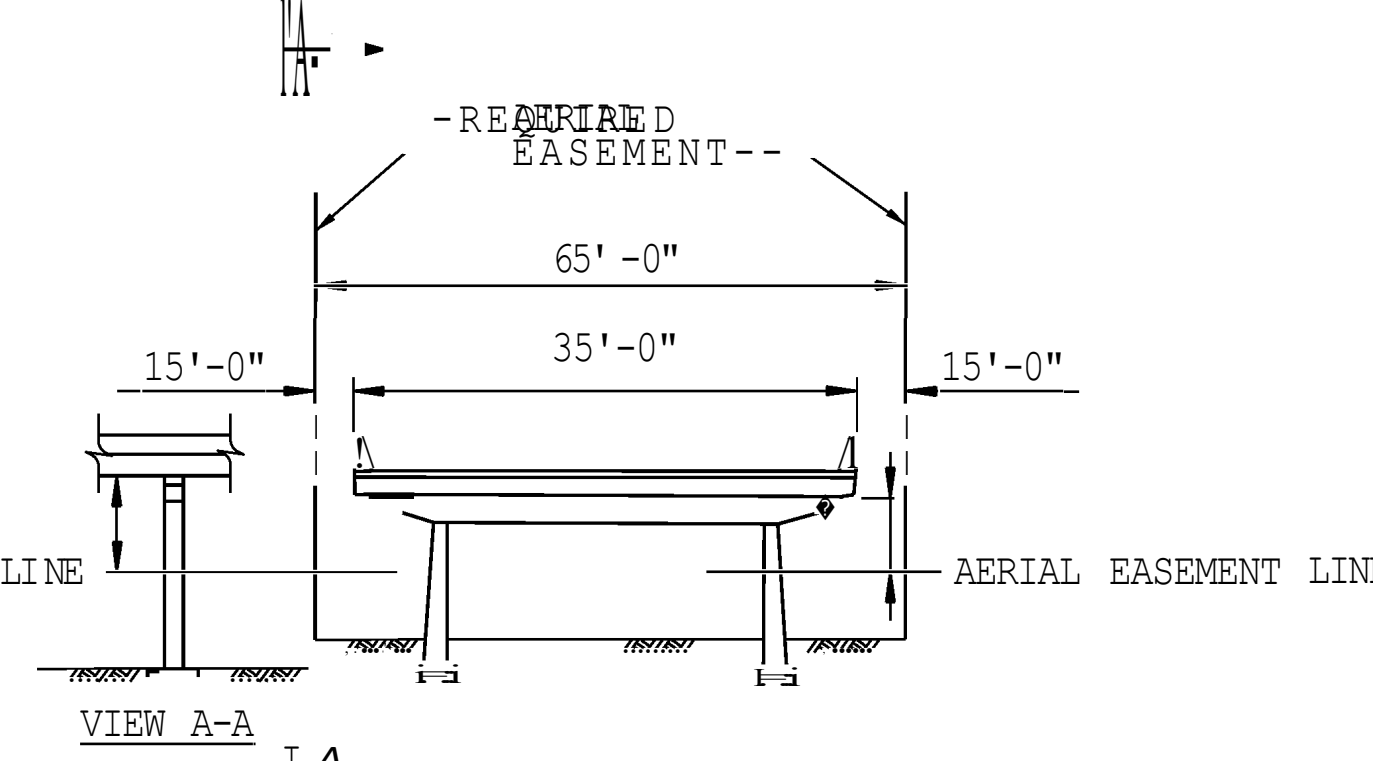
NO BUILDING OR OTHER FACILITY SHALL BE CONSTRUCTED ON THE PROPERTY WITHOUT PRIOR AUTHORITY OF THE DEPARTMENT OF TRANSPORTATION. IF AND WHEN SUCH AUTHORITY IS GRANTED, THE PLANS FOR THE BUILDING OR FACILITY AND CONSTRUCTION METHODS SHALL BE SUBJECT TO THE APPROVAL OF THE DEPARTMENT OF TRANSPORTATION.

NO INTERFERENCE SHALL BE MADE WITH THE RIGHT OF THE DEPARTMENT OF TRANSPORTATION TO ENTER UPON THE PROPERTY FOR THE PURPOSES OF INSPECTION, MAINTENANCE, REPAIR, PAINTING, RECONSTRUCTION OR ALTERATION OF THE STRUCTURE OR APPURTENANCES. MOVEABLE ITEMS MAY HAVE TO BE REMOVED BY THE OWNER DURING SOME OR ALL OF THE ABOVE OPERATIONS.

ANY SUBSTANTIAL CHANGE IN PROPERTY USE TO BE MADE SUBSEQUENT TO THE ACQUISITION OF THE EASEMENT SHALL BE SUBJECT TO THE APPROVAL OF THE DEPARTMENT OF TRANSPORTATION.

THE NOTES ON THIS DRAWING SHALL NOT BE CONSTRUED AS LIMITING OR INTERFERING IN ANY WAY WITH THE PRESENT AND FUTURE OPERATION, USE, MAINTENANCE, REPAIR, RENEWAL, CHANGE, ADDITION, BETTERMENT OR ALTERATION OF THE RAILROAD AND ITS SUPPORTING FACILITIES.

THE AREA WITHIN THE AERIAL EASEMENT BENEATH THE BRIDGE STRUCTURE SHALL BE AVAILABLE TO THE DEPARTMENT, ITS QUALIFIED EMPLOYEES AND CONTRACTORS IN ORDER TO INSPECT AND MAINTAIN THE STRUCTURE UPON CONTACTING THE RAILROAD AND ESTABLISHING PROPER PROTECTION FOR THE WORK IN ACCORDANCE WITH THE USUAL AND CUSTOMARY PROCEDURES THAT THE RAILROAD HAS ESTABLISHED FOR SUCH PROTECTION.



SKETCH SHOWING ESTATE TO BE ACQUIRED FOR A LIMITED AERIAL EASEMENT FROM STATION 101+85.00 TO STATION 104+26.79.

USE OF AIRSPACE BENEATH THE ESTABLISHED GRADELINE OF THE HIGHWAY SHALL PROVIDE SUFFICIENT VERTICAL AND HORIZONTAL CLEARANCES FOR THE CONSTRUCTION, OPERATION, MAINTENANCE, VENTILATION AND SAFETY OF THE HIGHWAY FACILITY. THE ESTATE ACQUIRED ABOVE THE AERIAL EASEMENT LINE MAY BE ENTERED ON BY MOVING VEHICLES SUCH AS TRUCKS OR RAILROAD ROLLING STOCK.

INDICATES SLOPE LIMITS



EARTHWORK SUMMARY ENTIRE PROJECT

THE INFORMATION ON ESTIMATED AMOUNTS OF EARTHWORK HAS BEEN USED IN THE PRELIMINARY ESTIMATE. DO NOT USE AS A WAIVER OF ANY PROVISIONS OF THE SPECIFICATIONS AND CONTRACTS.

| CUBIC YARDS OF EXCAVATION | | | | | CUBIC YARDS OF COMPLETED EMBANKMENT | CUBIC YARDS OF SELECTED BORROW EXCAVATION COARSE AGGREGATE NO.1 | CUBIC YARDS OF SELECTED BORROW EXCAVATION 206 ROCK | CUBIC YARDS OF SELECTED BORROW EXCAVATION ROCK CLASS, R-8 | CUBIC YARDS OF SELECTED BORROW EXCAVATION, STRUCTURE BACKFILL* | CUBIC YARDS OF WASTE |
|---------------------------|-----------------|-------------|--------------|-------------|-------------------------------------|---|--|---|--|----------------------|
| CLASS 1 EXC | CLASS 1 MOD EXC | CLASS 2 EXC | CLASS 3 EXC* | CLASS 4 EXC | | | | | | |
| | | | | | | | | | | |

* PART OF LUMP SUM STRUCTURE ITEM

GENERAL NOTES AND LOCATION MAP

GENERAL NOTES

THE LEGAL RIGHT-OF-WAY ON SR 1060, FORMERLY KNOWN AS LR 64261, FROM STATION 92+15 TO STATION 108+90 IS VARIABLE IN WIDTH BASED ON A PENNSYLVANIA DEPARTMENT OF HIGHWAYS PLAN TITLED "FINAL DRAWINGS FOR CONSTRUCTION AND CONDEMNATION OF RIGHT OF WAY FOR ROUTE NO. 64261 SECTION 1" IN WESTMORELAND COUNTY, SIGNED BY THE GOVERNOR ON OCTOBER 27, 1960 AND RECORDED IN THE WESTMORELAND COUNTY RECORDER OF DEEDS OFFICE, IN PLANS VOLUME 35, PAGE 11.

THE LEGAL RIGHT-OF-WAY ON SR 1069, FORMERLY KNOWN AS IR 990, FROM STATION 10+57.34 TO STATION 18+50 IS VARIABLE IN WIDTH BASED ON A PENNSYLVANIA DEPARTMENT OF HIGHWAYS PLAN TITLED "FINAL DRAWINGS FOR CONSTRUCTION AND CONDEMNATION OF RIGHT OF WAY FOR ROUTE NO. 64261 SECTION 1" IN WESTMORELAND COUNTY, SIGNED BY THE GOVERNOR ON OCTOBER 27, 1960 AND RECORDED IN THE WESTMORELAND COUNTY RECORDER OF DEEDS OFFICE, IN PLANS VOLUME 35, PAGE 11.

THE LEGAL RIGHT-OF-WAY ON SR 2047 FROM STATION 108+90 TO STATION 114+75 IS THIRTY THREE (33) FEET, BASED ON THE ACT OF MAY 1, 1933, P.L. 103, SECTION 1105, AS AMENDED. THERE IS NO PUBLIC RECORD OF ANY OTHER WIDTH.

THE LEGAL RIGHT-OF-WAY ON T-755 <LARIMAR ROAD> IS 33 FT, BASED ON THE ACT OF MAY 1, 1933, P.L. 103, SECTION 1105, AS AMENDED. T-755 <LARIMAR ROAD> HAS BEEN OPENED AND MAINTAINED FOR (21) YEARS OR MORE BY BELL TOWNSHIP, A SECOND-CLASS TOWNSHIP. THERE IS NO PUBLIC RECORD OF ANY OTHER WIDTH.

THE LEGAL RIGHT-OF-WAY ON T-345 (HIGH STREET) FROM STA 10+00 TO 13+73.30 IS 33 FT, BASED ON THE ACT OF MAY 1, 1933, P.L. 103, SECTION 1105, AS AMENDED. T-345 <HIGH STREET> HAS BEEN OPENED AND MAINTAINED FOR <21> YEARS OR MORE BY KISKIMINETAS TOWNSHIP, A SECOND-CLASS TOWNSHIP. THERE IS NO PUBLIC RECORD OF ANY OTHER WIDTH.

THE LEGAL RIGHT-OF-WAY ON T-346 <WATER STREET> IS 33 FT, BASED ON THE ACT OF MAY 1, 1933, P.L. 103, SECTION 1105, AS AMENDED. T-346 <WATER STREET> HAS BEEN OPENED AND MAINTAINED FOR (21) YEARS OR MORE BY KISKIMINETAS TOWNSHIP, A SECOND-CLASS TOWNSHIP. THERE IS NO PUBLIC RECORD OF ANY OTHER WIDTH.

DO NOT INTERFERE WITH OPERATION OF ANY FIRE HYDRANT, FIRE CALL BOX OR POLICE CALL BOX.

THE HORIZONTAL CONTROL IS TIED TO THE PENNSYLVANIA STATE PLANE COORDINATE SYSTEM (NAD 83, PA SOUTH ZONE). THE COMBINED FACTOR FOR THIS PROJECT IS 0.999960336. GROUND LEVEL DISTANCES CAN BE DERIVED BY DIVIDING THE NAD 83 DISTANCE, OR GRID DISTANCE, BY THE COMBINED FACTOR.

VERTICAL CONTROL IS BASED UPON THE NORTH AMERICAN VERTICAL DATUM OF 1988 <NAVD881>.

ALL CURVE DATA IS BASED ON THE ARC DEFINITION.

CONSTRUCT PROJECT IN ACCORDANCE WITH PUBLICATION 408 SPECIFICATIONS, DATED 2020.

SR 1060 PREVIOUSLY KNOWN AS LR 64261.

THE LOCATIONS OF UNDERGROUND UTILITIES SHOWN ON THE PLANS HAVE BEEN OBTAINED BY FIELD SURVEY AND SEARCHES OF AVAILABLE RECORDS. THE DEPARTMENT OF TRANSPORTATION DOES NOT GUARANTEE THEIR ACCURACY OR COMPLETENESS. VERIFY THEIR LOCATION IN THE FIELD PRIOR TO EXCAVATION.

THIS IS A FEDERAL AID PROJECT AND AS SUCH IS SUBJECT TO INSPECTION BY REPRESENTATIVES OF THE FEDERAL HIGHWAY ADMINISTRATION AND THE PENNSYLVANIA DEPARTMENT OF TRANSPORTATION.

TEMPORARY CONSTRUCTION EASEMENT. AN EASEMENT TO USE THE LAND AS NECESSARY DURING CONSTRUCTION OF THE PROJECT. THE EASEMENT IS REQUIRED ONLY UNTIL THE CONSTRUCTION OR WORK INDICATED BY THE PLAN IS COMPLETED, UNLESS SOONER RELINQUISHED IN WRITING BY THE DEPARTMENT.

DRAINAGE EASEMENT. AN EASEMENT FOR THE CONSTRUCTION, INSPECTION, MAINTENANCE, REPAIR, RECONSTRUCTION AND ALTERATION OF THE HIGHWAY DRAINAGE FACILITIES. THE EASEMENT SHALL NOT PREVENT THE PROPERTY OWNER FROM MAKING ANY LEGAL USE OF THE AREA WHICH IS NOT DETRIMENTAL TO THE NECESSARY FLOW OF WATER. HOWEVER, NO STRUCTURE OF ANY KIND MAY BE ERRECTED IN THE AREA, NOR MAY ANY PIPE OR DITCH BE CONNECTED TO THE DEPARTMENT'S PIPE OR DITCH WITHOUT ADVANCED WRITTEN APPROVAL BY THE DEPARTMENT OF TRANSPORTATION.

THREE WORKING DAYS PRIOR TO EXCAVATION, THE CONTRACTOR MUST CONTACT THE PA ONE CALL SYSTEM, INC., PHONE 1-800-242-1776, SERIAL NO. _____ BELL TOWNSHIP, WESTMORELAND COUNTY. SERIAL NO. _____ KISKIMINETAS TOWNSHIP, ARMSTRONG COUNTY. ADDITIONAL INFORMATION IS AVAILABLE AT <https://www.palcal1.o.g/pa811/public/>.

THE CONTRACTOR IS REQUIRED TO NOTIFY THE DEPARTMENT AND SUBMIT AN ALLEGED VIOLATION REPORT <AVRI> TO THE PA PUBLIC UTILITY COMMISSION THROUGH THE PA ONE CALL SYSTEM, WWW.PALCALL.ORG, WITHIN TEN (10) BUSINESS DAYS AFTER A UTILITY LINE IS STRUCK, DAMAGED, OR PREVIOUS DAMAGE IS DISCOVERED AS REQUIRED BY PENNSYLVANIA'S UNDERGROUND UTILITY LINE PROTECTION LAW ACT 50 <P.L.852, NO. 287 AMENDED OCT. 30, 2017>.

DEMOLISH AND REMOVE BUILDINGS AND STRUCTURES MARKED [Q]AS LUMP SUM ITEMS AT THE LOCATIONS INDICATED.

PLANS PREPARED BY
MICHAEL BAKER INTERNATIONAL
MOON TOWNSHIP, PA

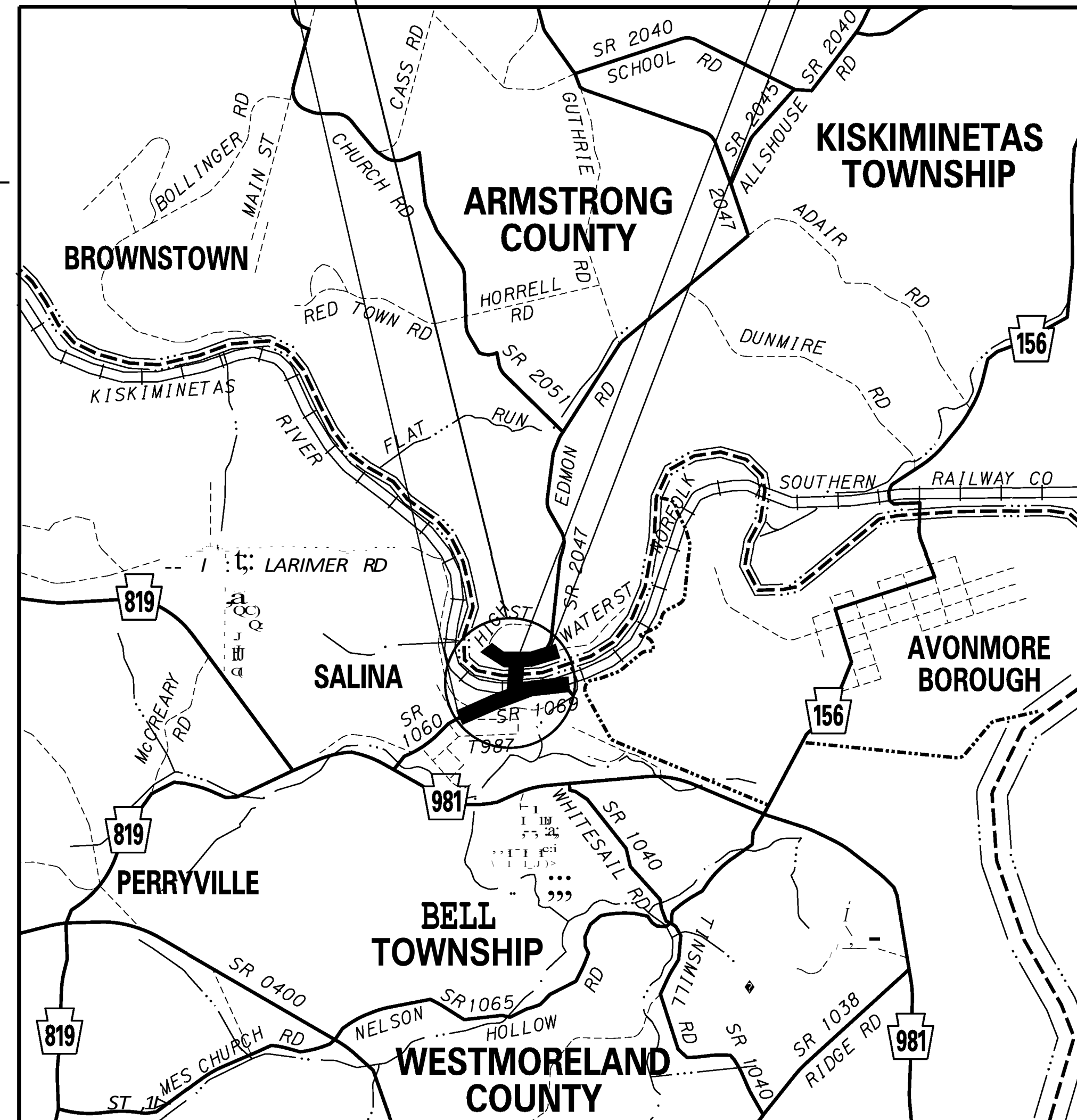
PROJECT LOCATION

LIMIT OF WORK
STA 92+15.00
SEGMENT 0006 OFFSET 1390
SR 1060 SECTION A20
BELL TOWNSHIP
WESTMORELAND COUNTY

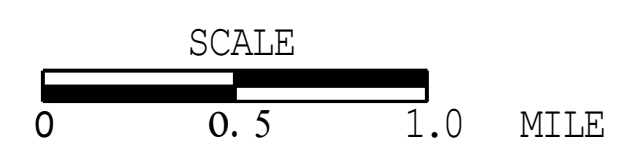
LIMIT OF WORK
STA 108+90.00
SEGMENT 0010 OFFSET 0066
SR 2047 SECTION A20
KISKIMINETAS TOWNSHIP
ARMSTRONG COUNTY

LIMIT OF WORK
STA 108+90.00
SEGMENT 0010 OFFSET 0772
SR 1060 SECTION A20
BELL TOWNSHIP
WESTMORELAND COUNTY

LIMIT OF WORK
STA 114+75.00
SEGMENT 0010 OFFSET 0651
SR 2047 SECTION A20
KISKIMINETAS TOWNSHIP
ARMSTRONG COUNTY



LOCATION MAP



LEGEND

- COUNTY LINE
- - - - - TOWNSHIP LINE
- BOROUGH LINE
- ===== STATE HIGHWAY
- TOWNSHIP ROAD
- █ PROJECT
- STREAM
- ==== RAILROAD
- NO DETOUR

Exhibit "A"
Sheet 3 of 17

LIST OF PUBLIC UTILITIES

- E- WEST PENN POWER
143 WEST PENN DR.
JEANNETTE, PA 15644
ATTN: ROB HOLMAN
PHONE: (724) 523-7238
EMAIL: rholmanCfirstenergycorp.com
- CTV- COMCAST
1250 GREENSBURG RD.
LOWER BURRELL, PA 15068
ATTN: JOHN RADESHAK
PHONE: (724) 212-7236
EMAIL: John_RadeshakCcable.comcast.com
- T- VERIZON (GTE FIBER)
508 OLD FRANKSTOWN ROAD
MONROEVILLE, PA 15146
ATTN: MIKE FOWKES
PHONE: (412) 793-3803 (412) 558-5601
EMAIL: michael.g.fowkesverizon.com
- FOU-WINDSTREAM (INSIDE T-CUBED DUCT)
1003 TREASURE LAKE
DUBOIS, PA 15801
ATTN: LARRY GOGLIN
PHONE: (814) 715-1692
EMAIL: lorry.goglinCOwindstream.com
- FOU- T-CUBED
NORFOLK SOUTHERN CORPORATION
PUBLIC IMPROVEMENTS
1200 PEACHTREE ST. NE
ATLANTA, GEORGIA 30309
ATTN: SHAWN STARLING
PHONE: (404) 527-2536
EMAIL: douglas.starlingncscorp.com

PENNSYLVANIA ONE CALL SYSTEM, INC 1-800-242-1776
PRELIMINARY DESIGN SERIAL NO 20190420712 BELL TOWNSHIP, WESTMORELAND COUNTY
PRELIMINARY DESIGN SERIAL NO 20190420713 KISKIMINETAS TOWNSHIP, ARMSTRONG COUNTY
FINAL DESIGN SERIAL NO BELL TOWNSHIP, WESTMORELAND COUNTY
FINAL DESIGN SERIAL NO KISKIMINETAS TOWNSHIP, ARMSTRONG COUNTY

**DETAILS OTHER THAN THOSE INDICATED,
ARE ON THE FOLLOWING STANDARD DRAWINGS:**

| | | | |
|--------------------|---------------|----------|---------------|
| RC-10M (1 SHEET> | JUNE 1, 2010 | BC-701M | JAN 31, 2019 |
| RC-11M (2 SHEETS> | JUNE 1, 2010 | BC-713M | JAN 31, 2019 |
| RC-12M (2 SHEETS> | FEB 8, 2019 | BC-732M | JAN 31, 2019 |
| RC-13M (1 SHEET> | JUNE 1, 2010 | BC-734M | JAN 31, 2019 |
| RC-23M (3 SHEETS> | FEB 8, 2019 | BC-735M | SEPT 30, 2016 |
| RC-25M (4 SHEETS> | FEB 8, 2019 | BC-736M | JAN 31, 2019 |
| RC-28M (2 SHEETS> | FEB 8, 2019 | BC-751M | JAN 31, 2019 |
| RC-30M (5 SHEETS> | DEC 17, 2019 | BC-752M | SEPT 30, 2016 |
| RC-31M (2 SHEETS> | JUNE 1, 2010 | BC-753M | JAN 31, 2019 |
| RC-45M <20 SHEETS> | SEPT 15, 2016 | BC-754M | JAN 31, 2019 |
| RC-46M <34 SHEETS> | SEPT 15, 2016 | BC-756M | SEPT 30, 2016 |
| RC-SOM C18 SHEETS> | FEB 8, 2019 | BC-757M | SEPT 30, 2016 |
| RC-51M C14 SHEETS> | DEC 17, 2019 | BC-762M | JAN 31, 2019 |
| RC-54M <12 SHEETS> | DEC 17, 2019 | BC-788M | JAN 31, 2019 |
| RC-70M (3 SHEETS> | FEB 8, 2019 | | |
| RC-73M (4 SHEETS> | FEB 8, 2019 | | |
| RC-75M (1 SHEET> | JUNE 1, 2010 | TC-8600 | JUNE 13, 2013 |
| RC-77M (1 SHEET> | DEC 17, 2019 | TC-8604 | JUNE 13, 2013 |
| | | TC-8702B | JUNE 13, 2013 |
| | | TC-8716 | JUNE 13, 2013 |
| | | TC-8717 | JUNE 13, 2013 |

TABULATION OF OVERALL LENGTH

SR 1060 STA 92+15.00 TO STA 108+90.00 = 1675.00 FT = 0.317 MI
 SR 2047 STA 108+90.00 TO STA 114+75.00 = 585.00 FT = 0.111 MI
 TOTAL = 2260.00 FT = 0.428 MI

TABULATION OF CONSTRUCTION LENGTH

SR 1060 STA 93+15.00 TO STA 108+90.00 = 1575.00 FT = 0.298 MI
 SR 2047 STA 108+90.00 TO STA 113+75.00 = 485.00 FT = 0.092 MI
 TOTAL = 2060.00 FT = 0.390 MI

LIST OF STATION EQUALITIES

NONE

TABULATION OF PROJECT COORDINATES

BASED ON PENNSYLVANIA STATE PLANE COORDINATE SYSTEM <NAD 83, PA SOUTH ZONE1

| ROUTE | STATION | POINT | COORDINATES | | BEARING |
|--|-----------|-------------|--------------|-----------------|-----------------|
| | | | NORTH | EAST | |
| SURVEY AND CONSTR <t.SR 1060 AND SR 2047 | 92+00.00 | POT | 438038.0512 | 1482694.1841 | N 55° 43' 58 "E |
| | 93+26.97 | PC | 438154.5416 | 1482799.1139 | |
| | 93+93.52 | PI | 438192.0106 | 1482854.1088 | |
| | 94+58.85 | PT | 438209.6557 | 1482918.2728 | N 74° 37' 25 "E |
| | 97+87.89 | PC | 438296.9020 | 1483235.5322 | N 70° 35' 15 "E |
| | 98+93.60 | PI | 438324.9326 | 1483337.4616 | |
| | 99+99.23 | PCC | 438360.0684 | 1483437.1652 | |
| | 100+90.56 | PI | 438390.4229 | 1483523.3014 | N 5° 49' 11 "E |
| | 101+62.01 | PT | 438481.2803 | 1483532.5621 | |
| | 108+46.01 | PC | 439161.7543 | 1483601.9198 | |
| | 109+72.61 | PI | 439287.7063 | 1483614.7575 | N 88° 27' 49 "E |
| 110+53.71 | PT | 439291.1009 | 1483741.3165 | | |
| 111+31.61 | PC | 439293.1896 | 1483819.1871 | | |
| 112+24.56 | PI | 439295.6818 | 1483912.1000 | N 54° 02' 07 "E | |
| 113+11.88 | PT | 439350.2679 | 1483987.3289 | | |
| 114+50.00 | POT | 439431.3847 | 1484099.1216 | | |
| SURVEY AND CONSTR <t.SR 1069 | 10+00.00 | POT | 438361.5528 | 1483441.1855 | N 68° 53' 11 "E |
| | 13+63.29 | PC | 438492.4180 | 1483780.0893 | |
| | 16+15.52 | pI | 438583.2772 | 1484015.3884 | |
| | 18+64.82 | PT | 438609.5974 | 1484266.2436 | N 84° 00' 37 "E |
| | 19+00.00 | POT | 438613.2684 | 1484301.2309 | |
| SURVEY AND CONSTR <t.T-345 | 9+50.00 | POT | 439344.0965 | 1483279.7173 | S 49° 51' 52 "E |
| | 10+00.72 | PC | 439311.4007 | 1483318.4961 | |
| | 10+48.30 | pI | 439280.7353 | 1483399.0708 | |
| | 10+91.64 | PT | 439282.0109 | 1483402.4224 | N aa° 21' 49 "E |
| | 14+30.65 | POT | 439291.1009 | 1483741.3165 | |
| SOUTH ACCESS ROAD ILL. | 10+00.00 | POT | 438600.2977 | 1484218.8592 | S 83° 13' 41 "W |
| | 10+76.76 | PC | 438591.2462 | 1484142.6347 | |
| | 11+17.50 | PI | 438586.4425 | 1484102.1816 | |
| | 11+52.12 | PT | 438616.3327 | 1484074.5029 | N 42° 48' 00 "W |
| | 11+81.63 | PC | 438637.9871 | 1484054.4506 | |
| | 12+10.48 | pI | 438659.1536 | 1484034.8502 | |
| | 12+37.01 | PT | 438662.9397 | 1484006.2521 | N 82° 27' 30 "W |
| | 15+99.30 | PC | 438710.4897 | 1483647.0915 | |
| | 16+57.52 | PI | 438718.1313 | 1483589.3716 | |
| | 16+91.74 | PT | 438776.0547 | 1483595.2755 | N 5° 49' 11 "E |
| | 21+94.85 | POT | 439276.5659 | 1483646.2904 | |
| | | | | | |
| NORTH ACCESS ROAD ILL. | 10+00.00 | POT | 439272.8455 | 1483515.1203 | S 3° 04' 44 "E |
| | 11+19.72 | PC | 439153.3004 | 1483521.5502 | |
| | 11+38.19 | pI | 439134.8558 | 1483522.5423 | |
| | 11+51.53 | PT | 439130.3861 | 1483540.4646 | S 75° 59' 47 "E |
| | 13+15.56 | PC | 439090.6949 | 1483699.6141 | |
| | 13+71.15 | pI | 439077.2433 | 1483753.5511 | |
| | 13+72.97 | PT | 439045.8165 | 1483707.6981 | S 55° 34' 26 "W |
| | 14+83.66 | POT | 438983.2375 | 1483616.3927 | |
| | | | | | |

NOTE: FOUR C41 PLACE COORDINATES ARE USED FOR COMPUTATIONAL PURPOSES ONLY AND DO NOT IMPLY A PRECISION BEYOND TWO C21 PLACES.

Exhibit "A"
Sheet 4 of 17

PROJECT COORDINATES

PLANS PREPARED BY
MICHAEL BAKER INTERNATIONAL
MOON TOWNSHIP, PA

16:28:03 AM

16:27:22Z

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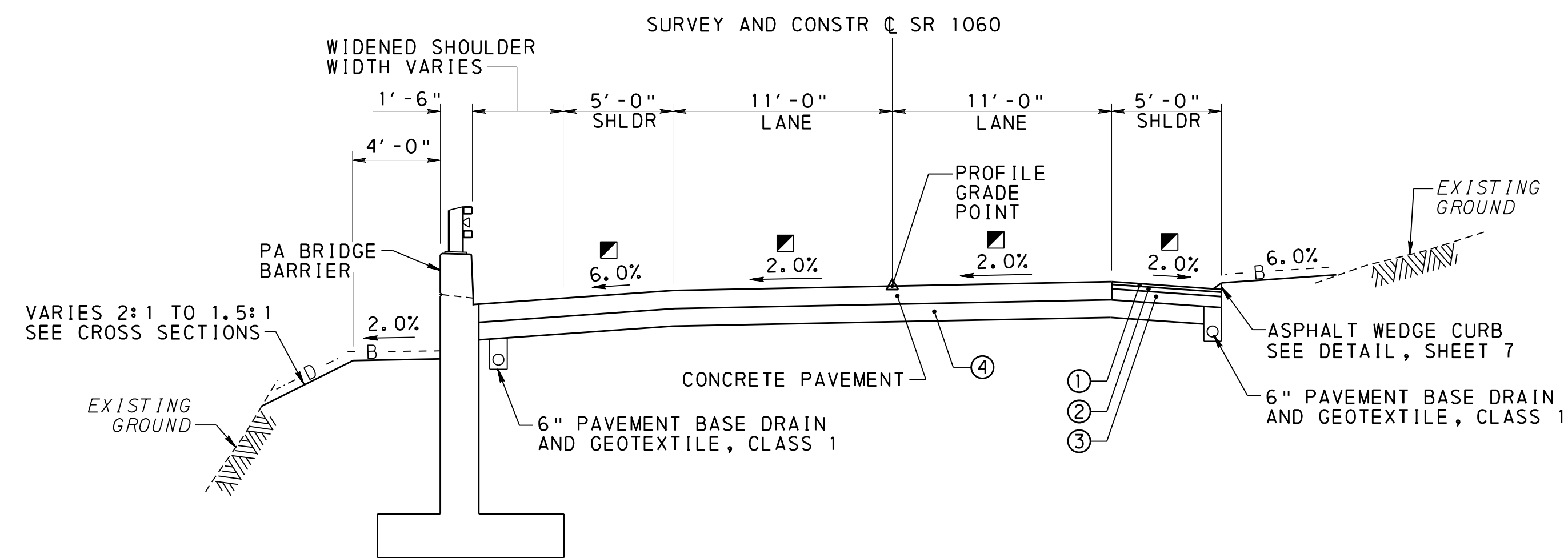
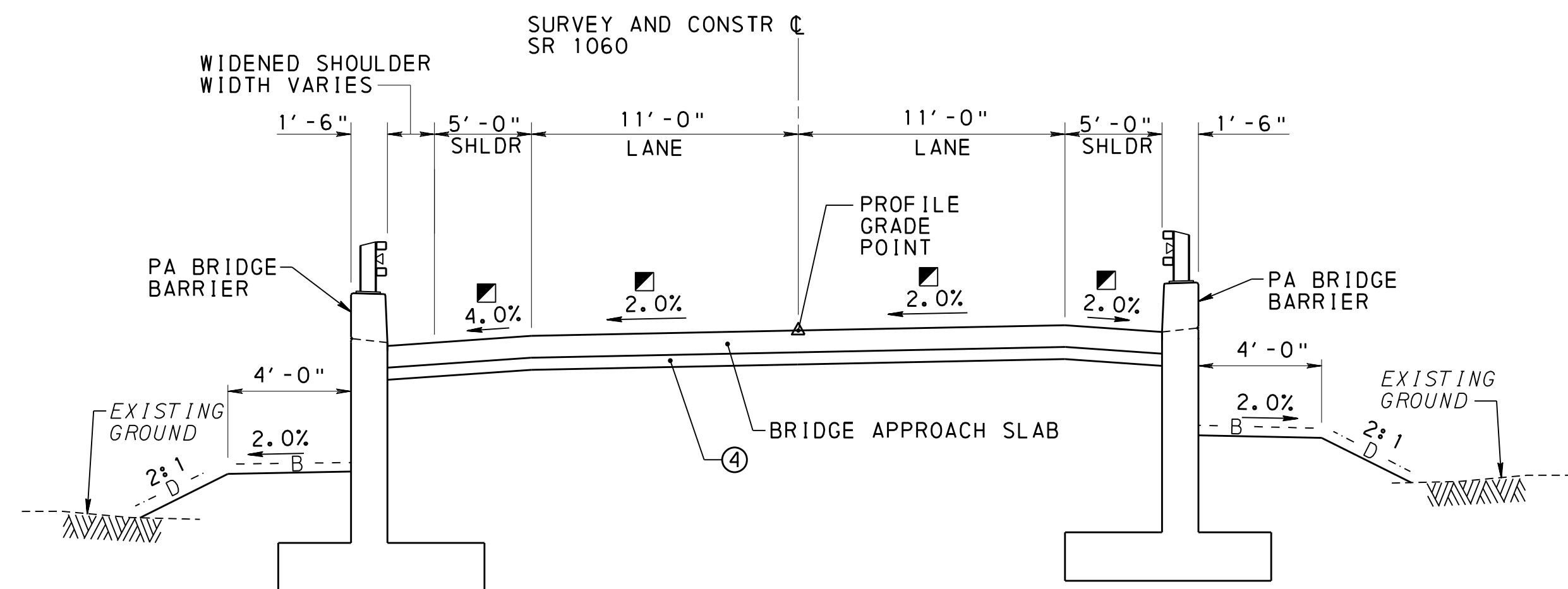
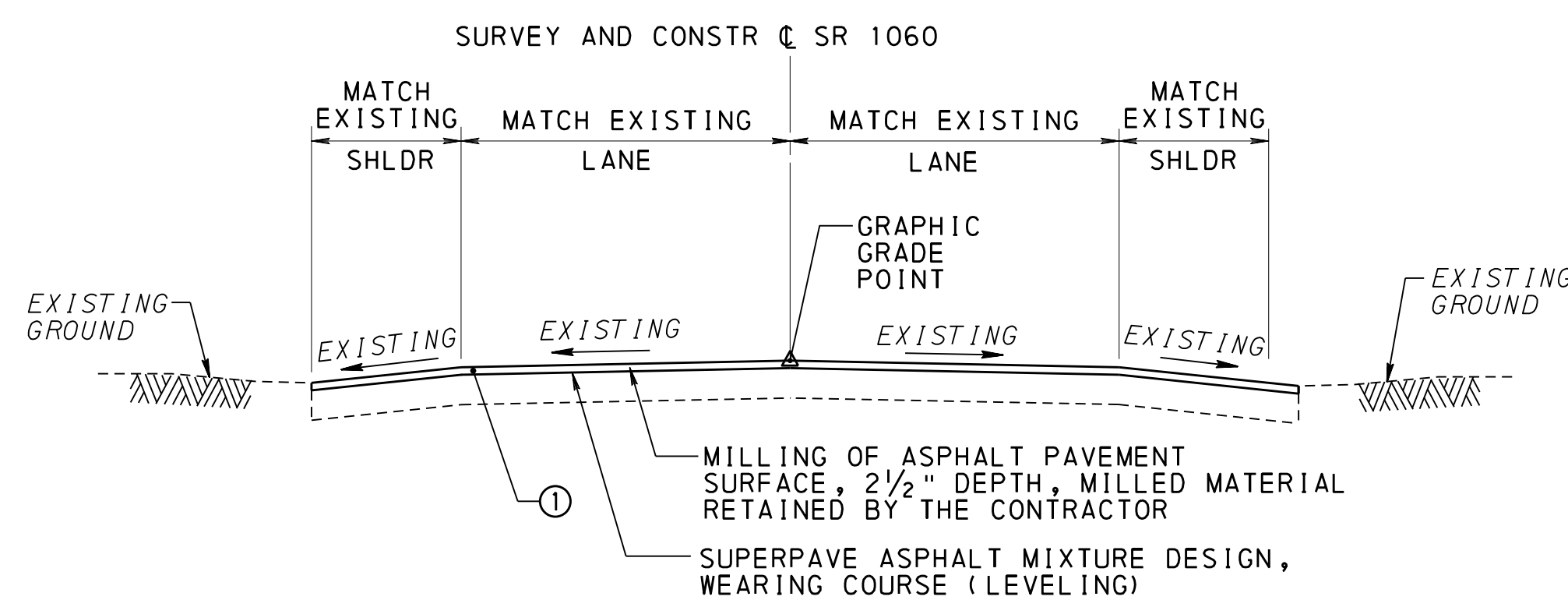
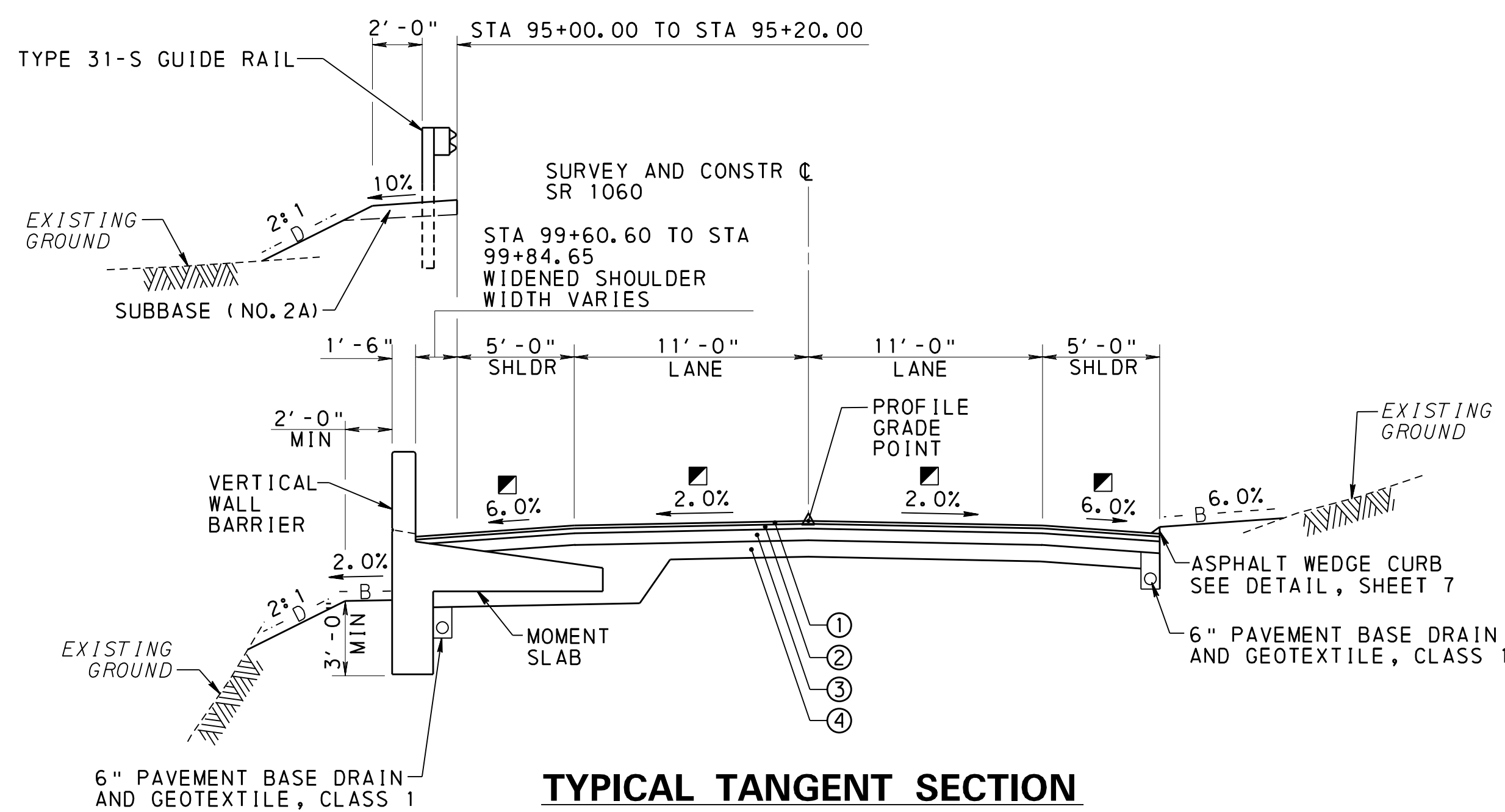
| DISTRICT | COUNTY | ROUTE | SECTION | SHEET |
|---|-----------|-------|---------|---------|
| 12-0 | * | 1060 | A20 | 5 OF 21 |
| BELL TOWNSHIP AND KISKIMINETAS TOWNSHIP | | | | |
| REVISION NUMBER | REVISIONS | DATE | BY | |
| | | | | |
| | | | | |
| | | | | |

- ① SUPERPAVE ASPHALT MIXTURE DESIGN, WEARING COURSE, DEPTH TO BE DETERMINED
- ② SUPERPAVE ASPHALT MIXTURE DESIGN, BINDER COURSE, DEPTH TO BE DETERMINED
- ③ SUPERPAVE ASPHALT MIXTURE DESIGN, BASE COURSE, DEPTH TO BE DETERMINED
- ④ SUBBASE (NO. 2A), DEPTH TO BE DETERMINED

| STA | EOS | EOP | EOP | EOS |
|---------------|------|------|------|------|
| STA 113+64.38 | 6.0% | 2.0% | 2.0% | 6.0% |
| STA 113+32.88 | 6.0% | 2.0% | 0.0% | 4.0% |
| STA 113+01.38 | 6.0% | 2.0% | 2.0% | 2.0% |
| STA 111+42.11 | 6.0% | 2.0% | 2.0% | 2.0% |
| STA 111+10.61 | 6.0% | 2.0% | 0.0% | 4.0% |
| STA 111+06.21 | 6.0% | 2.0% | 0.3% | 6.0% |
| STA 110+79.11 | 4.3% | 0.3% | 2.0% | 6.0% |
| STA 110+74.71 | 4.0% | 0.0% | 2.0% | 6.0% |
| STA 110+43.21 | 2.0% | 2.0% | 2.0% | 6.0% |
| STA 109+50.00 | 2.0% | 2.0% | 2.0% | 6.0% |
| STA 109+00.00 | 2.0% | 2.0% | 2.0% | 4.0% |
| STA 108+56.51 | 2.0% | 2.0% | 2.0% | 4.0% |
| STA 108+25.01 | 2.0% | 0.0% | 2.0% | 4.0% |
| STA 107+93.51 | 2.0% | 2.0% | 2.0% | 4.0% |
| STA 107+46.80 | 4.0% | 2.0% | 2.0% | 4.0% |
| STA 102+14.51 | 4.0% | 2.0% | 2.0% | 4.0% |
| STA 101+83.01 | 4.0% | 2.0% | 0.0% | 3.0% |
| STA 101+51.51 | 4.0% | 2.0% | 2.0% | 2.0% |
| STA 101+00.00 | 4.0% | 2.0% | 2.0% | 2.0% |
| STA 100+50.00 | 6.0% | 2.0% | 2.0% | 2.0% |
| STA 99+77.73 | 6.0% | 2.0% | 2.0% | 2.0% |
| STA 99+46.23 | 6.0% | 2.0% | 0.0% | 4.0% |
| STA 99+14.73 | 6.0% | 2.0% | 2.0% | 6.0% |

CROSS SLOPE TRANSITIONS

● SUPERELEVATION ROTATION POINT IS ABOUT THE CENTERLINE



NOTES:

APPLY ALL ROADWAY JOINT SEALING IN ACCORDANCE WITH PUBLICATION 408 SECTION 469 (INCIDENTAL TO THE COURSE BEING PLACED).

APPLY SEPARATE LAYER OF ASPHALT TACK COAT FOR EACH PAVING COURSE, INCLUDING MULTIPLE LIFTS OF THE SAME MATERIAL.

REFER TO RC-10M FOR CUT SLOPE ROUNDING.

■ SEE CROSS SLOPE TRANSITION TABLE.

SEEDING LEGEND

-- B --- B --- SEEDING & SOIL SUPPLEMENTS - FORMULA B SEED MIXTURE AND MULCHING-STRAW ON 4" OF TOPSOIL (3:1 AND FLATTER)

-- D --- D --- SEEDING & SOIL SUPPLEMENTS - FORMULA D SEED MIXTURE (STEEPER THAN 3:1)

Exhibit "A"
Sheet 5 of 17

TYPICAL SECTIONS

PLANS PREPARED BY
MICHAEL BAKER INTERNATIONAL
MOON TOWNSHIP, PA

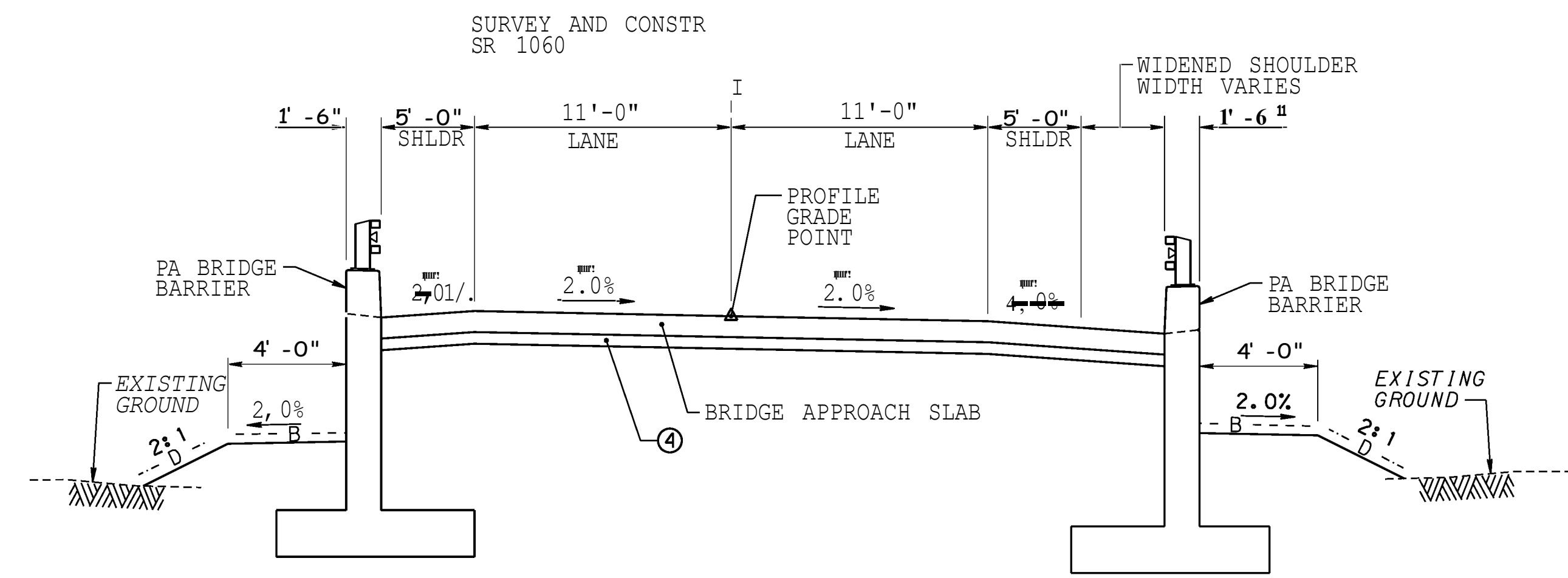
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10/27/2020

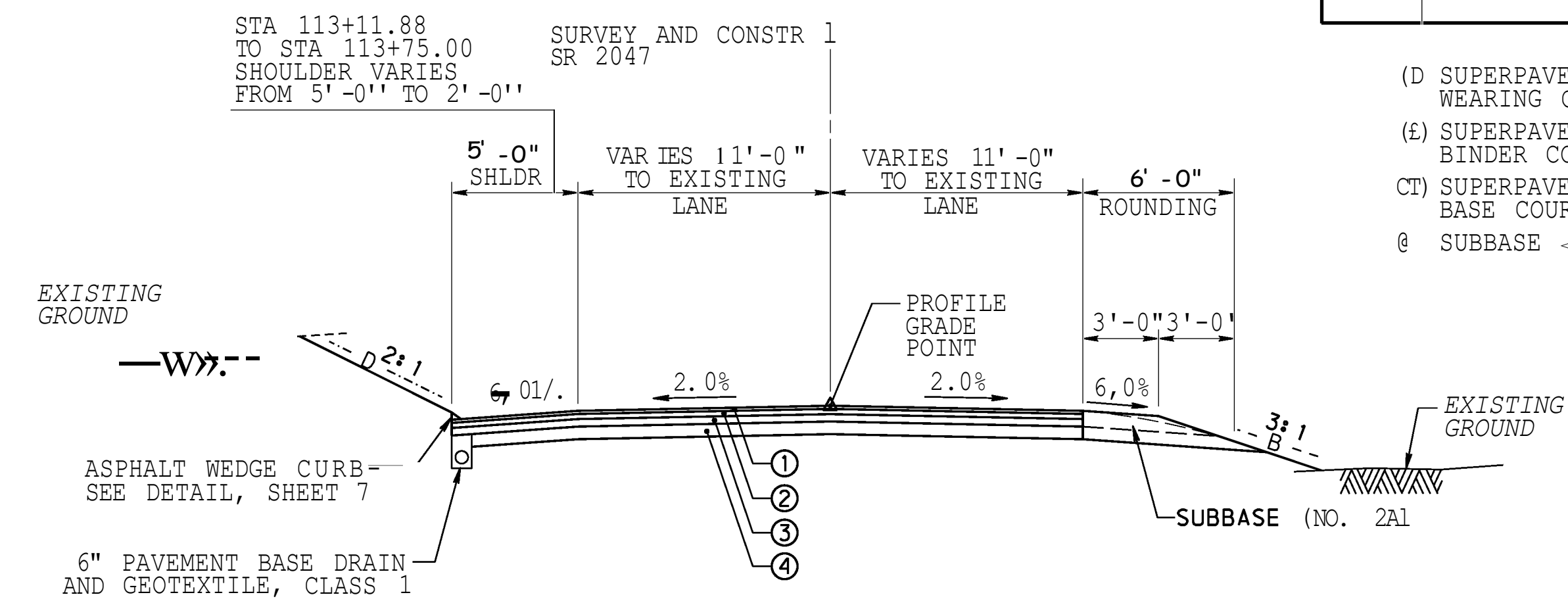
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* WESTMORELAND COUNTY
AND ARMSTRONG COUNTY

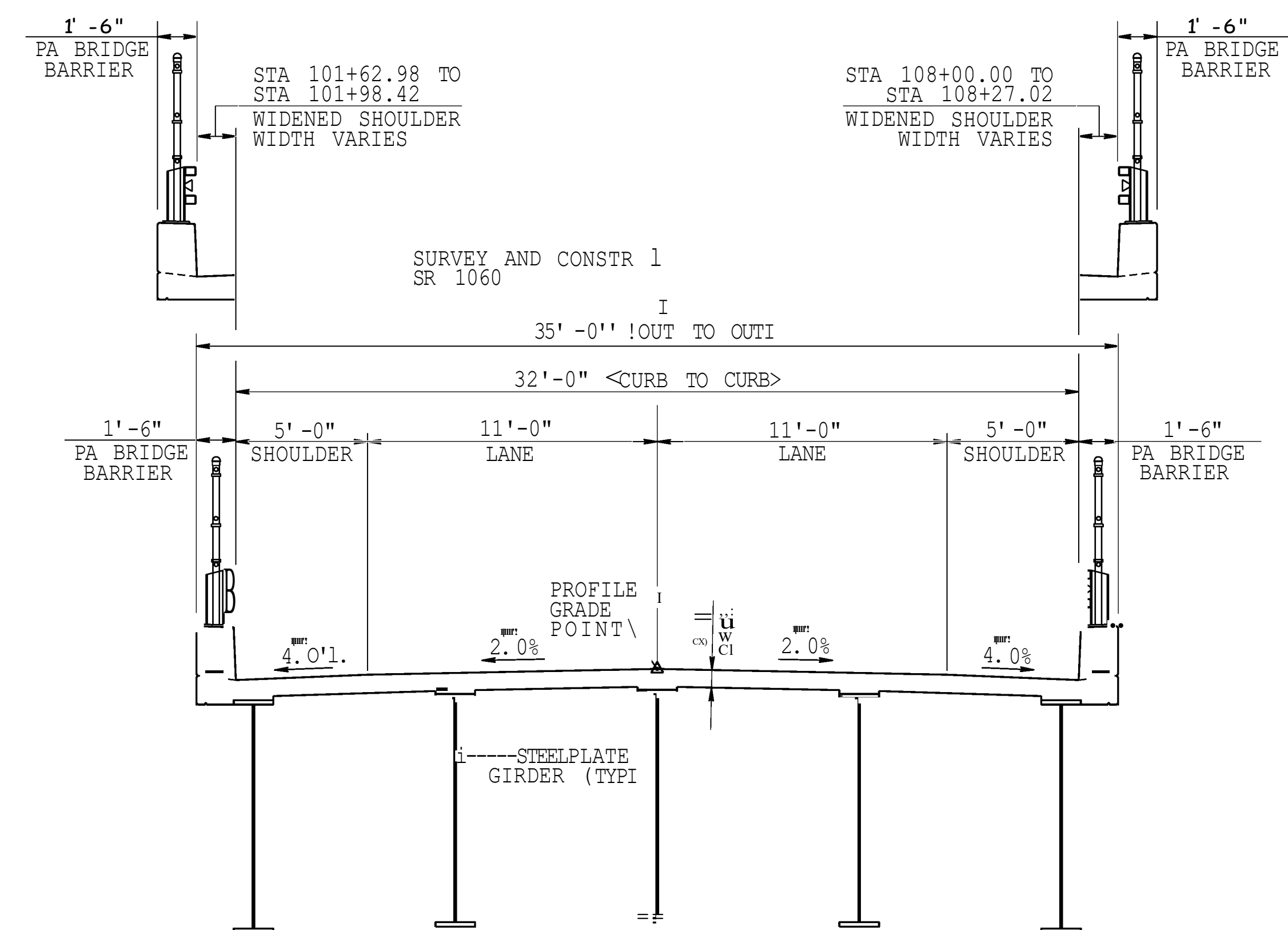
| DISTRICT | COUNTY | ROUTE | SECTION | SHEET |
|---|-----------|-------|---------|---------|
| 12-0 | * | 1060 | A20 | 6 OF 21 |
| BELL TOWNSHIP AND KISKIMINETAS TOWNSHIP | | | | |
| REVISION NUMBER | REVISIONS | DATE | BY | |
| | | | | |
| | | | | |
| | | | | |



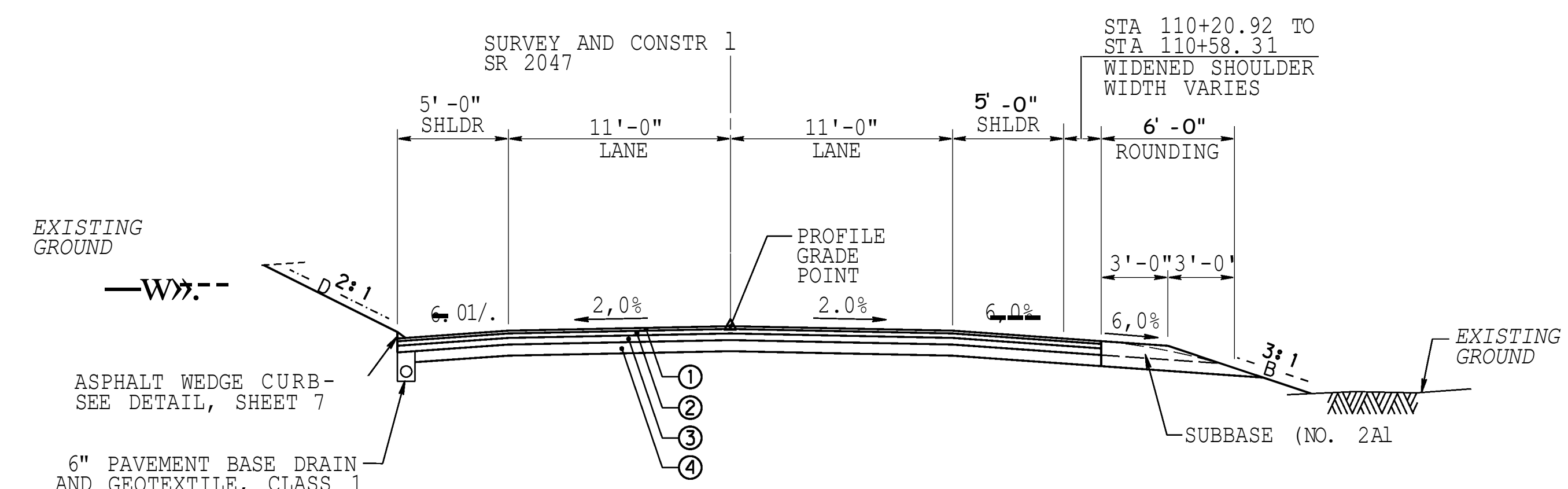
TYPICAL SECTION
SR 1060
STA 108+27.02 TO STA 108+52.02
NOT TO SCALE



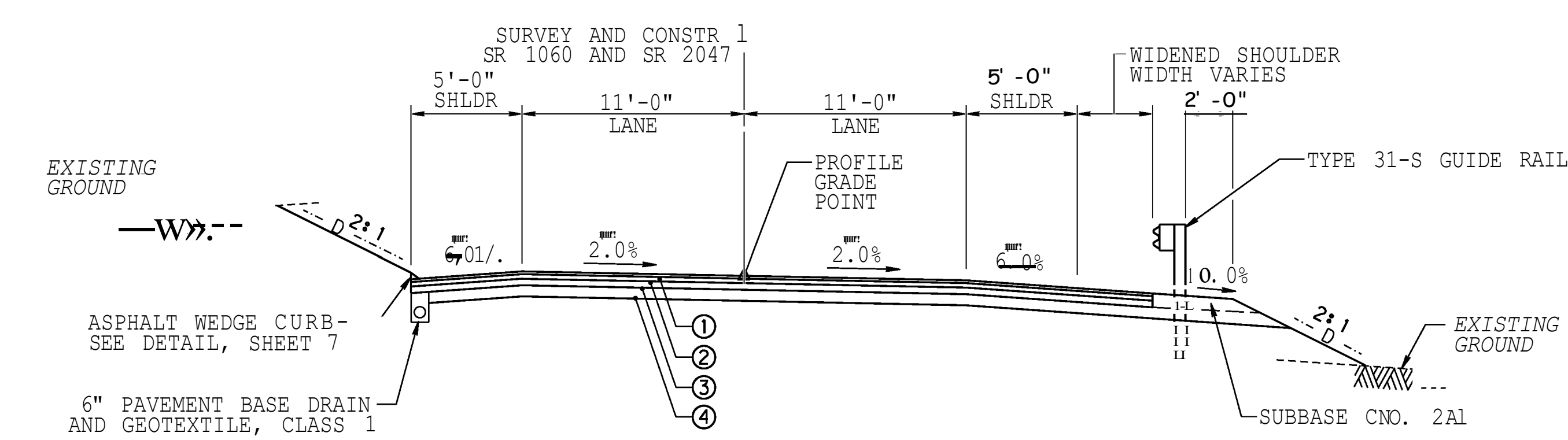
TYPICAL SECTION
SR 2047
STA 111+87.28 TO STA 113+75.00
NOT TO SCALE



TYPICAL BRIDGE SECTION
SR 1060
STA 101+62.98 TO STA 108+27.02
NOT TO SCALE



TYPICAL SECTION
SR 2047
STA 110+20.92 TO STA 111+87.28
NOT TO SCALE



TYPICAL SECTION
SR 1060 AND SR 2047
STA 108+52.02 TO STA 110+20.92
NOT TO SCALE

- (D) SUPERPAVE ASPHALT MIXTURE DESIGN, WEARING COURSE, DEPTH TO BE DETERMINED
- (E) SUPERPAVE ASPHALT MIXTURE DESIGN, BINDER COURSE, DEPTH TO BE DETERMINED
- (C) SUPERPAVE ASPHALT MIXTURE DESIGN, BASE COURSE, DEPTH TO BE DETERMINED
- @ SUBBASE <NO. 2A1, DEPTH TO BE DETERMINED

NOTES:
APPLY ALL ROADWAY JOINT SEALING IN ACCORDANCE WITH PUBLICATION 408 SECTION 469 (INCIDENTAL TO THE COURSE BEING PLACED).
APPLY SEPARATE LAYER OF ASPHALT TACK COAT FOR EACH PAVING COURSE, INCLUDING MULTIPLE LIFTS OF THE SAME MATERIAL.
REFER TO RC-10M FOR CUT SLOPE ROUNDING.
SEE CROSS SLOPE TRANSITION TABLE, SHEET 5,

SEEDING LEGEND
-- B --- B --- SEEDING & SOIL SUPPLEMENTS - FORMULA B SEED MIXTURE AND MULCHING-STRAW ON 4" OF TOPSOIL (3:1 AND FLATTER!)
-- D --- D --- SEEDING & SOIL SUPPLEMENTS - FORMULA D SEED MIXTURE (STEEPER THAN 3:1)

Exhibit "A"
Sheet 6 of 17

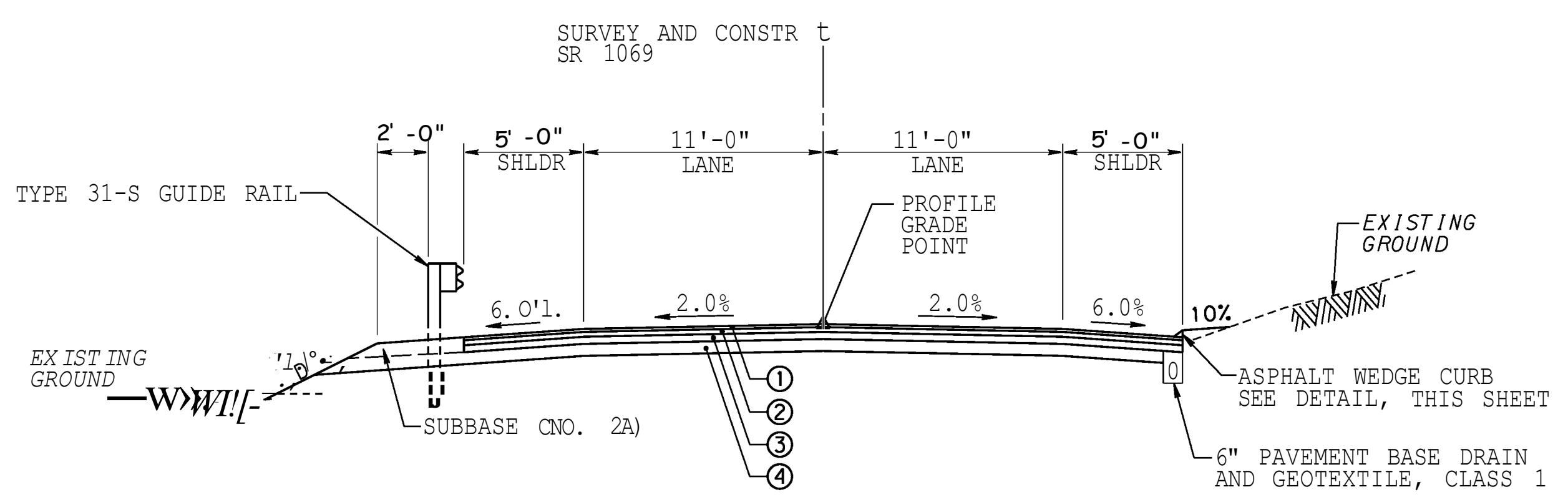
PLANS PREPARED BY
MICHAEL BAKER INTERNATIONAL
MOON TOWNSHIP, PA

TYPICAL SECTIONS

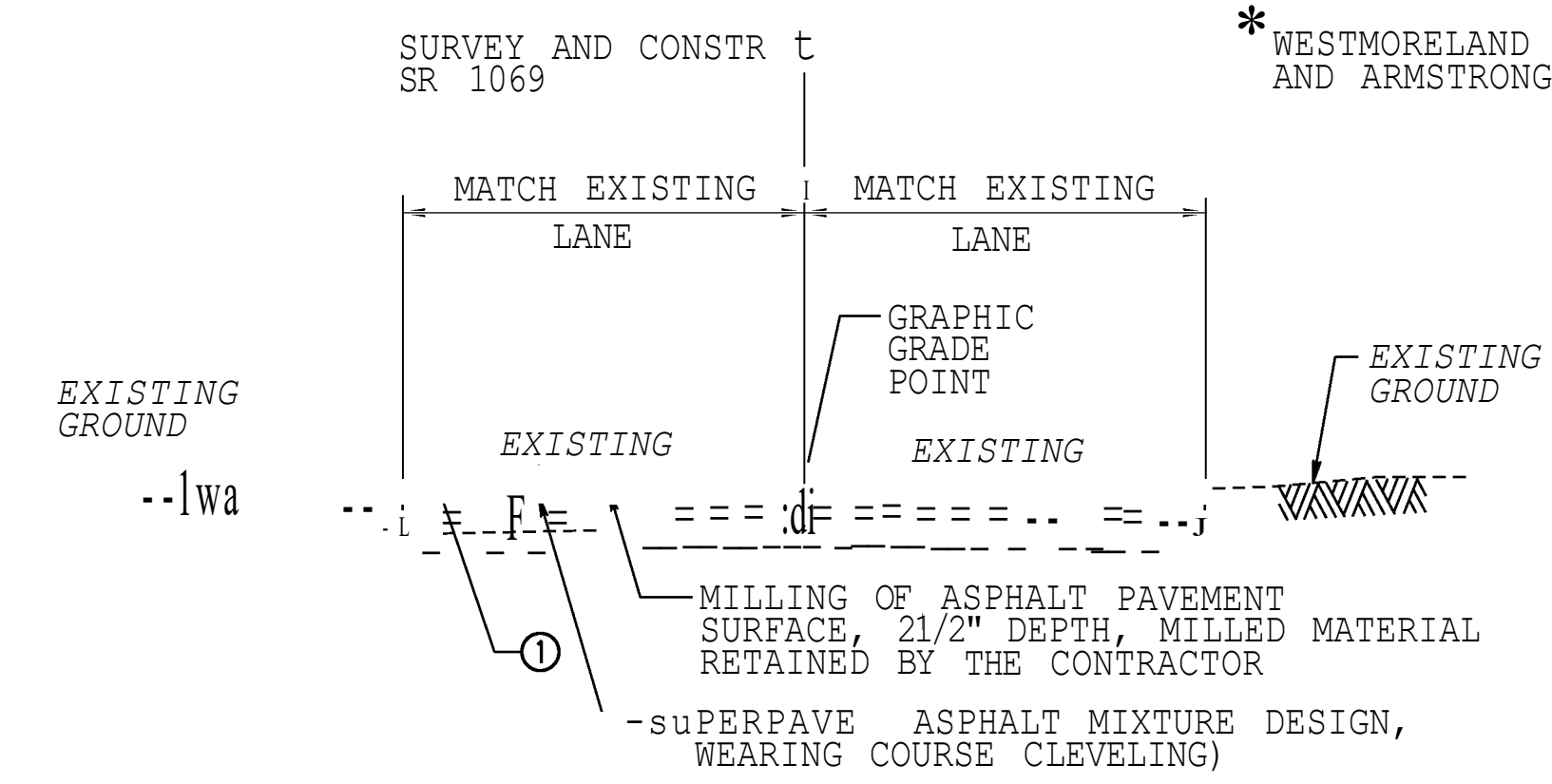
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| DISTRICT | COUNTY | ROUTE | SECTION | SHEET |
|---|-----------|-------|---------|---------|
| 12-0 | * | 1060 | A20 | 7 OF 21 |
| BELL TOWNSHIP AND KISKIMINETAS TOWNSHIP | | | | |
| REVISION NUMBER | REVISIONS | DATE | BY | |
| | | | | |
| | | | | |
| | | | | |

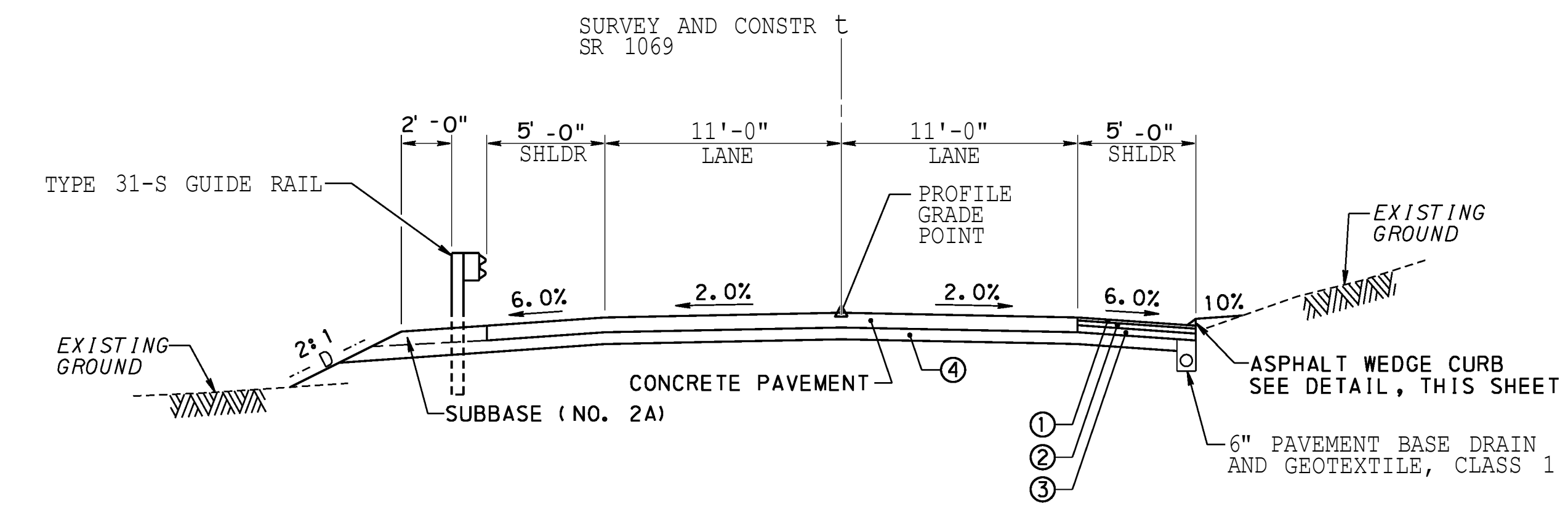
* WESTMORELAND COUNTY AND ARMSTRONG COUNTY



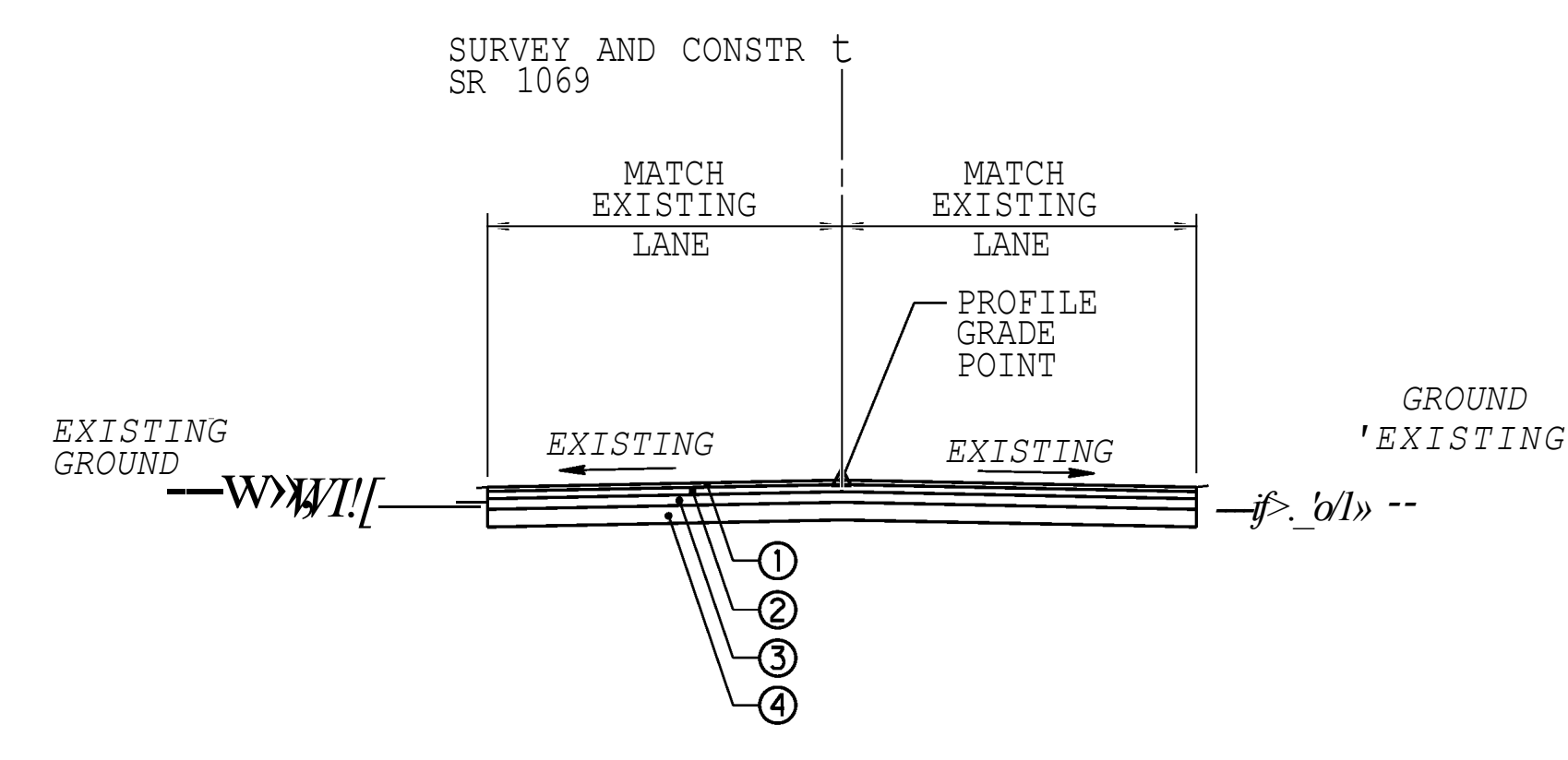
TYPICAL SECTION
SR 1069
STA 11+71.46 TO STA 15+50.00
NOT TO SCALE



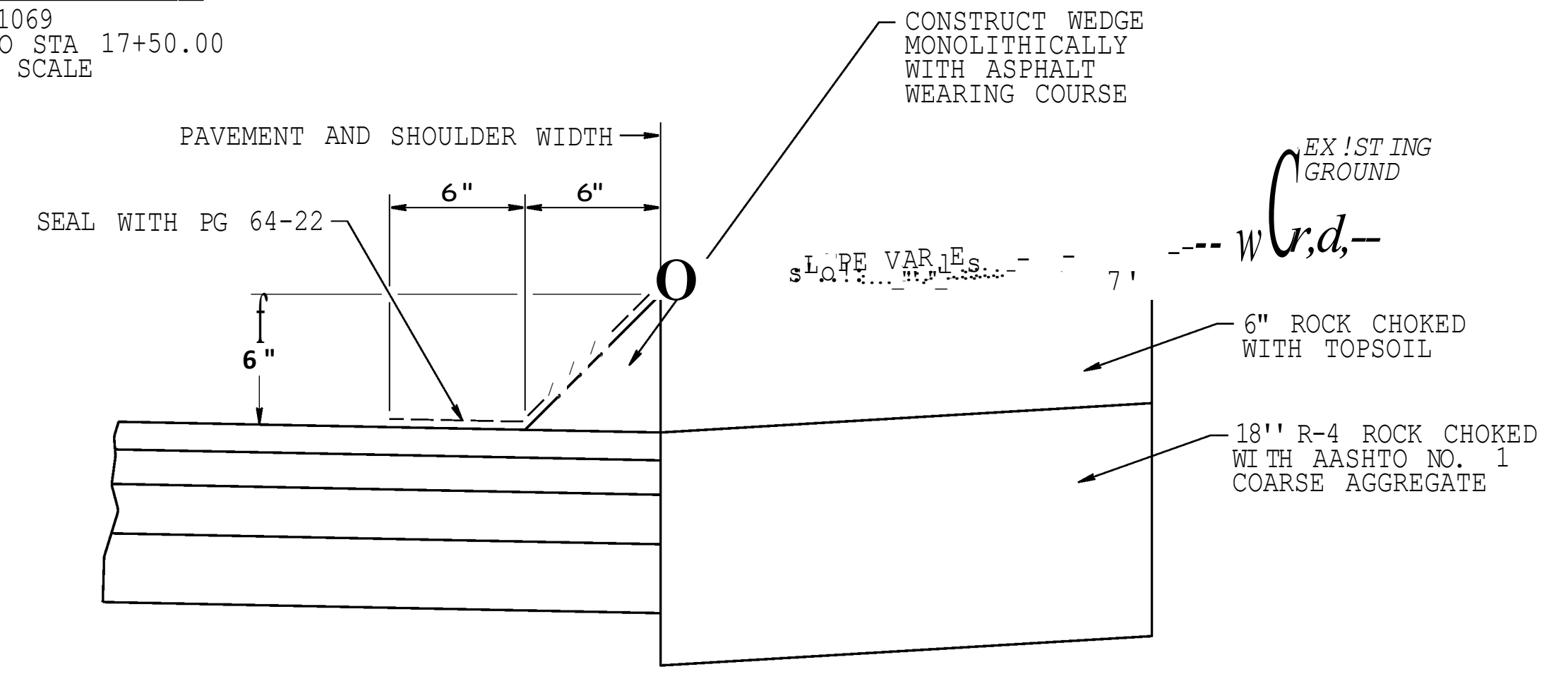
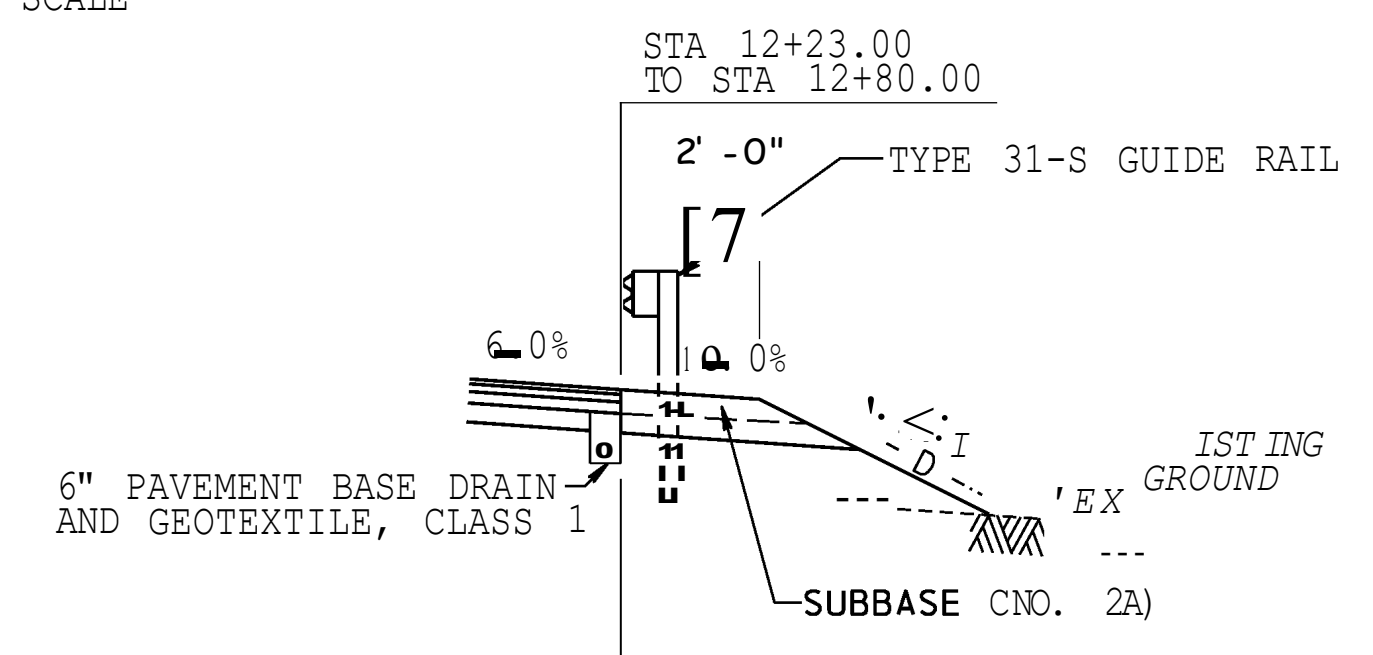
TYPICAL TANGENT SECTION
SR 1069
STA 17+50.00 TO STA 18+50.00
NOT TO SCALE



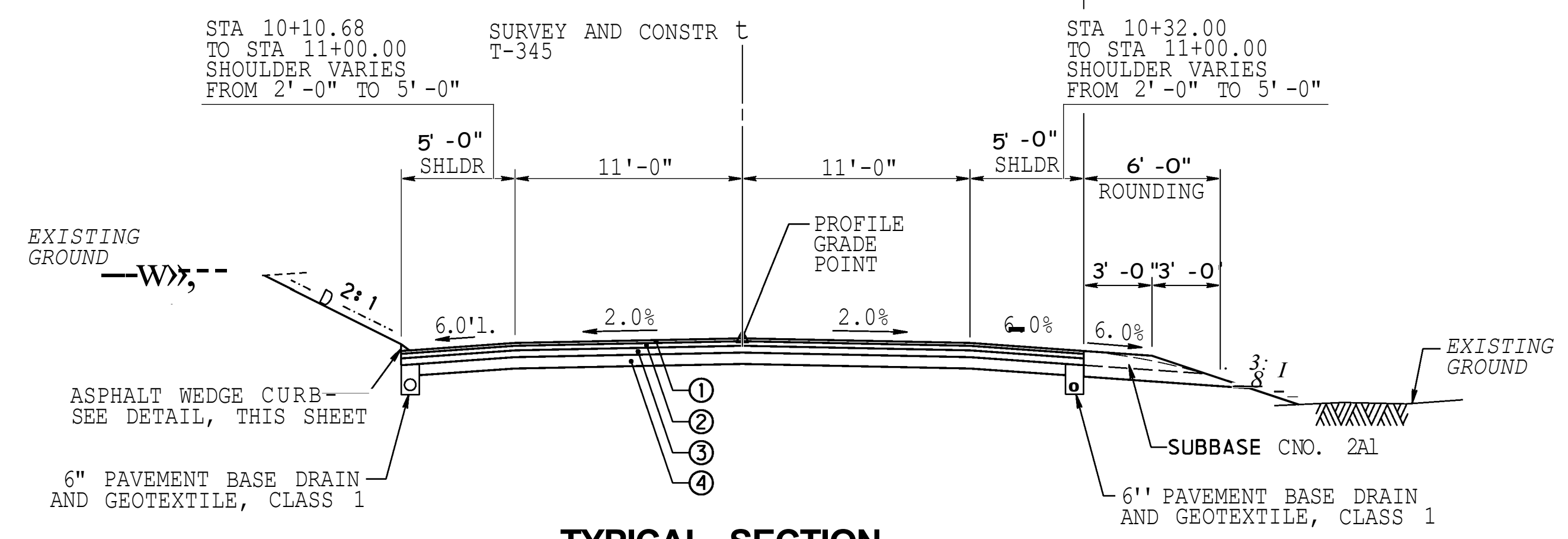
TYPICAL SECTION
SR 1069
STA 10+57.34 TO STA 11+71.46
NOT TO SCALE



TYPICAL SECTION
SR 1069
STA 15+50.00 TO STA 17+50.00
NOT TO SCALE



ASPHALT WEDGE CURB DETAIL
NOT TO SCALE
NO PAY ITEM



TYPICAL SECTION
T-345
STA 10+00.00 TO STA 13+73.30
NOT TO SCALE

NOTES:
APPLY ALL ROADWAY JOINT SEALING IN ACCORDANCE WITH PUBLICATION 408 SECTION 469 IDENTICAL TO THE COURSE BEING PLACED.
APPLY SEPARATE LAYER OF ASPHALT TACK COAT FOR EACH PAVING COURSE, INCLUDING MULTIPLE LIFTS OF THE SAME MATERIAL.
REFER TO RC-10M FOR CUT SLOPE ROUNDING.

SEEDING LEGEND
-- B --- B -- SEEDING & SOIL SUPPLEMENTS - FORMULA B SEED MIXTURE AND MULCHING-STRAW ON 4" OF TOPSOIL C3:1 AND FLATTER)
-- D --- D -- SEEDING & SOIL SUPPLEMENTS - FORMULA D SEED MIXTURE CSTEPPER THAN 3:11

PLANS PREPARED BY
MICHAEL BAKER INTERNATIONAL
MOON TOWNSHIP, PA

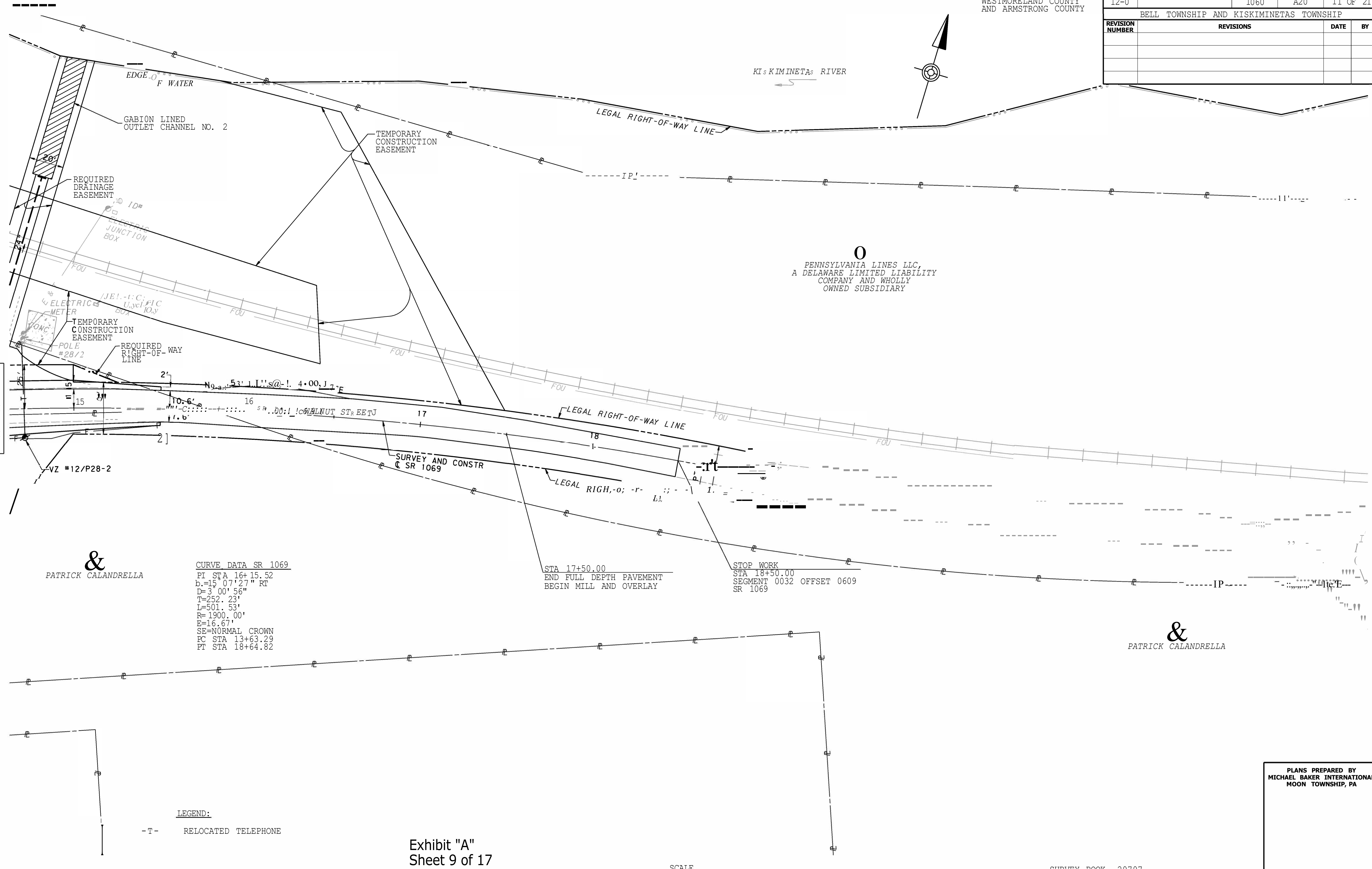
TYPICAL SECTIONS

Exhibit "A"
Sheet 7 of 17

10:28:45 AM 10/21/2020 10:21:2020

* WESTMORELAND COUNTY
AND ARMSTRONG COUNTY

| | | | | |
|---|-----------|-------|---------|----------|
| DISTRICT | COUNTY | ROUTE | SECTION | SHEET |
| 12-0 | * | 1060 | A20 | 11 OF 21 |
| BELL TOWNSHIP AND KISKIMINETAS TOWNSHIP | | | | |
| REVISION NUMBER | REVISIONS | DATE | BY | |
| | | | | |
| | | | | |



0
PENNSYLVANIA LINES LLC,
A DELAWARE LIMITED LIABILITY
COMPANY AND WHOLLY
OWNED SUBSIDIARY

&
PATRICK CALANDRELLA

CURVE DATA SR 1069
 PI STA 16+15.52
 b=15' 07" 2.7" RT
 D=3' 00" 56"
 T=252.23'
 L=501.53'
 R=1900.00'
 E=16.67'
 SE=NORMAL CROWN
 PC STA 13+63.29
 PT STA 18+64.82

STA 17+50.00
END FULL DEPTH PAVEMENT
BEGIN MILL AND OVERLAY

STOP WORK
STA 18+50.00
SEGMENT 0032 OFFSET 0609
SR 1069

&
PATRICK CALANDRELLA

LEGEND:
-T- RELOCATED TELEPHONE

Exhibit "A"
Sheet 9 of 17



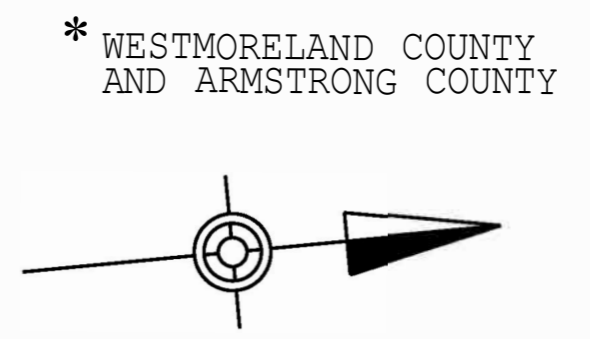
SURVEY BOOK 29797
FOR PROFILE, SEE SHEETS 19& 20

PLANS PREPARED BY
MICHAEL BAKER INTERNATIONAL
MOON TOWNSHIP, PA

N:\sp_1990_2010\proj\1060\sr_1069\plan\1069_09.dwg
 DES: CJS DWG: BSP CRD: DLJ

| DISTRICT | COUNTY | ROUTE | SECTION | SHEET |
|---|--------------|-------|---------|----------|
| 12-0 | WESTMORELAND | 1060 | A20 | 12 OF 21 |
| BELL TOWNSHIP AND KISKIMINETAS TOWNSHIP | | | | |
| REVISION NUMBER | REVISIONS | DATE | BY | |
| | | | | |

| PROPOSED STRUCTURE DATA | |
|---------------------------------|--|
| SR 1060 OVER KISKIMINETAS RIVER | |
| STATION | 104+95.00 |
| TYPE | 3-SPAN CONT COMP STEEL PLATE GIRDER BRIDGE |
| SPAN LENGTH | 200'-0", 250'-0", 200'-0" |
| UNDERCLEARANCE | 24'-7" |
| ROADWAY WIDTH | 32'-0" |
| SKEW | 90°00'00" |
| STRUCTURE NO | S-39414 |
| RECOMMENDED DATE | |
| EXISTING STRUCTURE DATA | |
| STATION | 105+60000 |
| TYPE | 3-SPAN STEEL THRU TRUSS BRIDGE |
| SPAN LENGTH | 220'-0", 220'-0", 220'-0" |
| UNDERCLEARANCE | 20'-9" \pm |
| CLEAR ROADWAY WIDTH | 18'-9" |
| SKEW | 90°00'00" |
| STRUCTURE NO | S-12902, S-17680, S-36670 |
| BUILT DATE | 1906 |



PROPERTY TABULATION

- 0 BIG VISIONS CORPORATION, A CORPORATION EXISTING UNDER THE LAWS OF THE COMMONWEALTH OF PENNSYLVANIA
- 0 PENNSYLVANIA LINES LLC, A DELAWARE LIMITED LIABILITY COMPANY AND WHOLLY OWNED SUBSIDIARY
- 4 ELDEN L. KING, JR AND CINDY L. KING, HUSBAND AND WIFE
- 5 ANTONIO CALL/PARE AND MARYE. CALL/PARE, HUSBAND & WIFE
- 6 DAVID A. CARNEY, A SINGLE MAN AND DEBORAH SENGE, A SINGLE WOMAN
- 7 JOHN O. GARTLEY AND ROSANNA GARTLEY, HIS WIFE
- 8 EDWARD USKO, JR. AND KAREN M. USKO, HUSBAND AND WIFE

- LEGEND:**
- TU - RELOCATED TELEPHONE
 - (!)-END STRUCTURE BEGIN APPROACH SLAB STA 108+27.02
 - (u)-END APPROACH SLAB STA 108+52.02
 - t) THRIE-BEAM TO PA BRIDGE BARRIER TRANSITION

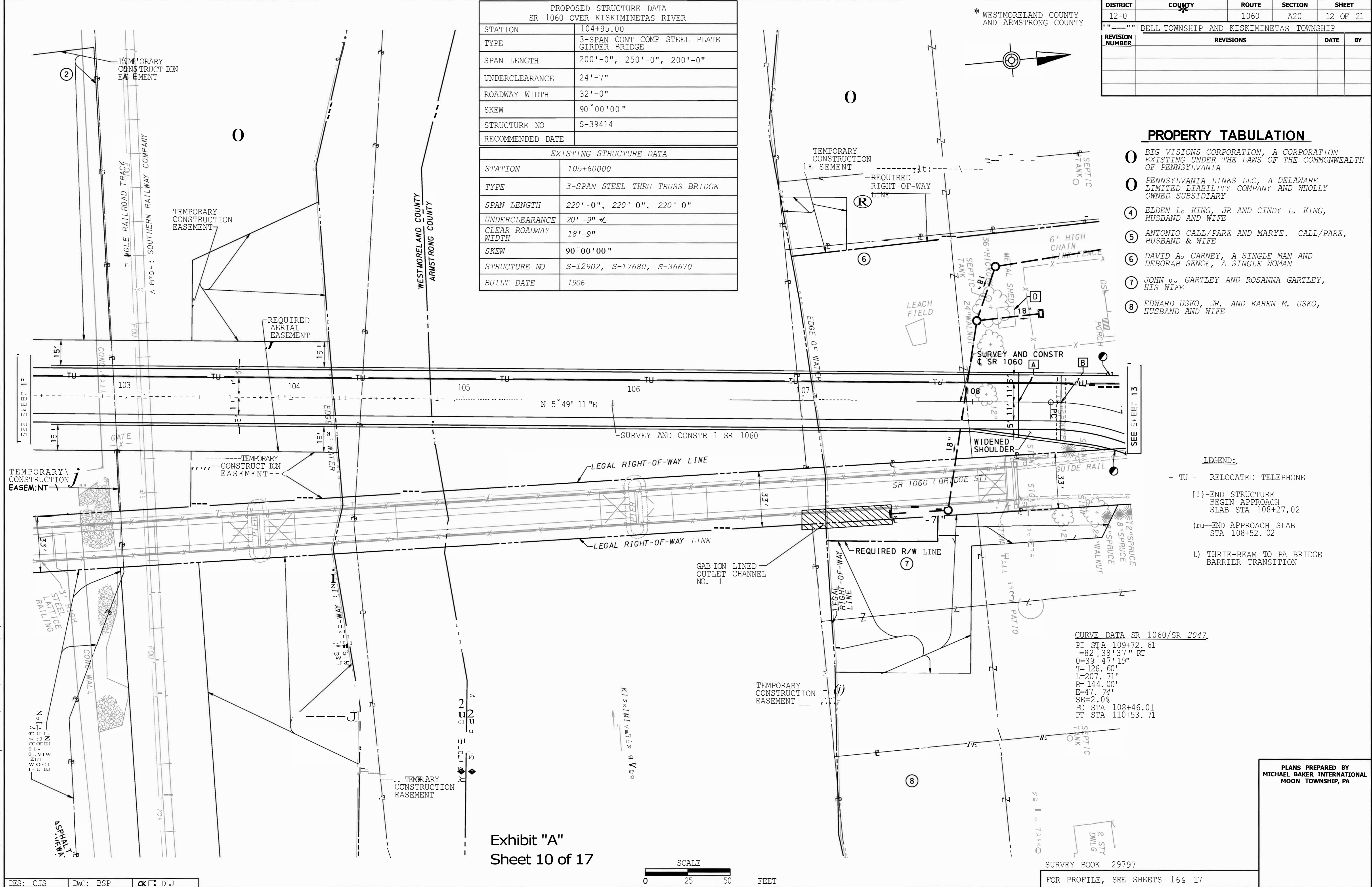
CURVE DATA SR 1060/SR 2047
 PI STA 109+72.61
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 O=39'47'19"
 T=126.60'
 L=207.71'
 R=144.00'
 E=47.74'
 SE=2.0%
 PC STA 108+46.01
 PT STA 110+53.71

Exhibit "A"
 Sheet 10 of 17



SURVEY BOOK 29797
 FOR PROFILE, SEE SHEETS 16 & 17

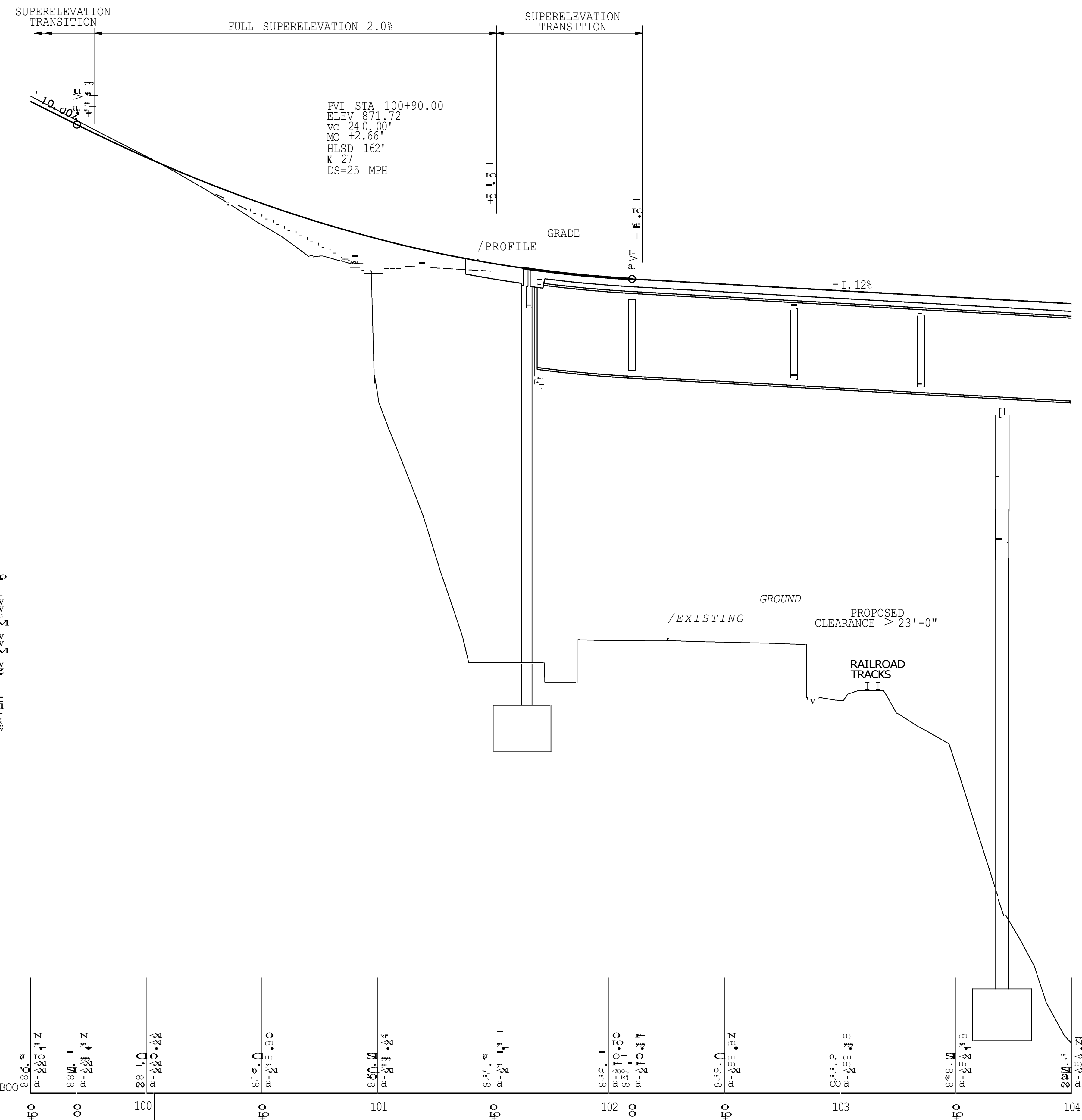
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PLANS PREPARED BY
 MICHAEL BAKER INTERNATIONAL
 MOON TOWNSHIP, PA

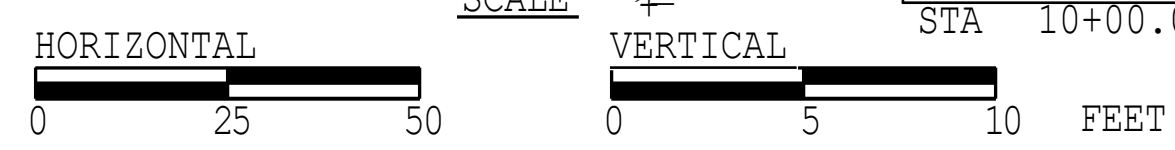
* WESTMORELAND COUNTY
AND ARMSTRONG COUNTY

| DISTRICT | COUNTY | ROUTE | SECTION | SHEET |
|---|-----------|-------|---------|----------|
| 12-0 | * | 1060 | A20 | 16 OF 21 |
| BELL TOWNSHIP AND KISKIMINETAS TOWNSHIP | | | | |
| REVISION NUMBER | REVISIONS | DATE | BY | |
| | | | | |
| | | | | |
| | | | | |



MATCH TO NE SEE SHEET 6

MATCH TO NE SEE SHEET 7



STA 100+03.52 SURVEY AND CONSTR SR 1060 =
STA 10+00.00 SURVEY AND CONSTR SR 1069 =

PROFILE SR 1060

Exhibit "A"
Sheet 12 of 17

PLANS PREPARED BY
MICHAEL BAKER INTERNATIONAL
MOON TOWNSHIP, PA

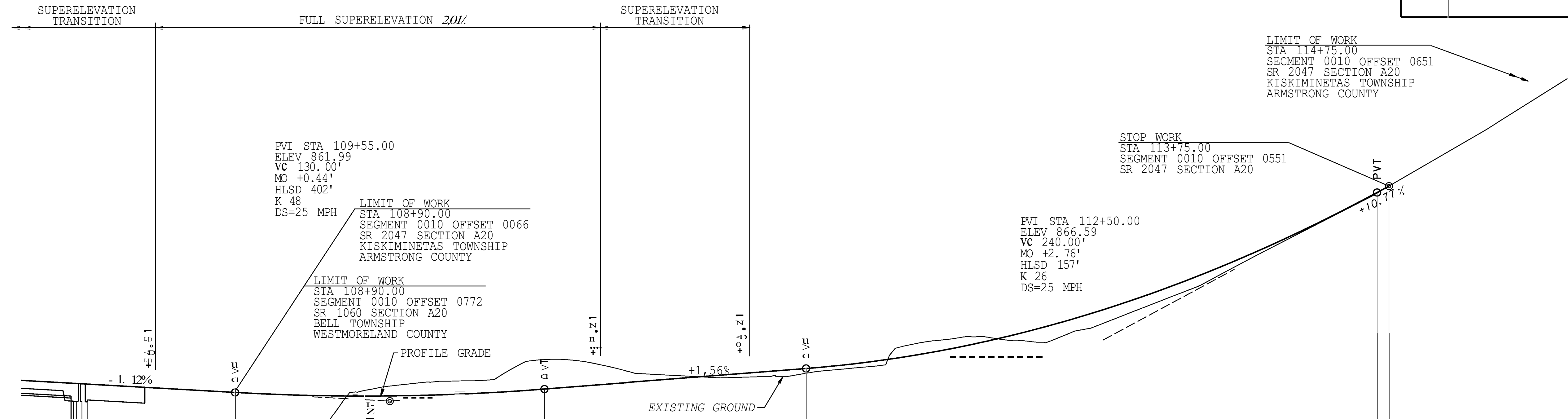
SURVEY BOOK #29797
FOR PLAN, SEE SHEETS 10 & 12

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DES: CJS | DWG: BSP | CRD: DLJ

* WESTMORELAND COUNTY
AND ARMSTRONG COUNTY

| DISTRICT | COUNTY | ROUTE | SECTION | SHEET |
|---|-----------|-------|---------|----------|
| 12-0 | * | 1060 | A20 | 18 OF 21 |
| BELL TOWNSHIP AND KISKIMINETAS TOWNSHIP | | | | |
| REVISION NUMBER | REVISIONS | DATE | BY | |
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10:33:49 AM
 10:27:2520
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DATE: 11/28/17
 DRAWN BY: JCS

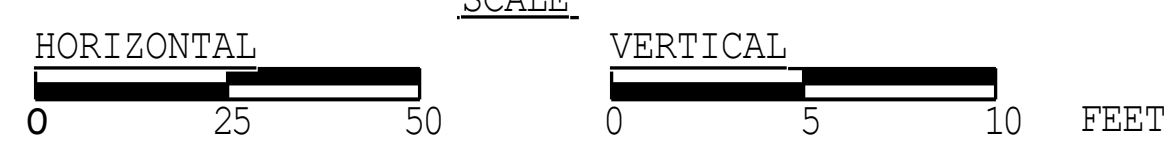
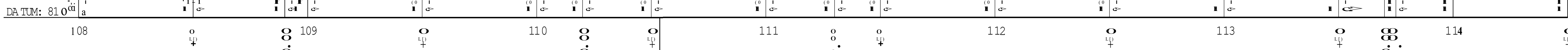


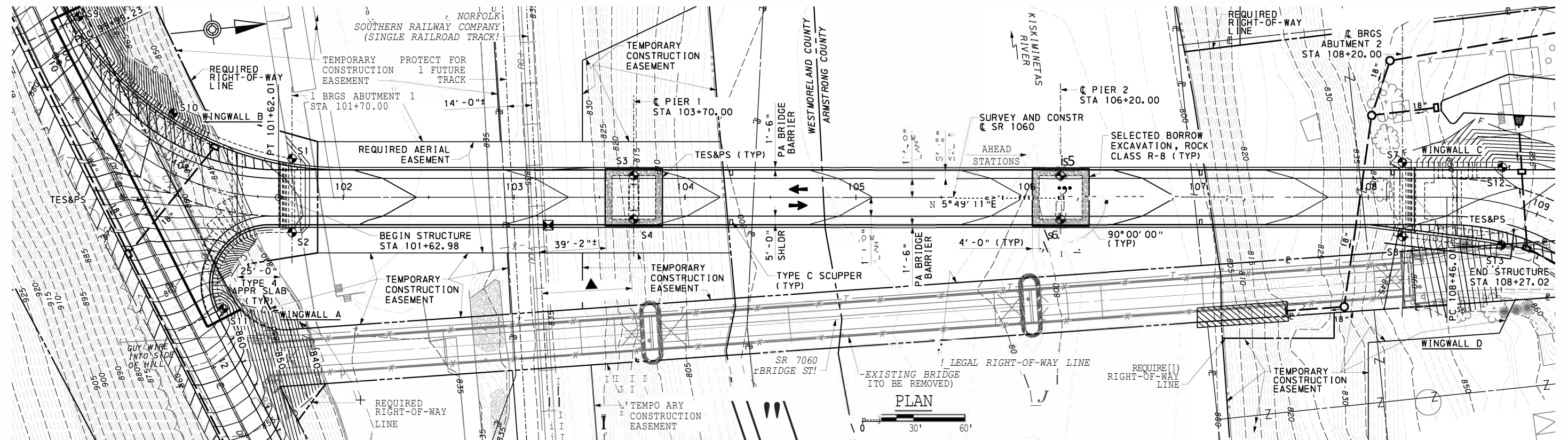
Exhibit "A"
Sheet 14 of 17

PROFILE SR 1060 /SR 2047

STA 110+53.71 SURVEY AND CONSTR C SR 2047 =
 STA 14+30.65 SURVEY AND CONSTR I T-345

SURVEY BOOK w29797
FOR PLAN, SEE SHEETS 13 & 14

PLANS PREPARED BY
MICHAEL BAKER INTERNATIONAL
MOON TOWNSHIP, PA



HORIZONTAL CURVE DATA SR 1060

PI STA = 100+90.56
 Δ = 64°46'04" LT
 D = 39°47'19"
 T = 91.33'
 L = 162.78'
 R = 144.00'
 E = 26.52'
 SE = 2.0%
 PCC STA 99+99.23
 PT STA 101+62.01

HORIZONTAL CURVE DATA SR 1060/SR 2047

PI STA = 109+72.61
 Δ = 82°38'37" RT
 D = 39°47'19"
 T = 126.60'
 L = 207.71'
 R = 144.00'
 E = 47.74'
 SE = 2.0%
 PC STA 108+46.01
 PT STA 110+53.71

| INDEX OF DRAWINGS | |
|-------------------|-----------------------------------|
| SHEET NO | TITLE |
| 1 | TYPE, SIZE & LOCATION PLAN |
| 2 | TYPE, SIZE & LOCATION ELEVATION |
| 3 | TYPE, SIZE & LOCATION TYP SECTION |

- LEGEND:**
- + INDICATES TRAFFIC DIRECTION
 - - - EXISTING CONTOURS 1'-0" INTERVALS!
 - PROPOSED CONTOURS (1'-0" INTERVALS)
 - ⊕ PROPOSED CORE BORING LOCATIONS
 - ⊠ POINT OF MINIMUM VERTICAL CLEARANCE STA 103+19.97 OFFSET 16.25' RT TOP OF RAIL ELEV 835.50
 - TES&PS TEMPORARY EXCAVATION SUPPORT AND PROTECTION SYSTEM
 - ▲ MINIMUM HORIZONTAL CLEARANCE: 52' -10 1/2" PROVIDED 25' -0" REQUIRED
 - T- EXISTING TELEPHONE
 - FOU- EXISTING FIBER OPTIC
 - CTV- EXISTING CABLE TV

HYDRAULIC DATA:

DRAINAGE AREA = 1440.00 SQUARE MILES

25 YEAR FLOOD (DESIGN STORM)
 DISCHARGE = 47,000 CFS
 PROPOSED WATER SURFACE ELEVATION = 813.03
 PROPOSED CHANNEL VELOCITY = 10.6 FPS

100 YEAR FLOOD
 DISCHARGE = 65,254 CFS
 PROPOSED WATER SURFACE ELEVATION = 815.92
 PROPOSED CHANNEL VELOCITY = 12.0 FPS

FLOOD OF RECORD: MARCH 18, 1936 ELEV 852.84

NOTES:

AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS 2017, AND AS SUPPLEMENTED BY DESIGN MANUAL, PART 4, DECEMBER 2019 EDITION.

LIVE LOAD DISTRIBUTION TO BEAMS IS BASED UPON DM-4 DISTRIBUTION FACTORS APPROXIMATE METHOD.

DESIGN IS IN ACCORDANCE WITH THE LRFD METHOD.

PHL-93, ML-80, TK527, P-82 (204 KIP PERMIT LOAD) OR P2016-13 (330 KIP PERMIT LOAD)

FATIGUE DESIGN IS BASED ON THE FOLLOWING:
 STEEL STRUCTURES: ADT 19 (2041)
 (ONE DIRECTIONAL!)

INCLUDES SURFACE AREA DENSITY OF 0.030 KSF FOR FUTURE WEARING SURFACE ON THE DECK SLAB.

INCLUDES A SURFACE AREA DENSITY OF 0.015 KSF FOR PERMANENT METAL DECK FORMS WHICH TAKES INTO ACCOUNT THE WEIGHT OF THE FORM, PLUS THE WEIGHT OF THE CONCRETE IN THE VALLEYS OF THE FORMS.

INCLUDES 0.015 KIP/FT FOR UTILITIES.

INCLUDES 0.026 KIP/FT FOR PROTECTIVE FENCE OVER RAILROAD PROPERTY.

THE CONTRACTOR SHALL PROVIDE OPEN ACCESS ALONG THE NORFOLK SOUTHERN MAINTENANCE WAY THROUGHOUT CONSTRUCTION.

TEMPORARY RAILROAD CLEARANCES:
 22' -0" VERTICAL ABOVE TOP OF HIGHEST RAIL
 13' -0" HORIZONTAL FROM 1 TANGENT TRACK
 14' -0" HORIZONTAL FROM 1 CURVED TRACK
 EXISTING SUBSTANDARD CLEARANCES SHALL NOT BE FURTHER REDUCED WITHOUT WRITTEN PERMISSION FROM NS.

| PROPOSED CORE BORINGS | | | |
|-----------------------|---------|--------|----|
| BORING NUMBER | STATION | OFFSET | |
| S1 | 101+70 | 22.75' | LT |
| S2 | 101+70 | 20.50' | RT |
| S3 | 103+70 | 12.75' | LT |
| S4 | 103+70 | 12.75' | RT |
| S5 | 106+20 | 12.75' | LT |
| S6 | 106+20 | 12.75' | RT |
| S7 | 108+20 | 20.50' | LT |
| S8 | 108+20 | 23.00' | RT |
| S9 | 99+84 | 22.00' | LT |
| S10 | 100+79 | 31.00' | LT |
| S11 | 101+40 | 67.50' | RT |
| S12 | 108+75 | 20.75' | LT |
| S13 | 108+85 | 23.75' | RT |

| DESCRIPTION | DWG NO | RECM'D DATE |
|---|---------|-------------|
| CLASSIFICATION OF EARTHWORK FOR STRUCTURES | RC-11M | 6/1/2010 |
| BACKFILL AT STRUCTURES | RC-12M | 2/8/2019 |
| GUIDE RAIL TO BRIDGE BARRIER TRANSITIONS | RC-50M | 2/8/2019 |
| PROTECTIVE FENCE | BC-701M | 1/31/2019 |
| PA BRIDGE BARRIER | BC-713M | 1/31/2019 |
| PERMANENT METAL DECK FORMS | BC-732M | 1/31/2019 |
| ANCHOR SYSTEMS | BC-734M | 1/31/2019 |
| WALL CONSTRUCTION AND EXPANSION JOINT DETAILS | BC-735M | 9/30/2016 |
| REINFORCEMENT BAR FABRICATION DETAILS | BC-736M | 1/31/2019 |
| BRIDGE DRAINAGE | BC-751M | 1/31/2019 |
| CONCRETE DECK SLAB DETAILS | BC-752M | 9/30/2016 |
| STEEL GIRDER DETAILS | BC-753M | 1/31/2019 |
| STEEL DIAPHRAGMS FOR STEEL BEAM/GIRDER STRUCTURES (STRAIGHT GIRDERS ONLY) | BC-754M | 1/31/2019 |
| HIGH LOAD MULTI ROTATIONAL POT BEARINGS | BC-756M | 9/30/2016 |
| STEEL PILE TIP REINFORCEMENTS & SPLICES | BC-757M | 9/30/2016 |
| TOOTH EXPANSION DAM FOR PRESTRESSED CONCRETE & STEEL BEAM BRIDGES | BC-762M | 1/31/2019 |
| TYPICAL WATERPROOFING AND EXPANSION DETAILS | BC-788M | 1/31/2019 |

PREPARED BY:
Michael Baker
INTERNATIONAL
 AIRSIDE BUSINESS PARK
 100 AIRSIDE DRIVE
 MOON TOWNSHIP, PA 15108
 PHONE: (412) 269-6300

Exhibit "A"
 Sheet 15 of 17

| Mar-k | Descr- iption | By | Chk' d. | Reem' d. | Date |
|-----------|---------------|----|---------|----------|------|
| REVISIONS | | | | | |

SR 1060 PREVIOUSLY KNOWN AS LR 64261
 BMS STR ID: 64-1060-0010-0037 MPMS/ECMS PROJ: 81747 BRKEY: 67409

COMMONWEALTH OF PENNSYLVANIA
 DEPARTMENT OF TRANSPORTATION

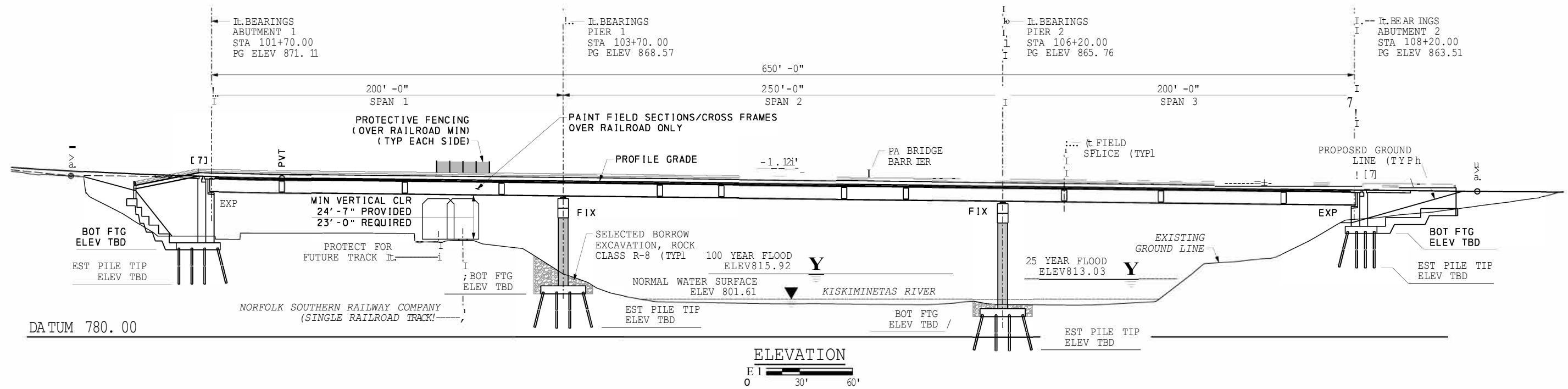
WESTMORELAND & ARMSTRONG COUNTIES
 SR 1060 SECTION A20
 SEGMENT 0010 OFFSET 0037
 SR 1060 STA 104+95.00 OVER KISKIMINETAS RIVER & NORFOLK SOUTHERN RAILWAY COMPANY

3 SPAN CONT COMP STEEL PLATE GIRDER TYPE, SIZE & LOCATION PLAN

RECOMMENDED _____
 CHIEF BRIDGE ENGINEER

SHEET 1 OF 3
 + SUPPLEMENTAL DRAWINGS
 S - 39414

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LEGEND:

- [7] MOVEMENT CLASSIFICATION TOOTH EXPANSION DAM
- EXP EXPANSION POT BEARING
- FIX FIXED POT BEARING
- T** WATER SURFACE ELEVATION

| Mar-k | Desc-ription | By | Chk'd | Reem'd | Date |
|-----------|--------------|----|-------|--------|------|
| REVISIONS | | | | | |

SR 1060 PREVIOUSLY KNOWN AS LR 64261
 BMS STR ID: 64-1060-0010-0037 MPMS/ECMS PROJ: 81747 BRKEY: 67409

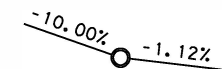
COMMONWEALTH OF PENNSYLVANIA
 DEPARTMENT OF TRANSPORTATION

WESTMORELAND & ARMSTRONG COUNTIES
 SR 1060 SECTION A20
 SEGMENT 0010 OFFSET 0037
 SR 1060 STA 104+95.00 OVER KISKIMINETAS RIVER &
 NORFOLK SOUTHERN RAILWAY COMPANY

3 SPAN CONT COMP STEEL PLATE GIRDER
 TYPE, SIZE & LOCATION ELEVATION

| | |
|-------------------|--------------|
| RECOMMENDED _____ | SHEET 2 OF 3 |
| | S - 39414 |

VERTICAL CURVE DATA



PVI STA = 100+90.00
 PVI ELEV = 871.72
 VC = 240.00'
 MO = 2.66'

VERTICAL CURVE DATA

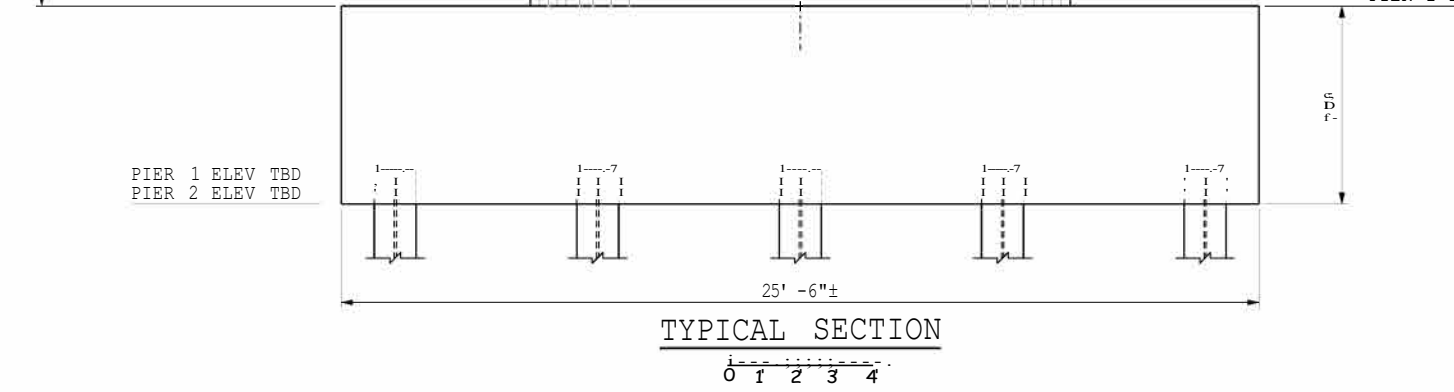
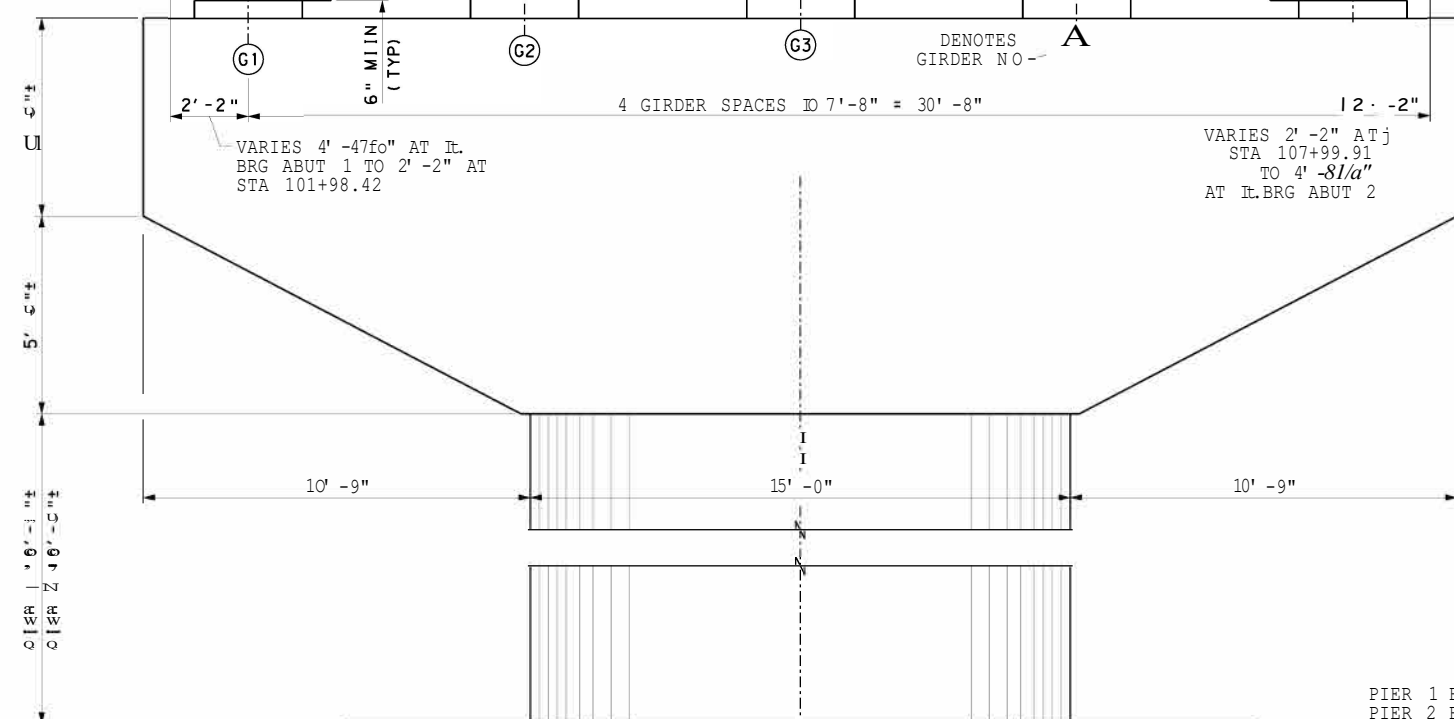
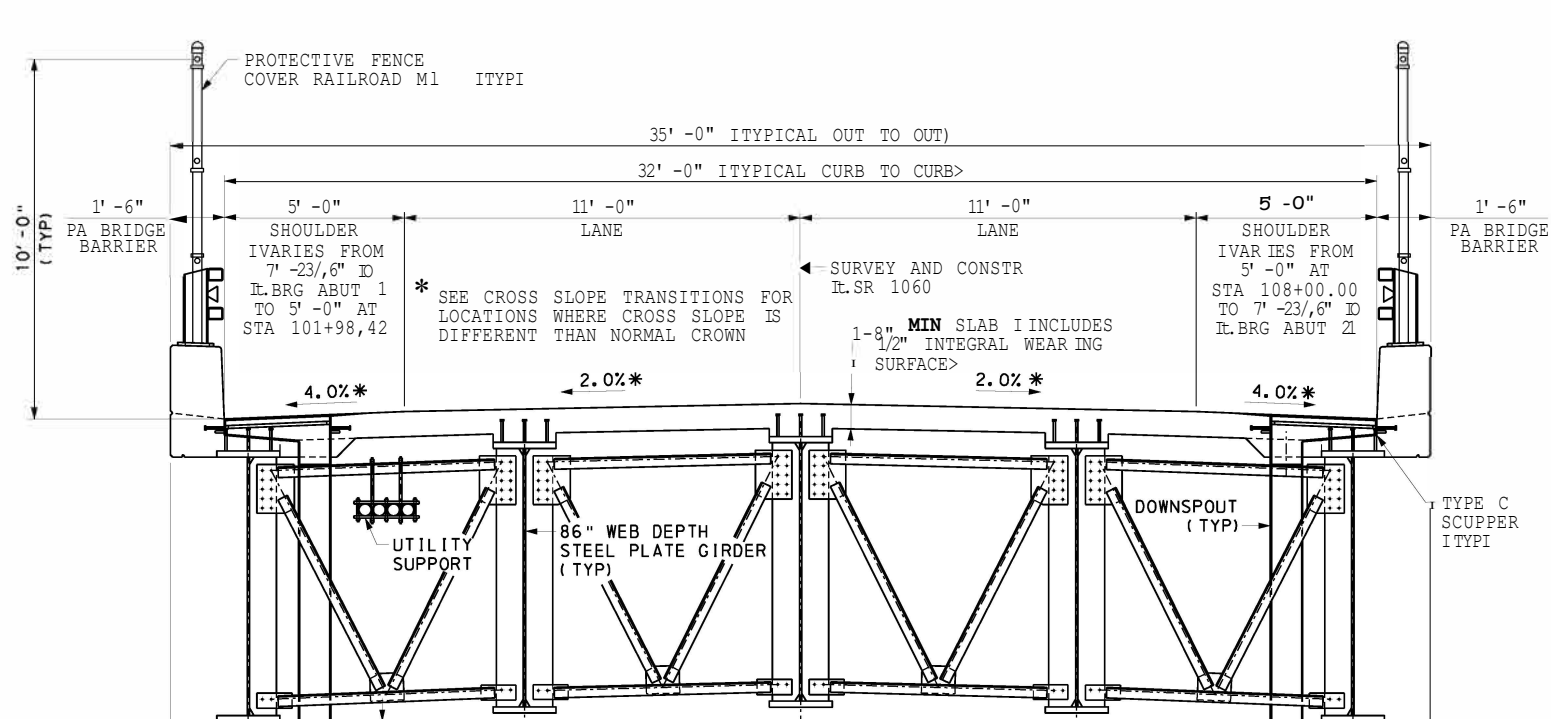
PVI STA = 109+55.00
 PVI ELEV = 861.99
 VC = 130.00'
 MO = 0.44'

NOTES:

- TEMPORARY RAILROAD CLEARANCES •
- 22'-0" VERTICAL ABOVE TOP OF HIGHEST RAIL
- 13'-0" HORIZONTAL FROM TANGENT TRACK
- 14'-0" HORIZONTAL FROM CURVED TRACK
- EXISTING SUBSTANDARD CLEARANCES SHALL NOT BE FURTHER REDUCED WITHOUT WRITTEN PERMISSION FROM NS.

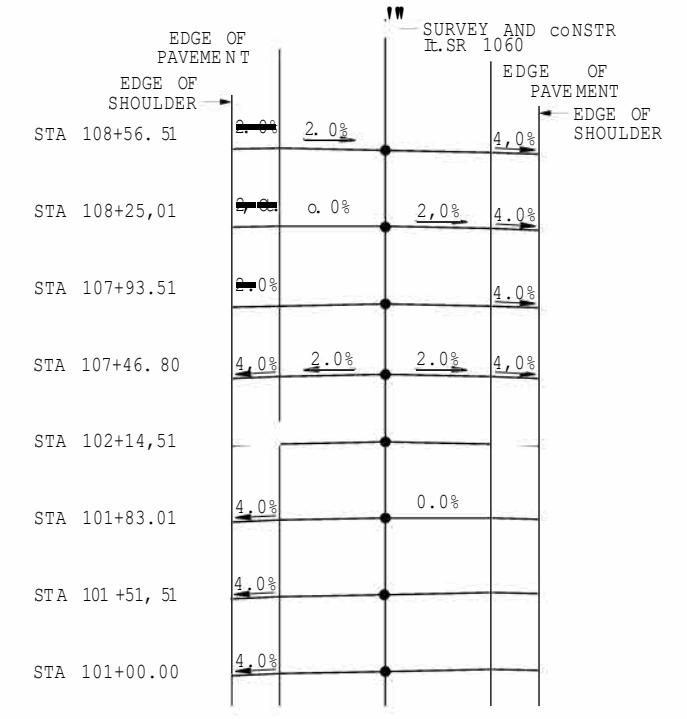
Exhibit "A"
 Sheet 16 of 17

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TYPICAL SECTION
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Exhibit "A"
Sheet 17 of 17



CROSS SLOPE TRANSITIONS
• SUPERELEVATION ROTATION POINT IS ABOUT THE CENTERLINE

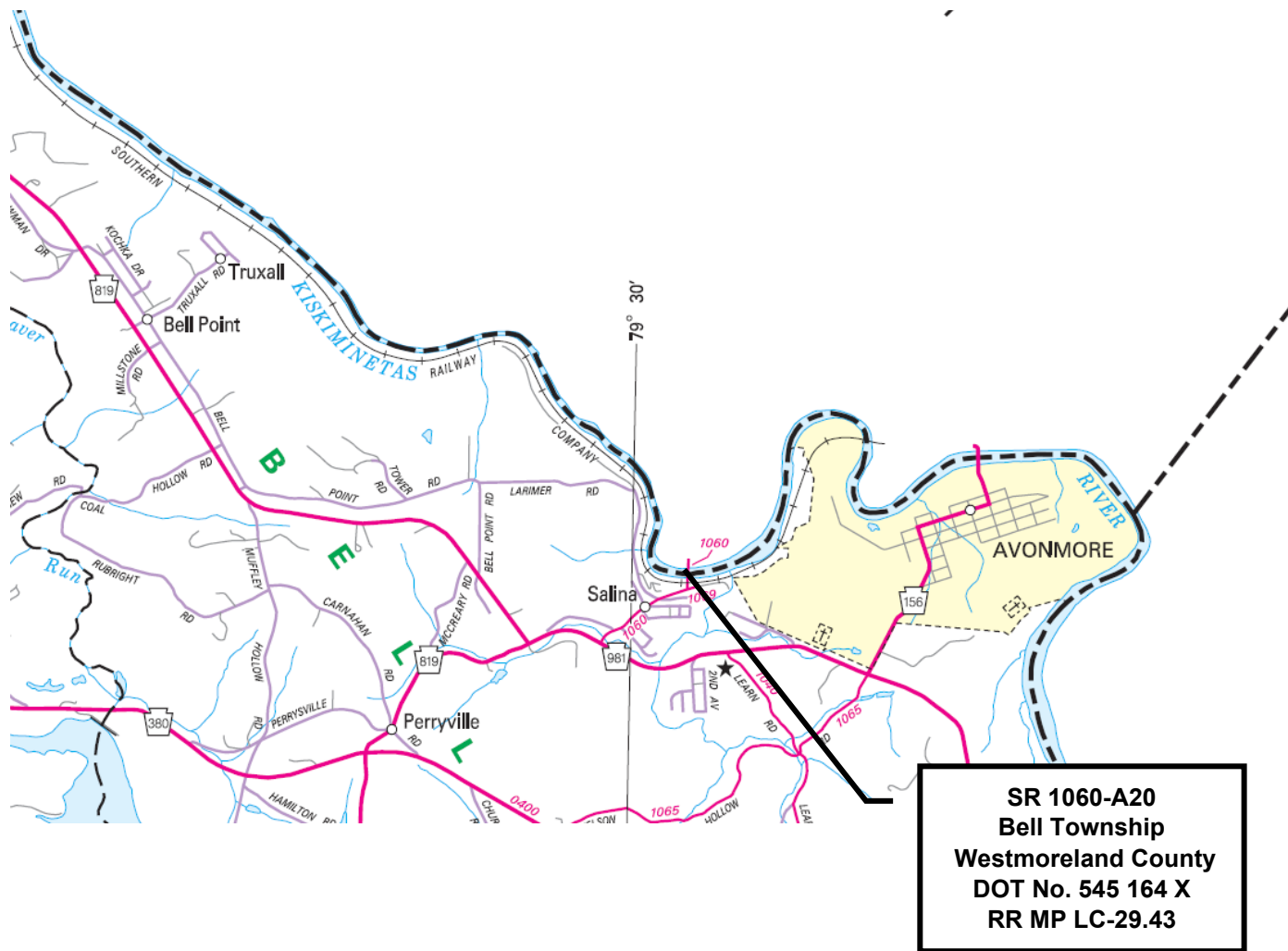
| Mark | Description | By | Chk' d. | Reem' d. | Date |
|-----------|-------------|----|---------|----------|------|
| REVISIONS | | | | | |

SR 1060 PREVIOUSLY KNOWN AS LR 64261
BMS STR ID: 64-1060-0010-0037 MPMS/ECMS PROJ: 81747 BRKEY: 67409

COMMONWEALTH OF PENNSYLVANIA
DEPARTMENT OF TRANSPORTATION
WESTMORELAND & ARMSTRONG COUNTIES
SR 1060 SECTION A20
SEGMENT 0010 OFFSET 0037
SR 1060 STA 104+95.00 OVER KISKIMINETAS RIVER &
NORFOLK SOUTHERN RAILWAY COMPANY
3 SPAN CONT COMP STEEL PLATE GIRDER
TYPE, SIZE & LOCATION TYP SECTION

| | |
|-------------------|--------------|
| RECOMMENDED _____ | SHEET 3 OF 3 |
| | S - 39414 |

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LOCATION MAP
EXHIBIT "B"