



**NAUMAN  
SMITH  
SHISLER & HALL**

Benjamin C. Dunlap, Jr, Esquire

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717-236-3010. Ext. 121

March 12, 2021

**VIA ELECTRONIC FILING**

Rosemary Chiavetta, Secretary  
Pennsylvania Public Utility Commission  
P. O. Box 3265  
Harrisburg, PA 17105-3265

**RE: Bridge structure where State Route 1025 crosses over a single track of Delaware and Hudson Railway Company, Inc. (264 293 K) in Nicholson Borough, Wyoming County / Docket No.: M-2013-2364201**

**Investigation upon the Commission's own motion to determine the condition and disposition of six (6) existing structures carrying various highways above the grade of the tracks of the Canadian Pacific Railroad in Great Bend Township, New Milford Township, Brooklyn Township, Hop Bottom Borough, Lathrop Township, Susquehanna County and Benton Township, Lackawanna County  
Docket No.: I-2015-2472242**

Dear Secretary Chiavetta:

Enclosed for the Commission's review and approval are Revised Bridge Repair Plans of Norfolk Southern Railway Company ("Norfolk Southern") for the crossings in the above-referenced matters. These plans are being sent to all parties on the attached service list for their review.

Sincerely yours,

Benjamin C. Dunlap, Jr.

BCDjr/io  
enclosures  
cc w/ enc.:

All Parties of Record





**GENERAL NOTES**

- CONSTRUCTION AND MATERIAL SPECIFICATIONS: CURRENT EDITION AMERICAN RAILWAY ENGINEERING & MAINTENANCE-OF-WAY "MANUAL FOR RAILWAY ENGINEERING", PENNSYLVANIA DEPARTMENT OF TRANSPORTATION STANDARDS FOR BRIDGE CONSTRUCTION CURRENT EDITION EXCEPT AS SUPERSEDED BY THESE PLANS, THE CONTRACT AND NORFOLK SOUTHERN SPECIFICATIONS.
- THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT, DUE TO THE NATURE OF RECONSTRUCTION PROJECTS, THE EXACT EXTENT OF RECONSTRUCTION WORK CANNOT ALWAYS BE ACCURATELY DETERMINED PRIOR TO COMMENCEMENT OF WORK. THESE CONTRACT DOCUMENTS HAVE BEEN PREPARED BASED ON FIELD INSPECTION AND OTHER INFORMATION AVAILABLE AT THE TIME. ACTUAL FIELD CONDITIONS MAY REQUIRE MODIFICATIONS TO CONSTRUCTION DETAILS AND WORK QUANTITIES. THE CONTRACTOR SHALL PERFORM THE WORK IN ACCORDANCE WITH FIELD CONDITIONS AS ORDERED BY THE ENGINEER.
- THE CONTRACTOR SHALL NOTE THAT ADDITIONAL WORK MAY BE REQUIRED AS THE CONTRACT PROGRESSES WHICH IS NOT SHOWN OR NOTED ON THE PLANS. THIS WORK SHALL BE PERFORMED BY THE CONTRACTOR AS ORDERED BY THE ENGINEER AND PAYMENT SHALL BE MADE AT THE UNIT AND PER FOR THE APPROPRIATE ITEMS. SEE SPECIFICATIONS WITHIN CONTRACT PROPOSAL.
- THE CONTRACTOR SHALL PROCEED IN A CONTINUOUS, SYSTEMATIC AND EXPEDITIOUS MANNER. LABOR, ETC. ON THE PLANS. THE CONTRACTOR WILL BE REQUIRED TO PLACE SUFFICIENT EQUIPMENT, LABOR, ETC. ON THE PLANS TO PROGRESS THE WORK MAY PROGRESS IN ACCORDANCE WITH THE APPROVED CONSTRUCTION SCHEDULE.
- THE CONTRACTOR SHALL VISIT THE SITE BEFORE BIDDING TO FAMILIARIZE HIMSELF WITH THE PRESENT CONDITION AND TO JUDGE FOR HIMSELF THE EXTENT AND NATURE OF THE WORK TO BE DONE UNDER THIS CONTRACT. NO EXTRA COMPENSATION WILL BE ALLOWED HIM BECAUSE OF HIS FAILURE TO INCLUDE IN THE CONTRACTOR'S BID, ALL ITEMS AND MATERIALS WHICH HE IS REQUIRED TO FURNISH IN ACCORDANCE WITH THE CONTRACT PLANS.
- DIMENSIONS OF THE EXISTING STRUCTURES SHOWN ON THESE PLANS ARE FOR GENERAL REFERENCE ONLY. THE CONTRACTOR SHALL TAKE ALL SUCH FIELD MEASUREMENTS AS ARE NECESSARY TO ASSURE PROPER CONSTRUCTION OF THE FINISH WORK.
- THERE SHALL BE NO CLAIM MADE BY THE CONTRACTOR AGAINST THE RAILROAD FOR WORK PERTAINING TO MODIFICATIONS AS MAY BE REQUIRED DUE TO ANY DIFFERENCE BETWEEN ACTUAL FIELD CONDITIONS AND THOSE SHOWN BY THE DETAILS AND DIMENSIONS IN THE CONTRACT PLANS OR THE RECORD DRAWINGS.
- THE CONTRACTOR IS ADVISED THAT ADDITIONAL "NOTES" WILL BE FOUND ON SUBSEQUENT SHEETS OF THE CONTRACT PLANS AND SUCH "NOTES" WHILE PERTAINING TO THE SPECIFIC DRAWINGS THEY ARE PLACED ON, ALSO SUPPLEMENT THE GENERAL NOTES LISTED HERE.
- THE CONTRACTOR WILL BE HELD RESPONSIBLE FOR ALL DAMAGES TO THE EXISTING STRUCTURE, TRACK, UTILITIES OR ADJACENT PROPERTIES OR FACILITIES, CAUSED BY HIS OPERATIONS, WHICH ARE NOT INCLUDED AS PART OF THE INTENDED WORK. ANY DAMAGE TO THE EXISTING STRUCTURE OR ADJACENT PROPERTIES, SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR WITHOUT COST TO THE RAILROAD AND TO THE SATISFACTION OF THE ENGINEER.
- IF, DURING THE REPAIR OPERATIONS, ANY COMPONENTS OF THE BRIDGE THAT ARE NOT SCHEDULED TO BE REMOVED, SUCH AS ABUTMENT STEMS, WINGWALLS, GIRDERS OR BEARINGS ARE DAMAGED BY THE CONTRACTOR'S PERSONNEL OR EQUIPMENT, THEY SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER, AT NO COST TO THE RAILROAD.
- THE CONTRACTOR IS REQUIRED TO PROTECT HIS WORKERS AT ALL TIMES IN CONFORMANCE WITH ALL FEDERAL, STATE AND LOCAL REGULATIONS AND P.A. BRIDGE WORKER SAFETY RULES.
- THE CONTRACTOR IS ADVISED THERE IS NO ELECTRIC POWER DISTRIBUTION SYSTEM AVAILABLE AT THE BRIDGE FOR HIS USE.
- ALTERATION OF THE CONSTRUCTION PLANS AND STRUCTURAL DETAILS ARE NOT PERMITTED WITHOUT EXPRESS PERMISSION OF NORFOLK SOUTHERN'S CHIEF ENGINEER OF BRIDGES AND STRUCTURES.
- CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS TO COMPLETE THE WORK.
- ALL TRAFFIC CONTROL MEASURES NEEDED TO COMPLETE WORK SHALL BE CONDUCTED IN ACCORDANCE WITH THE PENNSYLVANIA DEPARTMENT OF TRANSPORTATION SPECIFICATION AND ANY LOCAL CODES OR ORDINANCES THAT MAY APPLY.

**DESIGN LOADINGS**

- ALL LOADS ARE PER AASHTO CURRENT EDITION.

**DEMOLITION PLAN & ERECTION PLAN NOTES:**

- THE CONTRACTOR IS REQUIRED TO SUBMIT PLANS FOR THE SAFE DEMOLITION AND ANY NECESSARY FORMING OF THE EXISTING STRUCTURE IN ORDER TO PERFORM ALL NEEDED REPAIRS, TO THE RAILROAD PRIOR TO BEGINNING WORK.
- THE CONTRACTOR SHALL DISPOSE OF ALL EXCAVATED & REMOVED SUBSTRUCTURE & FILL MATERIALS AND DEMOLISHED MATERIALS OFF-SITE IN A LEGAL MANNER UNLESS OTHERWISE DIRECTED BY P.A.S. REPRESENTATIVES IN THE FIELD.
- THE CONTRACTOR IS RESPONSIBLE FOR THE TRAFFIC CONTROL PLAN, AND THIS SHOULD BE INCLUDED IN THE BID PRICE FOR MOBILIZATION OF THE EXISTING STRUCTURE.

PROTECTION OF RAILROAD OPERATIONS:  
 1. WHERE APPLICABLE, OPERATIONS SHALL BE COORDINATED WITH THE RAILROAD SO AS NOT TO INTERFERE WITH THE CONTINUED OPERATIONS OF TRAINS. A RAILROAD FLAGMAN (SUPPLIED BY NORFOLK SOUTHERN) SHALL BE PRESENT FOR ANY AND ALL WORK ABOVE OR UNDERNEATH THE BRIDGE ADJACENT TO THE TRACK.  
 2. THE CONTRACTOR IS REQUIRED TO FULLY COOPERATE WITH THE NORFOLK SOUTHERN FLAGMAN AND/OR NORFOLK SOUTHERN REPRESENTATIVE & IMMEDIATELY REMOVE ALL EQUIPMENT & PERSONNEL WORKING NEAR THE TRACK WHEN DIRECTED AS SUCH BY THE FLAGMAN.

UTILITIES:  
 1. THE CONTRACTOR SHALL BE AWARE OF THE OVERHEAD ELECTRIC AND UTILITY LINES ABOVE/UNDER AND PARALLEL TO THE EXISTING BRIDGE.

OTHER:  
 1. THE WORK FOR THIS PROJECT SHALL BE IN ACCORDANCE WITH THE NORFOLK SOUTHERN STANDARD SPECIFICATIONS FOR MATERIALS AND CONSTRUCTION DATED JANUARY 2019, PENNSYLVANIA DEPARTMENT OF TRANSPORTATION STANDARDS FOR BRIDGE CONSTRUCTION, CURRENT EDITION AND ANY SPECIAL PROVISIONS PROVIDED HEREIN.  
 2. P.A.S. WILL FURNISH A FLAGMAN AT NO COST TO THE CONTRACTOR. THE CONTRACTOR WILL BE RESPONSIBLE FOR ALL CONSTRUCTION ACTIVITIES WITH THE P.A.S. FLAGMAN.  
 3. THE CONTRACTOR SHALL SCHEDULE A PRECONSTRUCTION MEETING ONCE A CONTRACT HAS BEEN AWARDED.

4. THE CONTRACTOR IS RESPONSIBLE FOR THE INVESTIGATION, LOCATION, SUPPORT, PROTECTION, AND RESTORATION OF ALL EXISTING UTILITIES WHETHER SHOWN ON THESE PLANS OR NOT. IT IS BELIEVED THAT THEY ARE ESSENTIALLY CORRECT, BUT NS DOES NOT GUARANTEE THEIR ACCURACY OR COMPLETENESS. THE CONTRACTOR SHOULD VERIFY LOCATIONS WITH THE UTILITY COMPANIES NOT LESS THAN 72 HOURS BEFORE STARTING EXCAVATION ACTIVITIES.

5. RAILROAD RIGHT OF WAY LINES, IF SHOWN, WERE OBTAINED FROM VALUATION MAPS. ALL RIGHT OF WAY AND PROPERTY LINES ARE SHOWN FOR INFORMATION PURPOSES ONLY AS THEY HAVE NOT BEEN VERIFIED IN THE FIELD.

6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ANY LOCAL CONSTRUCTION PERMITS THAT MAY BE REQUIRED FOR CONSTRUCTION OF THIS PROJECT.

7. ANY SURVEYS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.

8. THE CONTRACTOR SHALL OBTAIN ANY TEMPORARY CONSTRUCTION ENTRANCE AND/OR DRIVEWAY PERMITS FROM THE APPROPRIATE GOVERNING AGENCY.

9. ON SITE AND GENERATED CONSTRUCTION REGULATIONS IS TO BE DISPOSED OF AS NON-REGULATED SOLID WASTE IN COMPLIANCE WITH LOCAL REGULATIONS. IF SUSPECTED ENVIRONMENTAL PROBLEMS ARE ENCOUNTERED, CONTACT THE P.A.S. ENVIRONMENTAL PROTECTION DEPARTMENT TO ASSIST IN THEIR PROPER HANDLING AND REMOVAL.

10. THE CONTRACTOR SHALL COORDINATE WITH P.A.S. SIGNALS DEPARTMENT FOR THE LOCATIONS OF SIGNAL WIRE WHETHER LIVE OR DEAD BEFORE ANY EXCAVATION WORK.

11. ALL CONCRETE REPAIRS SHALL BE IN ACCORDANCE WITH PENNSYLVANIA DEPARTMENT OF TRANSPORTATION STANDARDS FOR BRIDGE CONSTRUCTION, CURRENT EDITION.

SCOPE OF WORK:  
 1. PROVIDE TRAFFIC CONTROL.  
 2. DEMO EXISTING PARAPET WALL AND IRON BARRELS AS REQUIRED  
 3. REPAIR EXISTING BRIDGE SUBSTRUCTURE AND SUPERSTRUCTURE AS NOTED  
 4. REPAIR EXISTING PARAPET WALL AND PROTECTION FENCE  
 5. ATTACH THREE BEAM TO BRIDGE BARRIER TO BRIDGE AND EXISTING GUARDRAIL AS APPROPRIATE  
 6. BARRIER WALL CORNERS WITHOUT EXISTING GUARDRAIL TO ATTACH SHALL RECEIVE PENNDOT APPROVED END TREATMENT  
 7. REOPEN STRUCTURE TO NORMAL TRAFFIC


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**ESTIMATE OF QUANTITIES**

| BID ITEM | DESCRIPTION                                              | QTY | UNIT |
|----------|----------------------------------------------------------|-----|------|
| 1.       | EAST ABUTMENT REPAIRS* (APPROX. 125)                     | 1   | LS   |
| 2.       | WEST ABUTMENT REPAIRS* (APPROX. 125)                     | 1   | LS   |
| 3.       | PROPOSED BRIDGE BARRIER WALL W/ PROTECTION (APPROX. 240) | 1   | LS   |
| 4.       | PERFORMANCE BOND                                         | 1   | LS   |
| 5.       | PAVEMENT BOND                                            | 1   | LS   |
| 6.       | 3/4" DOWEL (86, MIN. LENGTH 24.5", MAX. LENGTH 54.5")    | 209 | EACH |
| 7.       | 3/4" DOWEL (86, MIN. LENGTH 24.5", MAX. LENGTH 54.5")    | 209 | EACH |
| 8.       | 3/4" DOWEL (86, MIN. LENGTH 24.5", MAX. LENGTH 54.5")    | 209 | EACH |
| 9.       | 3/4" DOWEL (86, MIN. LENGTH 24.5", MAX. LENGTH 54.5")    | 209 | EACH |
| 10.      | SAW CUT CONCRETE FACES*                                  | 1   | LS   |

\*INCLUDES EXCAVATION, REMOVAL OF LOOSE CONCRETE, CONCRETE, EPOXY, REBAR, HAULLING OFF ANY DEBRIS AND RESTORING THE SITE TO ITS ORIGINAL CONDITION.



**NORFOLK SOUTHERN**  
 NORFOLK SOUTHERN RAILWAY  
 HARRISBURG, PA

NORTHERN REGION

OVERPASS SR-1025  
 HA LINE, NICHOLSON, PA


BRIDGE REPAIRS

OFFICE OF CHIEF ENGINEER - BRIDGES & STRUCTURES  
 ALBANY, GA

|                                                  |                |
|--------------------------------------------------|----------------|
| DRAWING TITLE: GENERAL NOTES & INDEX OF DRAWINGS |                |
| DC                                               | VAL SEC        |
| CH                                               | WGM            |
| DATE                                             | DATE           |
| HA-652.69                                        | 02/08/2021     |
| SHEET NUMBER                                     | DRAWING NUMBER |
| 2 OF 16                                          | HA-652.69      |

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| DC  | 02-09-21 | FEINNOT | REVISIONS   |

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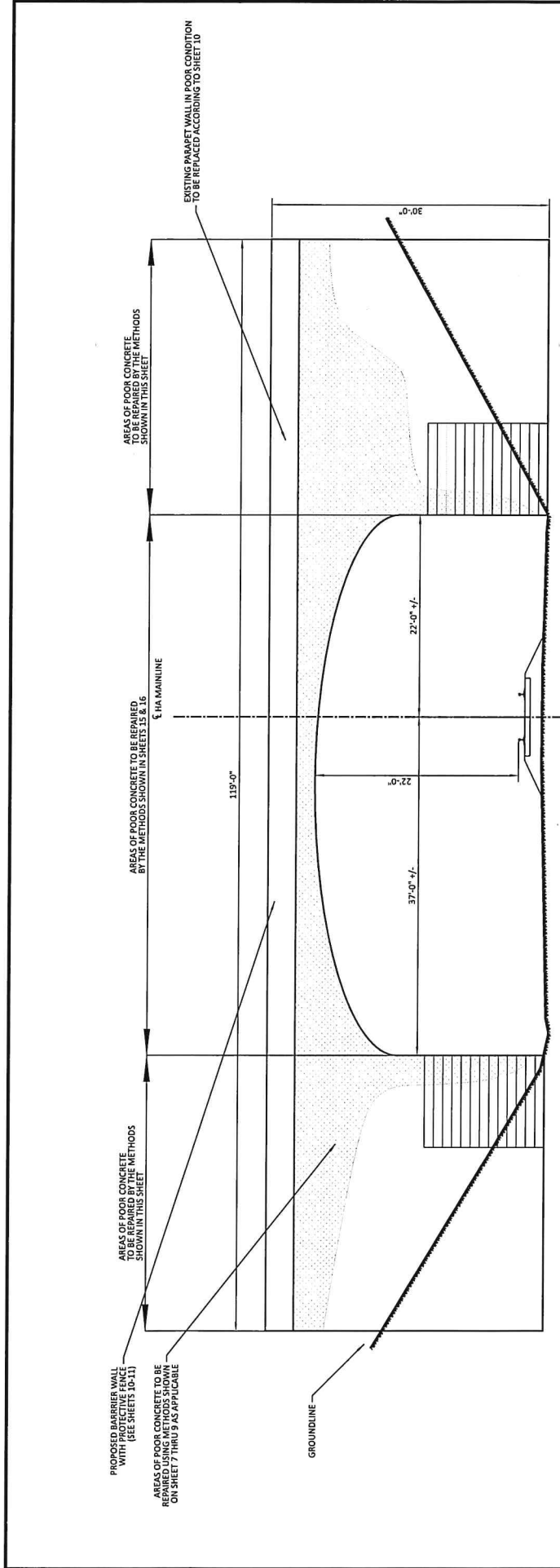


DIPANKAR CHANDRA  
 LICENSED PROFESSIONAL ENGINEER  
 IN PENNSYLVANIA  
 No. 1000000000









EXISTING SOUTH ELEVATION  
SCALE : 1/4" = 1'

NOTES:  
 1. ALL SURVEY INFORMATION AND DIMENSIONS SHOWN OR REFERENCED IN THIS PLAN OR ATTACHED DOCUMENTS ARE BASED ON ENGINEERS SITE VISIT, PRELIMINARY SURVEY, RECORD DRAWINGS, GIS INFORMATION AND OTHER METHODS AS AVAILABLE TO THE ENGINEER AT THE TIME OF PUBLICATION. THIS DOCUMENT IN NO WAY GUARANTEES THE ACCURACY OR PRECISION OF THE ABOVE LISTED INFORMATION. THIS INFORMATION IS GIVEN AS FOR THE PURPOSE OF GENERAL REFERENCE.  
 2. THE CONTRACTOR SHALL MEASURE AND VERIFY ALL MEASUREMENTS NEEDED TO COMPLETE THE SCOPE OF WORK IN AN APPROVED AND CORRECT MANNER.

**NORFOLK SOUTHERN**  
 NORFOLK SOUTHERN RAILWAY  
 HARRISBURG DIV

OVERPASS SR-1025  
 HA LINE, NICHOLSON, PA

BRIDGE REPAIRS

OFFICE OF CHIEF ENGINEER - BRIDGES & STRUCTURES  
 ATLANTA, GA

DRAWING TITLE: EXISTING SOUTH ELEVATION

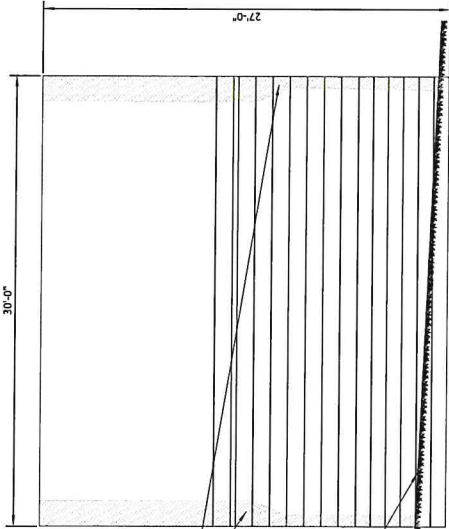
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| 02-08-21 | REVISION 2 |                   |
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DATE: 02/08/2021  
 SHEET NUMBER: 5 OF 16  
 DRAWING NUMBER: HA-652.69

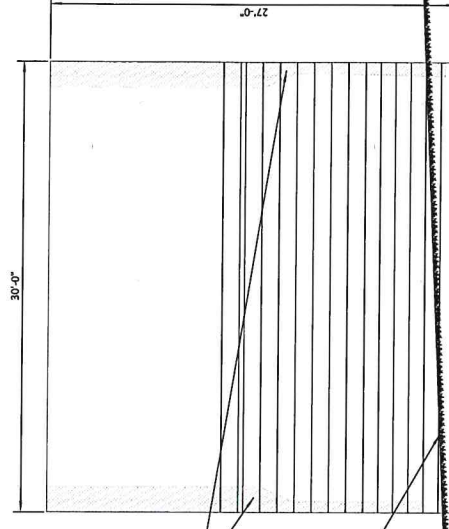
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EXISTING EAST ABUTMENT ELEVATION



EXISTING WEST ABUTMENT ELEVATION

**NJS NORFOLK SOUTHERN**  
 NORFOLK SOUTHERN RAILWAY HARRISBURG DIV  
 OVERPASS SR-1025  
 HA LINE, NICHOLSON, PA  
 BRIDGE REPAIRS  
 ALUMIA, GA.

OFFICE OF CHIEF ENGINEER - BRIDGES & STRUCTURES  
 DRAWING TITLE: EAST & WEST ABUTMENT ELEVATION

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| DOA MCC      | VAL SEC | IMP  | MILE POST  | HA-652.69      |
| CHK WKM      | FILE    | DATE | 02/08/2021 |                |
| SHEET NUMBER |         |      | 6 OF 16    | DRAWING NUMBER |
|              |         |      |            | HA-652.69      |



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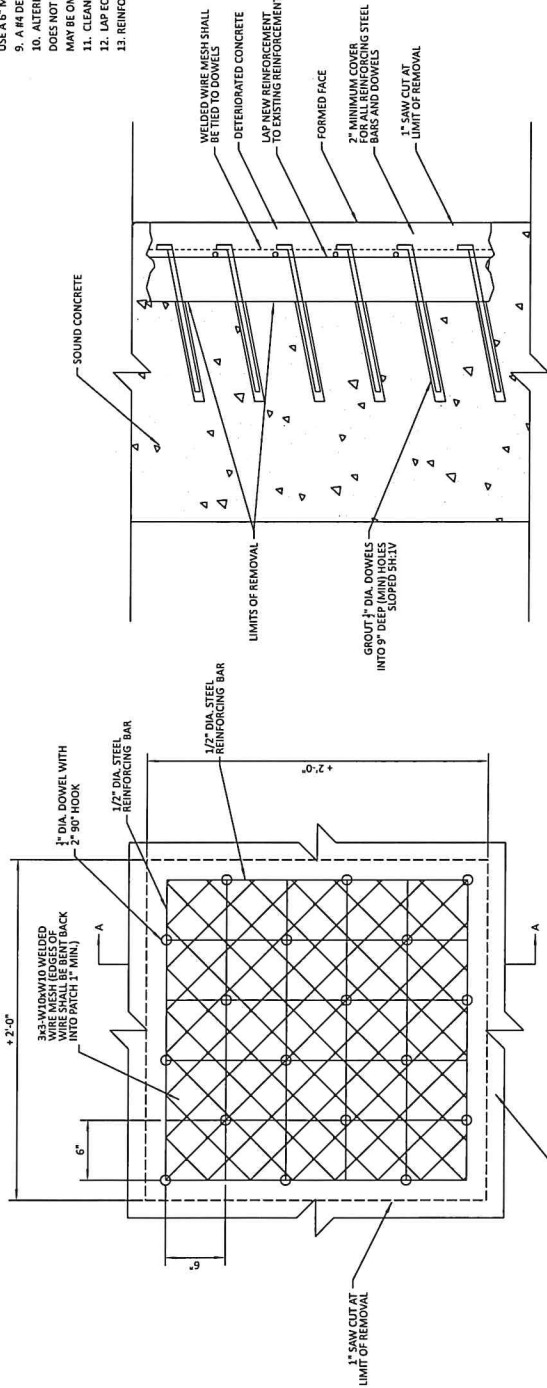
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| CCB | 12-31-19 | PENNDOT REVISIONS |  |
| BY  | DATE     | DESCRIPTION       |  |





**NOTES:**

1. ALL REPAIRS SHALL BE MADE IN ACCORDANCE WITH PENNSYLVANIA DEPARTMENT OF TRANSPORTATION STANDARDS FOR BRIDGE CONSTRUCTION BC-783M.
2. SQUARE OFF DETERIORATED CONCRETE TO SOUND CONCRETE WITH A SAWCUT OF 3/4" MINIMUM TO 1" MAXIMUM BUT NOT TO THE DEPTH OF THE REINFORCEMENT STEEL. BACK BEVEL EDGE BEYOND SAWCUT.
3. USE HAND TOOLS TO REMOVE ALL LOOSE AND DELAMINATED CONCRETE THAT PROVIDES A SOUND BOND BETWEEN EXISTING CONCRETE AND NEW CONCRETE. PNEUMATIC HAMMERS WITH IMPACT RATINGS OF 30 FT/LB OR LESS MAY BE USED IF REQUIRED.
4. IF DETERIORATED CONCRETE EXTENDS BEYOND THE PRIMARY REINFORCEMENT, REMOVE THE CONCRETE TO AT LEAST 3/4" BEHIND THE REINFORCEMENT.
5. APPLY AN EPOXY BONDING COMPOUND BETWEEN THE EXISTING AND THE NEW CLASS A CEMENT CONCRETE.
6. USE DOWELS ONLY WHEN W DIMENSION OF DETERIORATED CONCRETE IS GREATER THE 2'-0" AND NEW OR EXISTING REINFORCEMENT CANNOT ADEQUATELY BE DEVELOPED BY LAPPING WITH EXISTING REINFORCEMENT.
7. USE A TACHOMETER TO LOCATE EXISTING REINFORCEMENT WHEN DRILLING DOWEL HOLES TO AVOID DRILLING THRU EXISTING BARS.
8. AN APPROVED EPOXY ANCHORING SYSTEM IN 90° HOLES MAY REPLACE GROUT IN SLOPED HOLES. USE A 6" MINIMUM EMBEDMENT AND IN ACCORDANCE WITH THE MANUFACTURER'S INSTRUCTIONS.
9. A #4 DEFORMED REINFORCEMENT BENT "L" BAR MAY REPLACE THE 1/2" DIA DOWEL-HOOK.
10. ALTERNATE WIRE MESH MAY BE SUBSTITUTED FOR 3x3-WIDOW10, PROVIDED WIRE SPACING DOES NOT EXCEED 4" AND AN EQUIVALENT STEEL AREA IS PROVIDED. NEW REINFORCEMENT BARS MAY BE OMITTED IF WIRE MESH STEEL AREA EXCEEDS EXISTING REINFORCEMENT.
11. CLEAN EXISTING REINFORCEMENT BY MECHANICAL MEANS.
12. LAP EQUIVALENT NEW REINFORCEMENT TO THE EXISTING REINFORCEMENT AS DIRECTED.
13. REINFORCEMENT BARS TO BE EPOXY COATED.



ELEVATION VIEW (TYP)  
SCALE: NOT TO SCALE

SECTION A-A (TYP)  
SCALE: NOT TO SCALE

NOTE:  
THIS REPAIR IS TO BE USED FOR REPAIRS ON UNDERSIDE OF DECK. ELEVATIONS AND REPAIRS TO PORTIONS OF DELAMINATED FACES OF ABUTMENTS OVER 3/4" THICKNESS.

SECTION A-A (TYP)  
SCALE: NOT TO SCALE

**NORFOLK SOUTHERN**  
NORFOLK SOUTHERN RAILWAY  
HARRISBURG, PA

OVERPASS SR-1025  
HA LINE, NICHOLSON, PA

BRIDGE REPAIRS

ATLANTA, GA

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OFFICE OF CHIEF ENGINEER - BRIDGES & STRUCTURES

DRAWING TITLE: PROPOSED TYPE 2 CONCRETE REPAIR

|              |          |                |            |
|--------------|----------|----------------|------------|
| DOW MISC     | VAL. SEC | MAP            | MILE POST  |
| CHK: DBC     | FILE     | DATE           | 02/08/2021 |
| SHEET NUMBER |          | DRAWING NUMBER |            |
| 8 OF 16      |          | HA-652.69      |            |

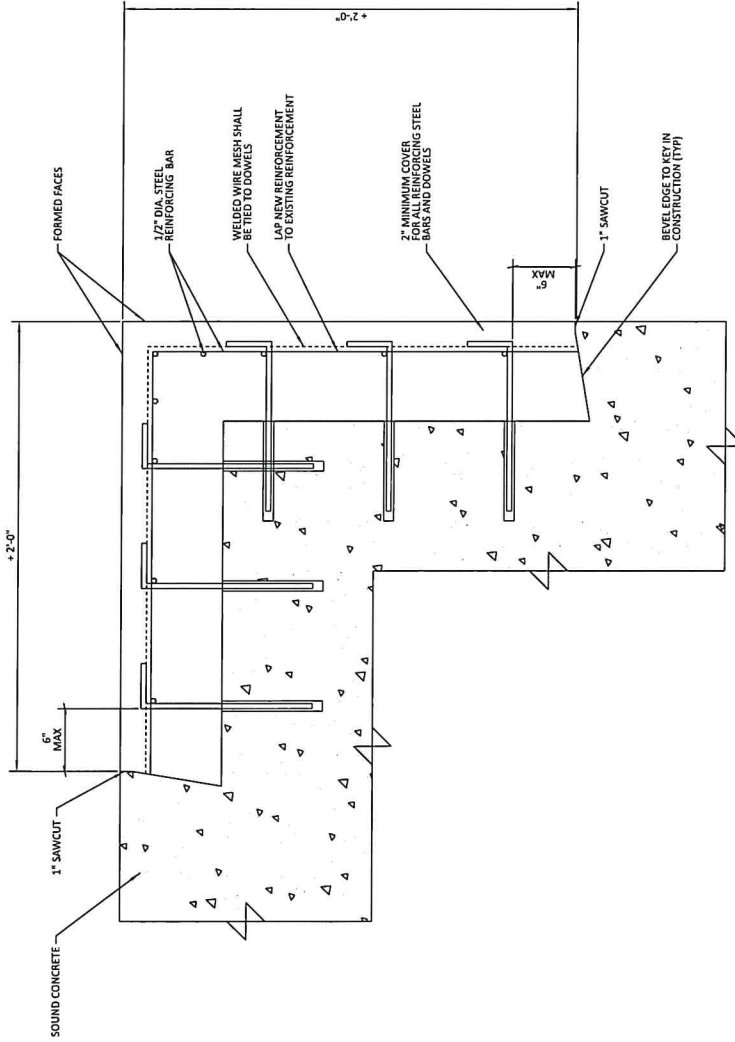
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| 1   | CCB | 02-08-21 | REVISION 2        |
| 2   | CCB | 12-31-19 | PENNDOT REVISIONS |

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**NOTES:**


1. ALL REPAIRS SHALL BE MADE IN ACCORDANCE WITH PENNSYLVANIA DEPARTMENT OF TRANSPORTATION STANDARDS FOR BRIDGE CONSTRUCTION BC-783M.
2. SQUARE OFF DETERIORATED CONCRETE TO SOUND CONCRETE WITH A SAWCUT OF 3/4" MINIMUM TO 1" MAXIMUM BUT NOT TO THE DEPTH OF THE REINFORCEMENT STEEL. BACK BEVEL EDGE BEYOND SAWCUT.
3. USE HAND TOOLS TO REMOVE ALL LOOSE AND DELAMINATED CONCRETE THAT PROVIDES A SOUND BOND BETWEEN EXISTING CONCRETE AND NEW CONCRETE. PNEUMATIC HAMMERS WITH IMPACT RATINGS OF 30 FT/LB OR LESS MAY BE USED IF REQUIRED.
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5. APPLY AN EPOXY BONDING COMPOUND BETWEEN THE EXISTING AND THE NEW CLASS AA CEMENT CONCRETE.
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7. USE A PACHOMETER TO LOCATE EXISTING REINFORCEMENT WHEN DRILLING DOWEL HOLES TO AVOID DRILLING THRU EXISTING BARS.
8. AN APPROVED EPOXY ANCHORING SYSTEM IN 90° HOLES MAY REPLACE GROUT IN SLOPED HOLES. USE A 6" MINIMUM EMBEDMENT AND IN ACCORDANCE WITH THE MANUFACTURERS INSTRUCTIONS.
9. A #4 DEFORMED REINFORCEMENT BENT "L" BAR MAY REPLACE THE 1/2" x DOWEL HOOK.
10. ALTERNATE WIRE MESH MAY BE SUBSTITUTED FOR 3x3-WIDOW10, PROVIDED WIRE SPACING DOES NOT EXCEED 4" AND AN EQUIVALENT STEEL AREA IS PROVIDED. NEW REINFORCEMENT BARS MAY BE OMITTED IF WIRE MESH STEEL AREA EXCEEDS EXISTING REINFORCEMENT.
11. CLEAN EXISTING REINFORCEMENT BY MECHANICAL MEANS.
12. LAP EQUIVALENT NEW REINFORCEMENT TO THE EXISTING REINFORCEMENT AS DIRECTED.
13. REINFORCEMENT BARS TO BE EPOXY COATED.



**CORNER REPAIR DETAIL (TYP)**

SCALE : NOT TO SCALE

NOTE:  
THIS REPAIR IS TO BE USED FOR REPAIRS ON UNDERSIDE OF DECK, ELEVATIONS AND REPAIRS TO PORTIONS OF DELAMINATED FACES OF ABUTMENTS OVER 3/4" THICKNESS.



**NORFOLK SOUTHERN**  
NORFOLK SOUTHERN RAILWAY HARRISBURG DV

**OVERPASS SR-1025**  
HA LINE, NICHOLSON, PA

BRIDGE REPAIRS

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OFFICE OF CHIEF ENGINEER - BRIDGES & STRUCTURES ALUMINACA

DRAWING TITLE: PROPOSED CORNER CONCRETE REPAIR

|                      |         |      |                          |
|----------------------|---------|------|--------------------------|
| DON MCC              | VAL SEC | MAP  | MILE POST HA-652.69      |
| CHK DBG              | FILE    | DATE | 02/08/2021               |
| SHEET NUMBER 9 OF 16 |         |      | DRAWING NUMBER HA-652.69 |

| REV | BY  | DATE     | DESCRIPTION        |
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| 1   | CGB | 02-08-21 | REVISION 2         |
| 2   | CGB | 12-31-19 | PERINDOT REVISIONS |

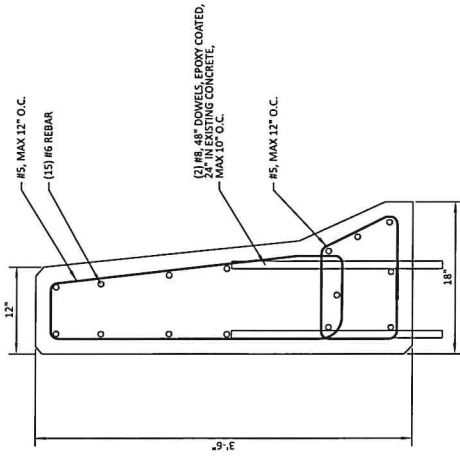
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EDDY GRANT GORDON  
PROFESSIONAL ENGINEER  
NO. 10000  
EXPIRES 12/31/2024

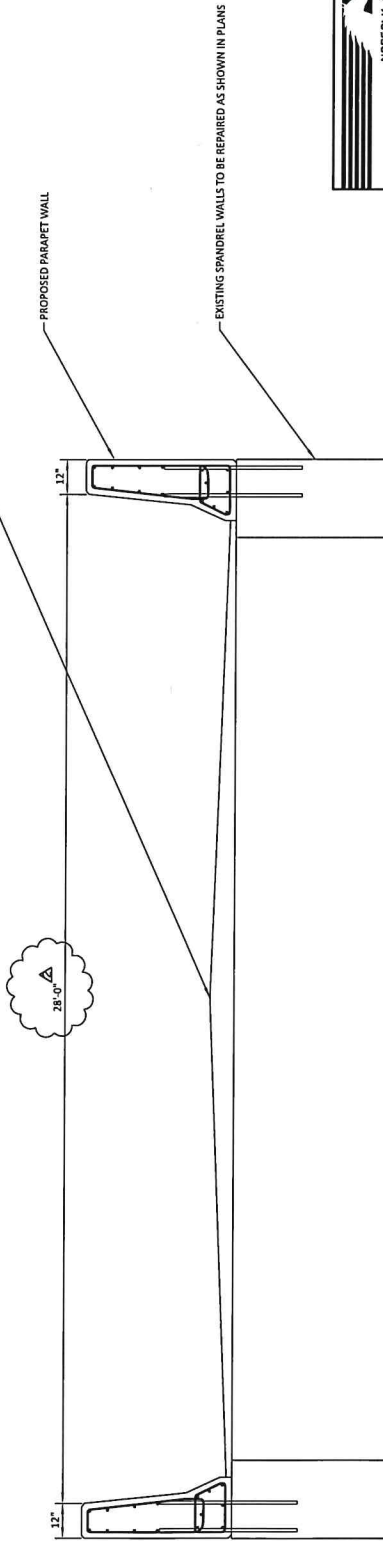
**NOTES:**

1. ALL REPAIRS SHALL BE MADE IN ACCORDANCE WITH PENNSYLVANIA DEPARTMENT OF TRANSPORTATION STANDARDS FOR BRIDGE CONSTRUCTION BC-783JM.
2. ALL CIP CONCRETE SHALL MEET OR EXCEED PENNDOT CLASS AA.
3. EXISTING PARAPET WALLS SHALL BE REMOVED BY METHODS TO KEEP EXISTING ROAD IN SERVICE AND NOT TO INTERFERE WITH RAILROAD OPERATIONS.
4. ONCE EXISTING PARAPET WALL IS REMOVED EXISTING SLAB CLEANED AND SPANDREL WALL IS TO BE REPAIRED ACCORDING TO THIS DOCUMENT AND THE ENGINEERS RECOMMENDATION.
5. ONCE SPANDREL WALL IS REPAIRED AND CURED 28 DAYS THE PROPOSED PARAPET WALLS SHALL BE CONSTRUCTED AS SHOWN IN THIS DOCUMENT.
6. TEMPORARY BARRIER WALL AND FENCING SHALL BE REMOVED BY CONTRACTOR ONCE PROPOSED PARAPET WALLS ARE CONSTRUCTED AND CURED FOR 28 DAYS.
7. EXISTING PAVEMENT IS NOT TO BE DAMAGED BY CONTRACTOR.
8. ALL CONCRETE REPAIRS SHALL BE MADE IN ACCORDANCE WITH AMERICAN CONCRETE INSTITUTE 318, CURRENT EDITION.



PROPOSED BARRIER WALL DETAIL

SCALE : NOT TO SCALE



BRIDGE CROSS-SECTION

SCALE : 1" = 20'

**NORFOLK SOUTHERN**  
NORFOLK SOUTHERN RAILWAY  
HARRISBURG DIV.

**OVERPASS SR-1025**  
HA LINE, NICHOLSON, PA  
BRIDGE REPAIRS

ATLANTA, GA.

NORTHERN REGION

OFFICE OF CHIEF ENGINEER - BRIDGES & STRUCTURES

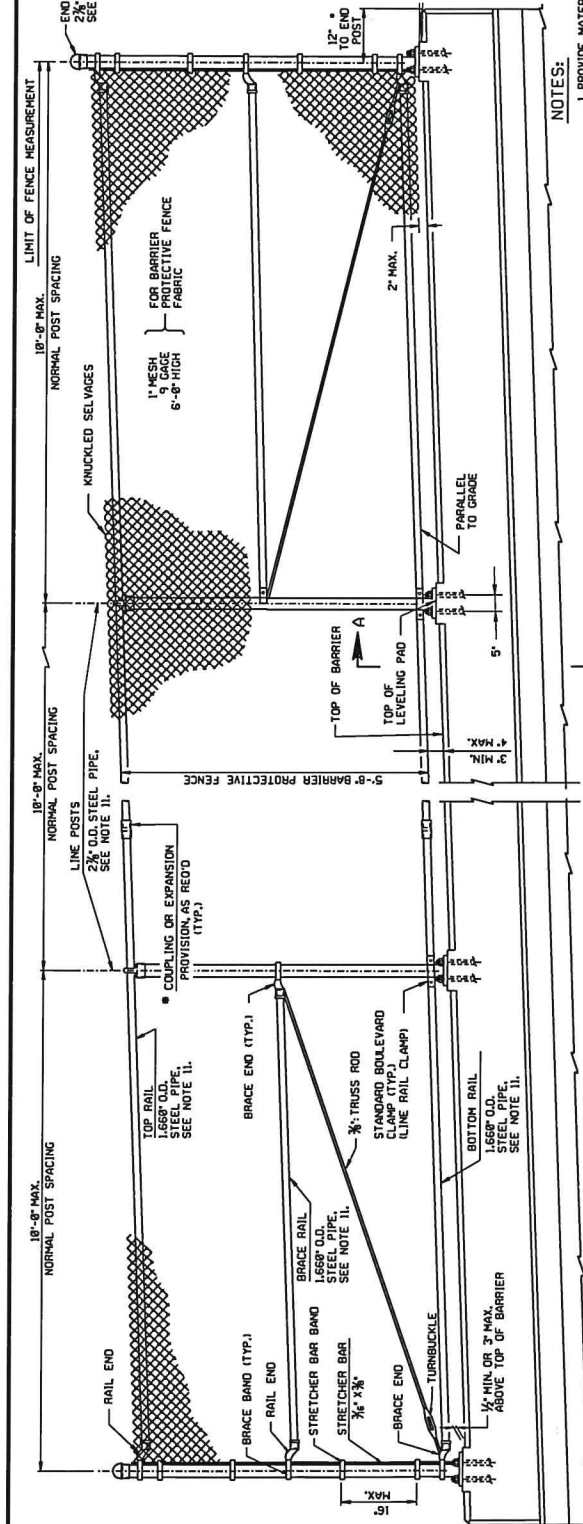
DRAWING TITLE: **PROPOSED BARRIER WALL**

|            |           |            |
|------------|-----------|------------|
| DATE       | HALE POST | HA-652.69  |
| 02/08/2021 | DATE      | 02/08/2021 |

| REV | BY  | DATE     | DESCRIPTION       |
|-----|-----|----------|-------------------|
| 1   | CCB | 02-08-21 | REVISION 2        |
| 2   | CCB | 12-31-19 | PENNDOT REVISIONS |

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MURFREESBORO, TN 37129  
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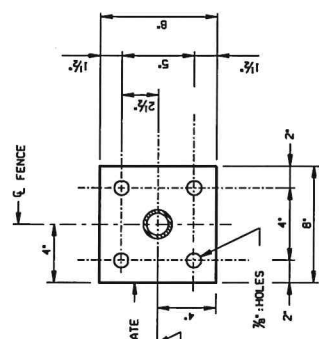




NOTES:

1. PROVIDE MATERIALS AND WORKMANSHIP IN ACCORDANCE WITH PUBLICATION 488.
2. PROVIDE SHIMS FROM APPROVED MATERIAL.
3. ONLY TOUCH-UP PAINTING OF MATERIAL IS PERMITTED.
4. PLACE FENCE POSTS AND ANCHOR BOLTS TRULY VERTICAL. PLACE RAILS PARALLEL TO GRADE.
5. NO LIGHTING POLES ARE USED.
6. AT BRIDGE EXPANSION JOINTS, PERMIT THE FENCE FABRIC AND RAILS TO EXPAND OR CONTRACT.
7. CLIP THE TIE WIRE FASTENERS AND BEND AWAY FROM TRAFFIC.
8. COAT ALL SURFACES OF THE BASE PLATES IN CONTACT WITH CONCRETE WITH CALKING COMPOUND PRIOR TO ERECTION AND ALIGNMENT. AFTER ERECTION AND ALIGNMENT, COAT ALL SURFACES OF THE BASE PLATES AND THE CONCRETE WITH CALKING COMPOUND MEETING THE REQUIREMENTS OF PUBLICATION 488, SECTION 785.816.
9. PLACE ANCHOR BOLTS WITH SIDEWALK OR BARRIER AND ACCURATELY SET AND BRACE AGAINST DISPLACEMENT. THE BASE PLATES AND CONCRETE IS PLACED LEVEL USING RAPID SET CONCRETE.
10. DESIGN DRAINAGE SYSTEM IN ACCORDANCE WITH DMA, SECTION PP. 3.2.3.
11. POSTS AND RAIL MATERIAL PER PUBLICATION 488, SECTION 1816.216.3.
12. REFER TO CONTRACT DOCUMENTS FOR POST SPACING.
13. PLACE CORNER POSTS AT ANGLE POINTS IN HORIZONTAL AND VERTICAL ALIGNMENT OF FENCE.

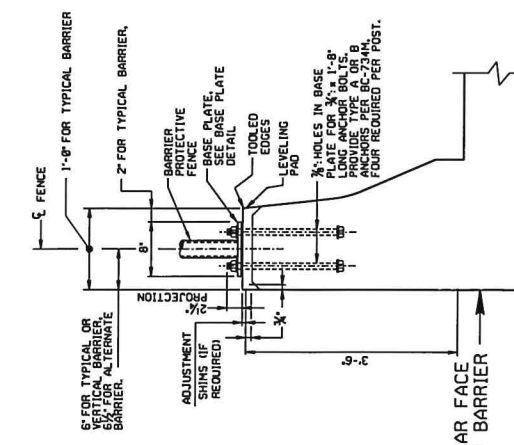
**TYPICAL FENCE ELEVATION**  
 #4" MIN. DISTANCE TO STRUCTURE EXPANSION JOINT.



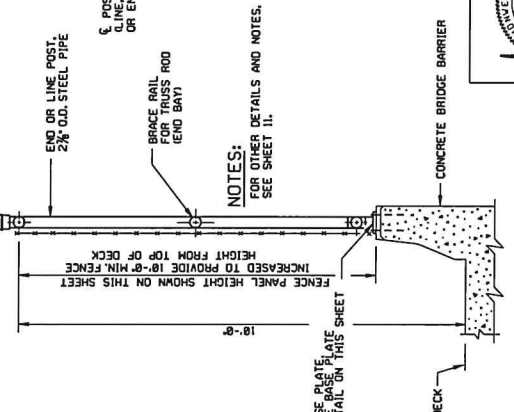
**BASE PLATE DETAIL**  
 (ALL DIMENSIONS ARE TYPICAL UNLESS NOTED OTHERWISE)  
 N.Y.S.

NOTES:  
 FOR OTHER DETAILS AND NOTES, SEE SHEET 11.

**SECTION A-A**  
 TYPICAL FENCE POST SECTION WITH ALL CONCRETE BARRIER SIMILAR (TYP. AND ALL CONCRETE BARRIER SIMILAR)



**SECTION D-D**  
 TYPICAL CONCRETE BARRIER AND NO SIDEWALK



**NORFOLK SOUTHERN**  
 NORFOLK SOUTHERN RAILWAY HARRISBURG DV  
 NORTHERN REGION  
 OVERPASS SR-1025  
 HA LINE, NICHOLSON, PA  
 BRIDGE REPAIRS

OFFICE OF CHIEF ENGINEER - BRIDGES & STRUCTURES  
 ALUMINUM  
 DRAWING TITLE: PROPOSED PROTECTIVE FENCE

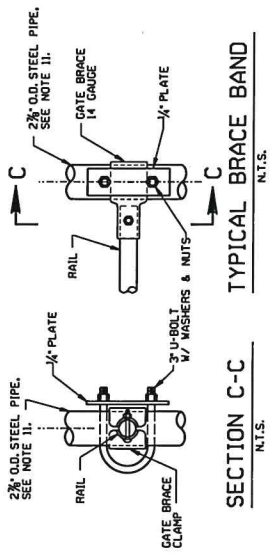
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| DATE | 02-08-21 | REVISION | 2                |
| REV  | BY       | DATE     | DESCRIPTION      |
| Δ    | CCB      | 12-31-19 | FEENDD REVISIONS |

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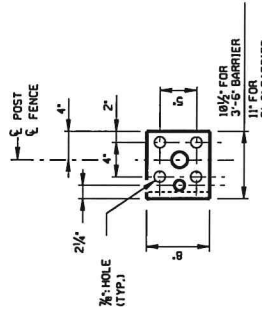
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|--------------|------------|----------------|------------|
| DATE         | 02/08/2021 | DATE           | 02/08/2021 |
| DRW          | DBC        | VAL            | SEC        |
| SHEET NUMBER | 11 OF 16   | DRAWING NUMBER | HA-652.69  |



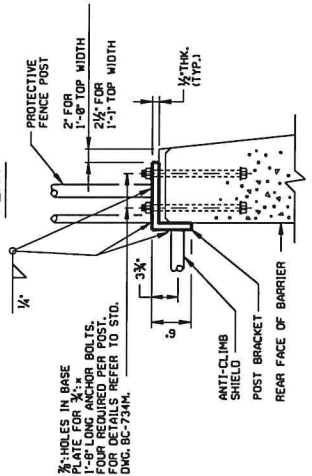




SECTION C-C TYPICAL BRACE BAND  
N.T.S.

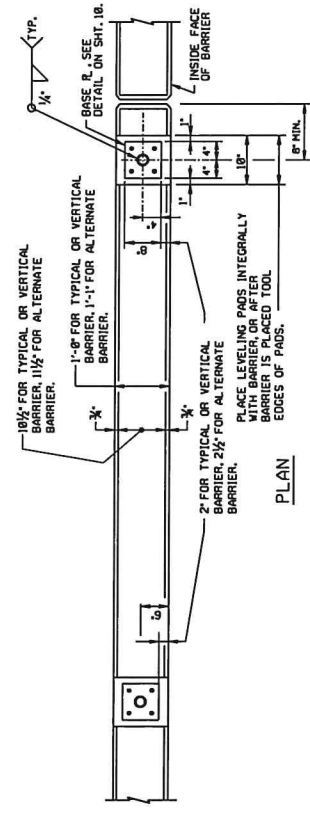


PLAN



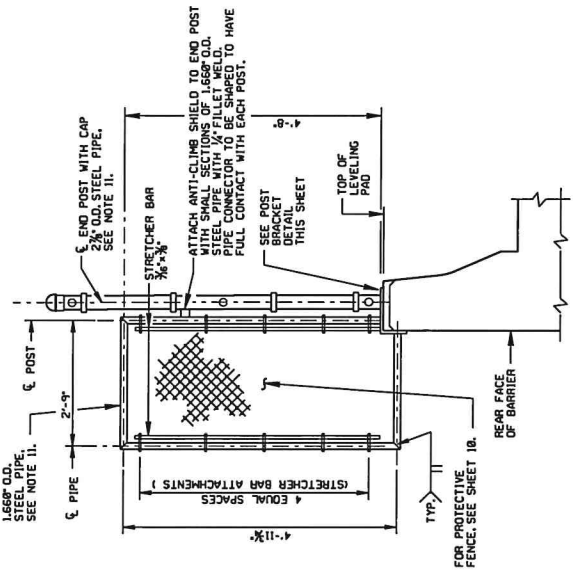
SECTION

POST BRACKET DETAIL  
AT ANTI-CLIMB SHIELD



BARRIER PROTECTIVE FENCE

FOR BASE PLATE DETAIL SEE SHEET 18.  
ALTERNATE SIDEWALK DETAIL SHOWN  
(TYP. AND ALT. CONCRETE BARRIER SIMILAR)



ANTI-CLIMB SHIELD DETAILS  
(1 REQ'D. FOR EACH END OF FENCE.)

**NORFOLK SOUTHERN**  
NORFOLK SOUTHERN RAILWAY  
HARRISBURG DIV.

NORTHERN REGION  
OVERPASS SR-1025  
HA LINE, NICHOLSON, PA  
BRIDGE REPAIRS

OFFICE OF CHIEF ENGINEER - BRIDGES & STRUCTURES  
ATLANTA, GA.

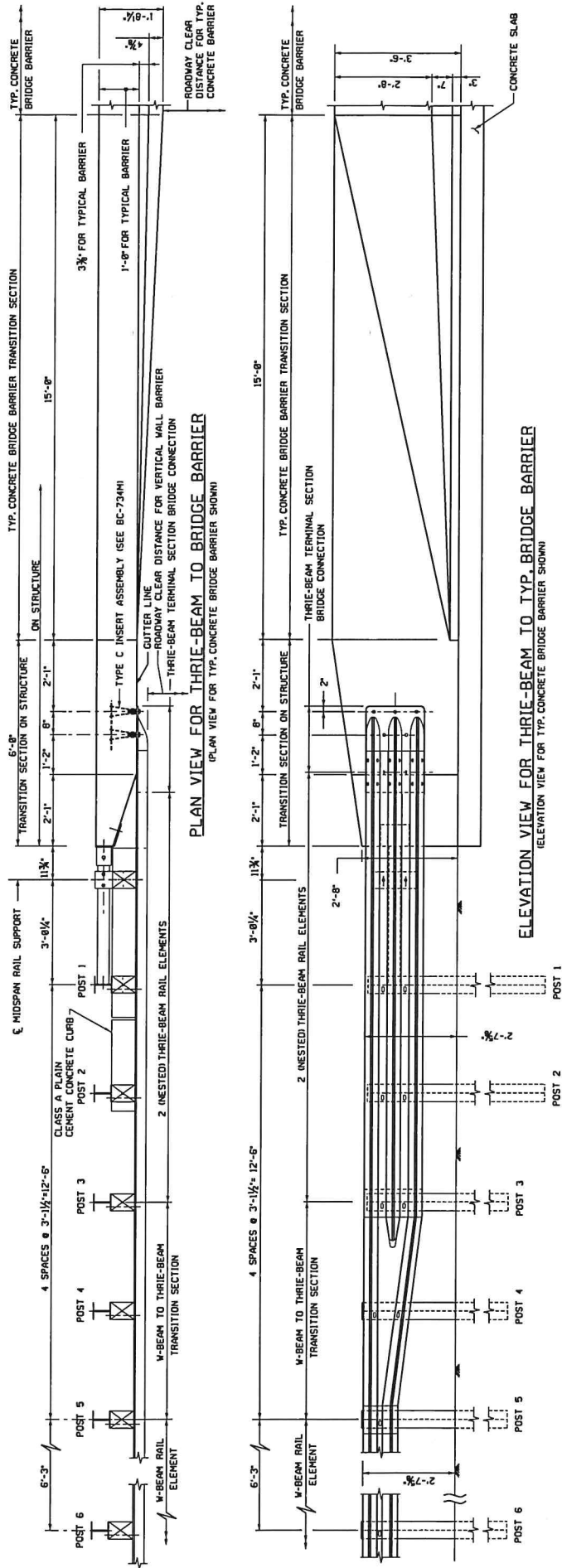
DRAWING TITLE: PROPOSED PROTECTIVE FENCE - DETAILS

|          |     |                   |
|----------|-----|-------------------|
| DATE     | BY  | DESCRIPTION       |
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(P) 615.663.1142

DATE: 02/08/2021  
SHEET NUMBER: 12 OF 16  
DRAWING NUMBER: HA-652.69





PLAN VIEW FOR THREE-BEAM TO BRIDGE BARRIER  
 (PLAN VIEW FOR TYP. CONCRETE BRIDGE BARRIER SHOWN)

ELEVATION VIEW FOR THREE-BEAM TO TYP. BRIDGE BARRIER  
 (ELEVATION VIEW FOR TYP. CONCRETE BRIDGE BARRIER SHOWN)

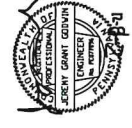
**NOTES:**

1. THREE-BEAM TO BRIDGE BARRIER TRANSITION HAS BEEN ACCEPTED BY FHWA AS A TL-3 BARRIER DESIGNATION.
2. PROVIDE MATERIALS AND WORKMANSHIP IN ACCORDANCE WITH PUB. 488.
3. USE PLAN DIMENSIONS WHEN DIFFERENT FROM THOSE SHOWN ON THIS STANDARD.
4. REINFORCED CONCRETE BARRIER AND EMBEDDED INSERTS ARE BRIDGE ITEMS.
5. SEE RC-98M AND RC-91M FOR DETAILS AND HARDWARE NOT SHOWN (INCLUDING END TREATMENT).
6. PROVIDE APPROACH END GUIDE RAIL TREATMENT AT BOTH THE APPROACH AND TRAILING ENDS OF STRUCTURE BARRIERS ON TWO LANE FACILITIES WITH TWO-WAY TRAFFIC ON FOUR LANE DIVIDED HIGHWAYS. GUIDE RAIL TRANSITION IS NOT REQUIRED ON TRAILING ENDS OF BARRIERS UNLESS WARRANTED BY OTHER OBSTRUCTIONS.
7. THE APPROACH END TRANSITION COMPONENTS ARE ROADWAY ITEMS.

**NORFOLK SOUTHERN**  
 NORFOLK SOUTHERN RAILWAY HARRISBURG DIV.  
 NORTHERN REGION OVERPASS SR-1025  
 HA LINE, NICHOLSON, PA  
 BRIDGE REPAIRS  
 OFFICE OF CHIEF ENGINEER - BRIDGES & STRUCTURES  
 ALABAMA, GA.

DRAWING TITLE: THREE-BEAM TO BRIDGE BARRIER WALL  
 DATE: 02/08/2021  
 SHEET NUMBER: 13 OF 16  
 DRAWING NUMBER: HA-652.69

DOOR MCC IVAL SEC IMP  
 DATE: 02/08/2021  
 DATE: 02/08/2021



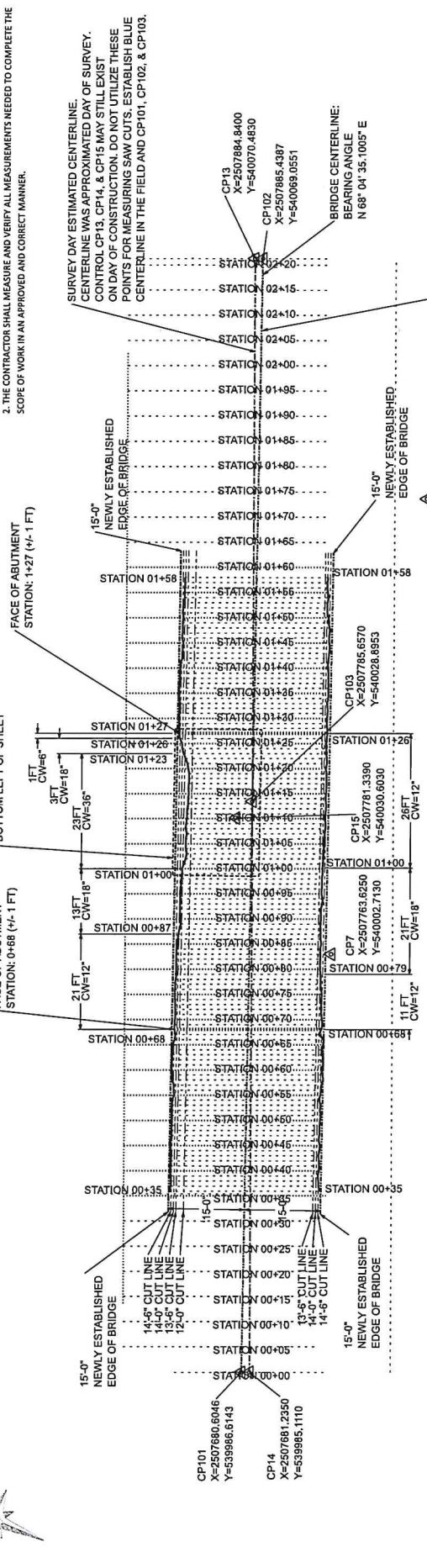
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 11437129  
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 (P) 615.663.1142





**NOTES:**

1. ALL SURVEY INFORMATION AND DIMENSIONS SHOWN OR REFERENCED IN THIS PLAN OR ATTACHED DOCUMENTS ARE BASED ON ENGINEERS SITE VISIT, PRELIMINARY SURVEY, RECORD DRAWINGS, GIS INFORMATION AND OTHER METHODS AS AVAILABLE TO THE ENGINEER AT THE TIME OF PUBLICATION. THIS DOCUMENT IN NO WAY GUARANTEES THE ACCURACY OR PRECISION OF THE ABOVE LISTED INFORMATION. THIS INFORMATION IS GIVEN AS FOR THE PURPOSE OF GENERAL REFERENCE.
2. THE CONTRACTOR SHALL MEASURE AND VERIFY ALL MEASUREMENTS NEEDED TO COMPLETE THE SCOPE OF WORK IN AN APPROVED AND CORRECT MANNER.



**NORFOLK SOUTHERN**  
 NORFOLK SOUTHERN RAILWAY HARRISBURG DIV  
 OVERPASS SR-1025  
 HA LINE, NICHOLSON, PA  
 BRIDGE REPAIRS  
 OFFICE OF CHIEF ENGINEER - BRIDGES & STRUCTURES  
 ALUMINA, GA  
 DRAWN BY: [Name] CHECKED BY: [Name] DATE: 02/08/2021

| REV | BY  | DATE     | DESCRIPTION       |
|-----|-----|----------|-------------------|
| 1   | WAS | 02-08-21 | REVISION 2        |
| 2   | CKS | 01-29-21 | PERINNO REVISIONS |

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 213 UPTOWN SQUARE  
 MURFREESBORO, TN 37129  
 (p) 615.663.1142



EXPLODED SAW-CUT VIEW





**BEFORE THE  
PENNSYLVANIA PUBLIC UTILITY COMMISSION**

|                                                                                                                                                                                                                                                                                                                                                                                                                |                                                                                    |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------|
| Bridge Structure where State Route 1025<br>crosses over a single track of Delaware and<br>Hudson Railway Company, Inc (264 293 K)<br>in Nicholson Borough, Wyoming County                                                                                                                                                                                                                                      | : M-2013-2364201<br>:<br>: ELECTRONICALLY FILED<br>:<br>:                          |
| Investigation upon the Commission's own<br>motion to determine the condition and<br>disposition of six (6) existing structures<br>carrying various highways above the grade<br>of the tracks of the Canadian Pacific<br>Railroad in Great Bend Township, New<br>Milford Township, Brooklyn Township,<br>Hop Bottom Borough, Lathrop Township,<br>Susquehanna County, and Benton<br>Township, Lackawanna County | : I-2015-2472242<br>:<br>: ELECTRONICALLY FILED<br>:<br>:<br>:<br>:<br>:<br>:<br>: |

**CERTIFICATE OF SERVICE**

I hereby certify that I served one (1) copy of *Norfolk Southern Railway Company's Revised Bridge Repair Plans* in the above-referenced matters, this day by electronic mail and U.S. Mail , addressed to:

**Via Electronic Mail**

Jennifer Brown-Sweeney, Esquire  
PennDOT, Office of Chief Counsel  
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Harrisburg, PA 17105  
Via Email: [jbrownswee@pa.gov](mailto:jbrownswee@pa.gov)

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c/o Carol Fernback  
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**Via U.S. Mail**

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2479 State Route 2096  
Hop Bottom, PA 18824

Ijeoma N. Okereke  
Ijeoma N. Okereke, Secretary to  
Benjamin C. Dunlap Jr., Esquire

Date: March, 12 2021