

**BEFORE
PENNSYLVANIA PUBLIC UTILITY COMMISSION**

In re: Application of Gigapower LLC for Application Docket No.
approval of the alteration of the crossing by
the installation of Fiber Optic Cable under
New Commerce Blvd crosses at grade of the
tracks of Luzerne & Susquehanna Railroad,
located in Hanover Township in Luzerne
County.

To Pennsylvania Public Utility Commission:

1. The name and address of applicant are:
GIGAPOWER, LLC (see local mailing address pg. 2)
311 S Akard St. – 21st Floor
Dallas, TX 75202
2. The name and address of applicant's attorney are:
Thomas Grace - CCO and Chief Counsel
GIGAPOWER, LLC
311 S Akard St. – 21st Floor
Dallas, TX 75202
3. GIGAPOWER, LLC provides Optical Network Infrastructure for use by Internet Service Providers and other data transmission.
4. Adding an underground bore of two 4" Conduits for a communications fiber optic cable at the South of intersection of New Commerce Blvd and Stewart Rd at point (41.209573 -75.922762) crossing the Luzerne & Susquehanna Railroad 72' South at point (41.209380, -75.922755).
5. The following municipalities who may be affected by or concerned with the proposed alteration of the crossing are the Hanover Township. The following property owners who may be affected by or concerned with the proposed alteration of the crossing are Luzerne & Susquehanna Railroad.
6. Estimated cost of construction not to exceed \$10,000 – Gigapower LLC. Is responsible for the installation, maintenance, and inspection of such facilities and will be the responsible for all associated costs related to the installation, maintenance, and inspection of such facilities.
7. The alteration is necessary or proper for the following reasons: To provide the residence and the business of the Hanover Township with access to high quality fiber internet.

Wherefore, applicant prays your Honorable Commission to approve the application:

Rich Rehak Jr.
Director of Network Operations NEPA
GIGAPOWER, LLC
Po BOX 179
East Greenville, PA 18041



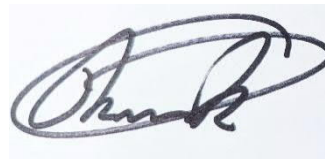
(Signature of Applicant)

§ 1.36. Verification.

- (a) Applications, petitions, formal complaints, motions and answers thereto containing an averment of fact not appearing of record in the action or containing a denial of fact shall be personally verified by a party thereto or by an authorized officer of the party if a corporation or association. Verification means a signed written statement of fact supported by oath or affirmation or made subject to the penalties of 18 Pa. C.S. §4904 (relating to unsworn falsification to authorities). If verification is required, notarization is not necessary.
- (b) The Verification form should comply substantially with the following:

VERIFICATION

I, Rich Rehak Jr. , hereby state that the facts above set forth are true and correct (or are true and correct to the best of my knowledge, information and belief) and that I expect to be able to prove the same at a hearing held in this matter. I understand that the statements herein are made subject to the penalties of 18 Pa. C.S. § 4904 (relating to unsworn falsification to authorities).



Date: January, 16, 2024

(Signature)

CERTIFICATE OF SERVICE

I hereby certify that I have this day served a true copy of the foregoing document upon the participants, listed below, in accordance with the requirements of § 1.54 (*relating to service by participant*).

CTSI, LLC, dba Frontier Communications
ATTN: Mary Rodman (PA.Structure.Access@ftr.com, rd1001@epix.net)
100 CTE Drive
Dallas, PA 18612

Comcast - Duryea
ATTN: Americomm (carol@americomm-llc.com, heather@americomm-llc.com)
21 State Ave (Suite 1)
Carlisle, PA 17013

Verizon Pennsylvania Inc.
ATTN: Empire/North Penn Telephone (ospeng@etcnpt.com)
ATTN: Deborah Barum (poleconduitadmin1@verizon.com, dgcavuto@ctdi.com)
34 Main Street 1026 Hay St.
Prattsburgh, NY 14873 Wilkinsburg, PA 15221

ION Holdco LLC
ATTN: Sheena Wiater (swiater@firstlight.net, smercier@firstlight.net, flf-licensing@firstlight.net, rsitler@firstlight.net)
7890 Lehigh Crossing
Victor, NY 14564

Teleport Communications America LLC (formerly TCG Delaware) CTSI, LLC, dba Frontier Communications
ATTN: Carlo Verdi III (cverdi@att.com, jay@trecgroup.com)
ATTN: Mary Rodman (PA.Structure.Access@ftr.com, rd1001@epix.net)
900 Old Marple Rd. 100 CTE Drive
Springfield, PA 19064

ZitoMedia / Zito West Holding, LLC
ATTN: Kelly Ragosta (kelly.ragosta@zitomedia.com)
PO Box 665 - 102 South Main Street
Coudersport, PA 16915

Railroad:
Margaret M. Thomas
Redevelopment Authority of Luzerne County
16 Luzerne Avenue – Suite #210
West Pittston, Pennsylvania 18643
570-655-3329

PennDOT:
Gonzalo, Jeremiah jgonzalo@pa.gov
55 Keystone Industrial Park Rd
Dunmore, PA 18512

Hanover:
Sam Guesto Township Manager –
1267 Sans Souci Pkwy Hanover Township, 18706
sguesto@hanovertownship.org

(List names and addresses of participants served.)

(Participants should include the Pennsylvania Department of Transportation, the Railroad Company, the county and municipality where the crossing is located, and any fixed utility companies with facilities at the crossing.)

Dated this 2nd day of February, 2024.



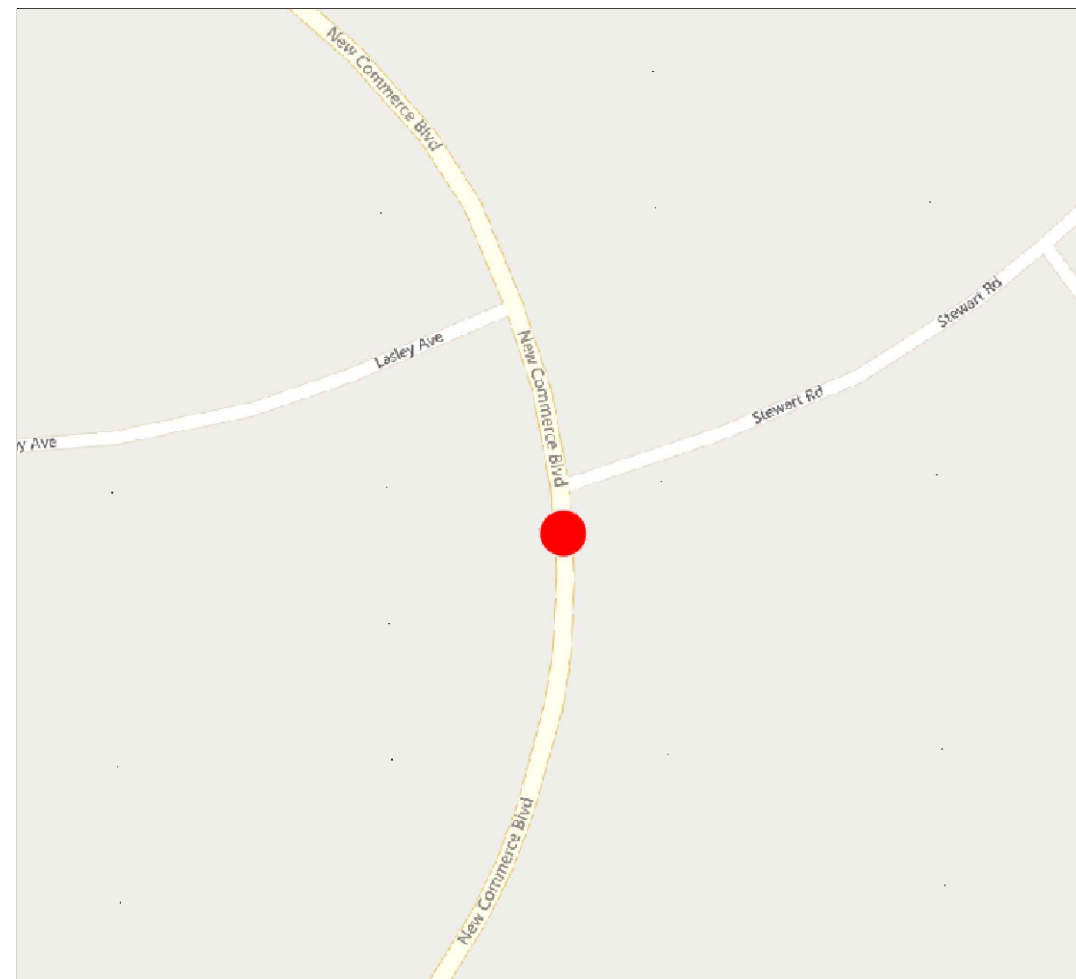
(Signature)



PROPOSED FIBER OPTIC AERIAL BUILD

LUZERNE & SUSQUEHANNA RAILWAY CROSSING

3001PA RR CROSSING #2
NEW COMMERCE BLVD
HANOVER TOWNSHIP, PA
LUZERNE COUNTY



VICINITY MAP
NTS

DRAWING INDEX

SHEET	TITLE
1	COVER
2	LEGENDS
3	GENERAL NOTES
4	GENERAL COSTRUCTION DETAILS
5	PLANVIEW
6	PROFILE VIEW

OWNER:



ENGINEERING FIRM:



4200 CORNING PLACE
CHARLOTTE, NC 28216

NOTES:

SCALE:

NORTH:

REV	DATE	DESCRIPTION
2	DATE	AS-BUILT
1	DATE	REVISED
0	#####	ORIGINAL

SHEET NAME: COVER

PROJECT MANAGER: ----

PROJECT ENGINEER:

PROJECT NAME: LUZERNE & SUSQUEHANNA RR CROSSING

SHEET NO: 1 OF 6



LEGEND

JOINT POLE	⊗	STREET LIGHT	⚙	EXISTING RISER	⊙⊙⊙⊙⊙
JOINT POLE W / TRANSFORMER	⊗	EXISTING GUY	▶	EXISTING GROUND	— — —
POWER POLE	×	PROPOSED GUY	<	PROPOSED RISER W/ FOOTAGE	• R+20'
POWER POLE W/TRANSFORMER	⊗	PROP. SIDEWALK GUY	∧	SPLICE POINT	⊠
TELEPHONE POLE	○	OVERHEAD GUY	∖	SLACK LOOP	⊠

— UG —	UNDERGROUND BUILD
— OB —	OVERHEAD BUILD
— — —	ROAD CENTERLINE
— - - -	RIGHT-OF-WAY (R/W)
— — — — —	EDGE-OF-PAVEMENT (EOP) OR FOG LINE
+++++	TRACK CENTERLINE
HH	HANDHOLE
MH	MANHOLE

OWNER:

GIGAPOWER

ENGINEERING FIRM:

CORNING

4200 CORNING PLACE
CHARLOTTE, NC 28216

NOTES:

SCALE:

NORTH:

2	DATE	AS-BUILT
1	DATE	REVISED
0	#####	ORIGINAL
REV	DATE	DESCRIPTION

SHEET NAME: **LEGENDS**

PROJECT MANAGER: ----

PROJECT ENGINEER:

PROJECT NAME: LUZERNE & SUSQUEHANNA RR CROSSING

SHEET NO: **2 OF 6**

GENERAL NOTES

CONTRACTOR MUST OBTAIN LOCATES PRIOR TO DISTURBING THE GROUND.

CONTRACTOR MUST HAVE A COPY OF THE APPROVED PERMIT FROM THE APPROPRIATE AGENCY ON THE JOBSITE AT ALL TIMES.

ALL CABLE WILL BE PLACED AT A MINIMUM DEPTH OF 36" DEEP UNLESS OTHERWISE DIRECTED BY A REPRESENTATIVE OF THE UTILITY OWNER.

ANY LANDSCAPING WILL BE REPLACED TO EQUAL OR BETTER THAN THAT WHICH EXISTED PRIOR TO WORK.

PROJECT SITE WILL BE PROPERLY SECURED PRIOR TO THE END OF EACH DAY.

ALL WORK IS TO BE IN ACCORDANCE WITH ALL AUTHORITIES HAVING JURISDICTION IN THE WORK ZONE.

CONTRACTORS ARE ADVISED TO CONTACT THE PROJECT MANAGER FOR ANY ADDITIONAL INFORMATION OR CLARIFICATION CONCERNING SCOPE OF WORK OR THE REQUIREMENTS NECESSARY FOR PROJECT COMPLETION.

CONTRACTOR IS RESPONSIBLE TO FIELD VERIFY ALL DIMENSIONS, QUANTITIES AND EXISTING CONDITIONS PRIOR TO CONSTRUCTION. IF A SIGNIFICANT CHANGE TO THE RUNNING LINE IS NEEDED, PLEASE CONTACT THE PROJECT MANAGER BEFORE PROCEEDING.

BEFORE CONSTRUCTION BEGINS, CONTRACTOR SHALL TAKE APPROPRIATE PRECAUTIONS TO AVOID ANY POTENTIAL OBSTRUCTIONS PRIOR TO PROCEEDING WITH WORK.

NO CONSTRUCTION ON PRIVATE PROPERTY WILL COMMENCE UNTIL APPROVAL IS GIVEN BY A REPRESENTATIVE OF THE UTILITY OWNER.

CONTRACTOR SHALL NOT PROCEED WITH WORK UNTIL THEY HAVE RECEIVED A PURCHASE ORDER AND HAVE BEEN DIRECTED TO DO SO BY AN AUTHORIZED REPRESENTATIVE OF THE UTILITY OWNER.

CONTRACTOR SHALL NOT EXCEED THE PURCHASE ORDER VALUE WITHOUT AUTHORIZATION IN WRITING FROM THE APPROPRIATE REPRESENTATIVE OF THE UTILITY OWNER.

AS-BUILTS WILL BE REQUIRED FOR EACH PROJECT INCLUDING CABLE FOOTAGE SEQUENTIALS AT EVERY ACCESS POINT, SLACK LOOP, SPLICE LOCATION, POLE AND TERMINATION POINT. CONTRACTOR SHOULD ALSO PROVIDE NOTES OF ALL CHANGES IN DEPTHS, RUNNING LINES, MH/HH LOCATIONS, AND ANY OTHER APPLICABLE NOTES TO DEPICT THE WORK THAT TOOK PLACE. NOTE: ALL MAJOR CHANGES NEED TO BE PRE-APPROVED BY AN AUTHORIZED REPRESENTATIVE OF THE UTILITY COMPANY PRIOR TO STARTING THE WORK.

SITE CONDITIONS

THE ACTUAL LOCATION OF EXISTING CONDUIT AND CABLES MAY VARY FROM THE LOCATION SHOWN. REPAIR OF ANY DAMAGED CONDUIT CONTAINING CABLE SHALL BE MADE BY USE OF PVC SPLIT DUCT. THE CONTRACTOR SHALL ENCLOSE THE EXISTING CABLES IN PVC.

THE LOCATIONS OF EXISTING UTILITIES SHOWN IN THIS PLAN ARE APPROXIMATE. WHEN WORK IS TO BE CONDUCTED IN THE VICINITY OF KNOWN UTILITIES, THEIR ACTUAL LOCATION MUST BE FIELD VERIFIED TO AVOID CONFLICTS OR DAMAGE TO THOSE UTILITIES. VARIATION IN LOCATION BETWEEN "RECORDED POSITIONS" AND ACTUAL POSITIONS SHOULD BE ANTICIPATED.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION OF ALL UNDERGROUND UTILITIES. BURIED UTILITIES MAY EXIST IN THE AREA IN ADDITION TO THOSE SHOWN ON THE PLAN. THE CONTRACTOR SHALL CONTACT PROPERTY OWNERS WHEN WORKING WITHIN PRIVATE EASEMENTS FOR LOCATION OF UNDERGROUND TANKS, PIPELINES, DRAIN TILES, OR OTHER BURIED IMPROVEMENTS. THE CONTRACTOR SHALL ALSO NOTIFY THE UTILITY NOTIFICATION CENTER PRIOR TO COMMENCING ANY CONSTRUCTION ACTIVITIES.

THE CONTRACTOR MUST ASSUME ALL BURIED UTILITIES ENCOUNTERED ARE ALIVE AND ACTIVE UNLESS SPECIFICALLY INSTRUCTED OTHERWISE BY OWNERS OR OPERATORS OF SAID UTILITIES.

DAMAGE TO SUB-SURFACE STRUCTURES IS THE SOLE RESPONSIBILITY OF THE PLACING CONTRACTOR.

THE CONTRACTOR SHALL PROTECT THE EXISTING TRAFFIC CONTROL LOOPS. IF EXISTING TRAFFIC CONTROL LOOPS ARE DAMAGED DURING CONSTRUCTION, THE ENTIRE LOOP WIRE FROM TERMINAL TO TERMINAL SHALL BE REPLACED IN ACCORDANCE WITH GOVERNING AGENCY STANDARDS AND REGULATIONS AT CONTRACTOR'S EXPENSE.

REMOVAL OF EXISTING ASPHALT PAVEMENT, CONCRETE CURBS, AND CONCRETE SIDEWALKS WILL BE "NEAT LINE" WITH SAW OR PAVEMENT CUTTER, PER REQUIREMENTS AND SPECIFICATIONS OF THE AGENCY OR DEPARTMENT RESPONSIBLE FOR EACH LOCATION.

IF CONCRETE PAVEMENT IS ENCOUNTERED WHILE EXCAVATING CONDUIT TRENCHES, THE CONCRETE REMOVAL WILL BE "NEAT LINE" WITH A PAVEMENT SAW. IF CONCRETE CURB RETURNS AND/OR SIDEWALKS ARE REPLACED DUE TO CONDUIT OR MANHOLE INSTALLATION, THE CONTRACTOR SHALL PLACE APPROVED HANDICAPPED SIDEWALK AND CURB ACCESS RAMPS IN CONFORMANCE WITH THE APPROPRIATE STATE STATUTES.

ALL MATERIALS NECESSARY FOR THE REPAIR OF STREETS, CURBS, SIDEWALKS, SANITARY SEWERS, STORM SEWERS, AND PUBLIC SERVICE UTILITIES, AND THE INSTALLATION OF SUCH MATERIALS SHALL BE IN CONFORMANCE WITH THE REQUIREMENTS AND SPECIFICATIONS OF THE AGENCY OR DEPARTMENT RESPONSIBLE FOR THE OPERATION AND MAINTENANCE OF THE REPAIRED FACILITY.

ALL WORK SHALL CONFORM TO THE SPECIFICATIONS OF THE JURISDICTIONAL PERMIT AGENCY.

ALL OPEN TRENCH WILL BE CLEARLY MARKED WITH BARRICADES OR CONES. STEEL PLATES OR OTHER TYPES OF BRIDGING SHALL BE PROVIDED TO COVER OPEN TRENCH IN THE TRAVEL PORTION OF THE STREETS. THESE PLATES OR BRIDGING SHALL BE ADEQUATE TO SUPPORT THE NORMAL VEHICLE LOADS ANTICIPATED IN THIS AREA AND SHALL BE IN PLACE DURING ALL NON-WORKING AREAS.

ALL SURFACES TO BE RESTORED TO ORIGINAL CONDITION, AND BACKFILL TO BE COMPACTED AS SPECIFIED. TRENCH EXCAVATION IN SURFACES WHICH INCLUDE CONCRETE TREATED BASE SHALL FOLLOW LOCAL AREA SPECIFICATIONS.

HAZARDOUS MATERIALS

THE CONTRACTOR SHALL NOTIFY THE JURISDICTIONAL PERMIT AGENCY IMMEDIATELY IF ANY MATERIALS ARE ENCOUNTERED THAT ARE CONSIDERED HAZARDOUS BY THE EPA, DEQ, OR OSHA. IF POTENTIALLY HAZARDOUS MATERIALS ARE ENCOUNTERED THE CONTRACTOR SHALL SECURE THE SITE AND PREVENT THE ACCIDENTAL EXPOSURE BY THE PUBLIC OR THE CONTRACTOR'S PERSONNEL.

THE CONTRACTOR MAY EXCAVATE UP TO, BUT SHALL NOT DISTURB KNOWN HAZARDOUS MATERIALS SUCH AS ASBESTOS, OILS, ACID, ETC. THE REMOVAL OF ALL HAZARDOUS MATERIALS MUST BE DONE BY AN APPROVED OR CERTIFIED HAZARDOUS MATERIALS CONTRACTOR LICENSED IN THE APPROPRIATE STATE.

A COPY OF ALL CORRESPONDENCE PERTINENT TO THE REMOVAL OF HAZARDOUS MATERIALS SHALL BE TRANSMITTED TO OWNER AND A COPY SHALL BE AVAILABLE AT THE PROJECT OFFICE AND THE JOB SITE.

AERIAL NOTES

AERIAL CONSTRUCTION TO BE PERFORMED TO INDUSTRY ACCEPTABLE STANDARDS.

ALL HEIGHTS OF CABLE PLACEMENT WILL BE RECORDED AT TIME OF CONSTRUCTION. DOCUMENT ALL POINTS OF ATTACHMENT.

ALL EXTENSION ARMS TO BE PLACED WILL BE EPOXY ARMS UNLESS OTHERWISE NOTED OR APPROVED BY THE INSPECTOR.

BOND STRAND TO POWER MGN WHERE APPLICABLE. ANCHORS TO BE USED WILL BE 3/4 SCREW IN TYPE.

ALL STRAPS WILL BE PLACED 4" BEFORE AND AFTER EVERY SUPPORTING CLAMP AT A MINIMUM OF 21" APART.

P.O.A. = POINT OF ATTACHMENT.

ADD MISSING GROUNDS.

REPAIR/REPLACE EXISTING LASHING WIRE IF DAMAGED.

CONSTRUCTION STAKING

IN AREAS WHERE THE CONDUIT ALIGNMENT IS NOT CLEARLY DEFINED BY CURB LINES, FENCE LINES, OR OTHER EVIDENCE OF THE RIGHT-OF-WAY, THE ENGINEER WILL PROVIDE CENTERLINE STAKES OR PAINT MARKS WHERE REQUIRED TO MAKE THE PROPOSED CONDUIT ALIGNMENT EVIDENT.

MANHOLE CENTERS WILL BE FIELD STAKED BY THE ENGINEER WHEN REQUESTED WITH OFFSET STAKES AT RIGHT ANGLES (90°) TO THE CONDUIT ALIGNMENT.

CLOSURES IDENTIFIED IN THE PLANS SHALL BE LOCATED BY THE CONTRACTOR. DEVIATION FROM PLAN LAYOUT SHALL BE APPROVED BY THE ENGINEER PRIOR TO CONDUIT AND/OR CLOSURE INSTALLATION.

IF ADDITIONAL, FIELD STAKING OR LOCATION OF CONDUITS, MANHOLES, PROPERTY LINES, ETC., BECOMES NECESSARY, THE CONTRACTOR IS TO NOTIFY THE INSPECTOR OR THE ENGINEER TWO WORKING DAYS PRIOR TO BEGINNING THE WORK.

PERMITS - FRANCHISES - EASEMENTS

PHYSICAL WORK SHALL NOT BE STARTED UNTIL THE GOVERNING AGENCY INSPECTOR AND THE CONTRACTOR ARE IN POSSESSION OF AND HAVE CAREFULLY REVIEWED AND FULLY UNDERSTAND ALL CONDITIONS AND SPECIFICATIONS SET FORTH IN THE REQUIRED PERMITS, FRANCHISES, AND/OR EASEMENTS.

PLACING FOREMAN TO HAVE A COPY OF THE PERMITS/EASEMENTS ON SITE AT ALL TIMES.

ANY CONFLICT BETWEEN WORK PRINT SPECIFICATIONS AND SPECIFICATIONS SET FORTH UNDER RELATED PERMITS, FRANCHISES, AND/OR EASEMENTS MUST BE CLEARED BY PROPER COMPANY AUTHORITY BEFORE PROGRESSING WITH WORK INVOLVED

TRAFFIC CONTROL

THIS PROJECT WILL INVOLVE WORKING ALONG A MAJOR ARTERIAL ROAD AND HEAVY TRAFFIC VOLUME SHOULD BE ANTICIPATED.

UNIFORM TRAFFIC FLOW SHALL BE MAINTAINED AT ALL TIMES. ONLY EQUIPMENT AND MATERIALS NECESSARY FOR IMMEDIATELY SCHEDULED OR IN PROGRESS WORK WILL BE MAINTAINED IN THE WORK AREA. ALL OTHER EQUIPMENT AND MATERIALS WILL BE "STORED OR STOCKPILED" IN SUCH A MANNER AS TO ELIMINATE HAZARDOUS CONDITIONS FOR TRAFFIC OR PEDESTRIANS DURING NON-WORKING OR SHUT DOWN PERIODS.

TRAFFIC WARNING DEVICES AND SIGNS SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS (U.S. GOVERNMENT PRINTING OFFICE) AND TO THE STATE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION. HIGH LEVEL WARNING TYPE DEVICES ARE TO BE USED AT ALL TIMES AND SPECIAL WARNING DEVICES MAY BE STIPULATED BY THE JURISDICTIONAL PERMIT AGENCY AT ANY TIME THE USE WILL ADD TO THE SAFETY AND PROTECTION OF TRAFFIC OR PEDESTRIANS IN THE CONSTRUCTION AREA.

ALL CONDUIT TRENCHING IN PAVED AREAS SHALL BE BACKFILLED WITH CRUSHED GRAVEL OR COMPLETELY COVERED AT THE COMPLETION OF EACH WORKING DAY. ANY BACKFILLED TRENCH SHALL BE CAPPED WITH A MINIMUM LAYER OF ASPHALTIC CONCRETE COLD PATCH AT THE END OF EACH WORKING DAY.

THE CONTRACTOR SHALL MARK THE CONDUIT TRENCH AND DEFINE HIS CONSTRUCTION AREA CLEARLY WITH BARRICADES, CONES, AND/OR OTHER VISIBLE METHODS THAT ALERT THE PUBLIC OF THE CONSTRUCTION ACTIVITY.

A TRAFFIC CONTROL PLAN SHALL BE PREPARED BY THE CONTRACTOR AS REQUIRED AND SUBMITTED TO EACH PERMITTING AGENCY REQUESTING SUCH PLAN FOR REVIEW AND APPROVAL OR REVISION PRIOR TO COMMENCING ANY CONSTRUCTION ACTIVITY FOR THIS PROJECT. THE APPROVED PLAN SHALL BE SUBMITTED TO THE AGENCY AND A COPY OF THE PLAN SHALL BE KEPT AT THE CONSTRUCTION SITE AND MUST BE READILY AVAILABLE REVIEW BY THE AGENCY REPRESENTATIVES.

SPECIAL UTILITY CLEARANCES

ALL WORK CONDUCTED ADJACENT TO WATER MAINS SHALL CONFORM TO THE FOLLOWING CONDITIONS:

A. WHEREVER POSSIBLE CONDUIT SHALL MAINTAIN A HORIZONTAL SEPARATION OF 3.0 FEET, MEASURED SURFACE TO SURFACE (OUTSIDE EDGE TO OUTSIDE EDGE), FROM PARALLEL WATER MAINS.

B. WHEREVER POSSIBLE, CONDUIT SHALL PASS UNDER EXISTING WATER MAINS AND MUST MAINTAIN 12" VERTICAL CLEAR SEPARATION. CONDUITS PASSING OVER WATER MAINS MUST ALSO MAINTAIN THE 12" VERTICAL SEPARATION.

C. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ESTABLISHING AND MAINTAINING THIS REQUIRED VERTICAL SEPARATION BY EITHER EXPOSING THE WATER MAIN EVERY 100 FEET IN THOSE AREAS WHERE HORIZONTAL SEPARATION IS LESS THAN 3.0 FEET OR BY UTILIZING THE DEPTHS OF ADJACENT WATER VALVES. IF THE CONTRACTOR UTILIZES THE ADJACENT WATER TO DETERMINE WATER MAIN DEPTH, HE SHALL CONTACT THE AGENCY AT EACH SUCH LOCATION AND THE AGENCY WILL DETERMINE THE NECESSARY DEPTH OF THE TOP OF THE CONDUIT AT THAT POINT.

D. THE VERTICAL AND HORIZONTAL SEPARATION SHALL BE MAINTAINED AT ALL TIMES UNLESS SPECIFICALLY REVISED BY AGREEMENT BETWEEN THE JURISDICTIONAL PERMIT AGENCY AND THE AGENCY ANY SPECIFIC DEVIATION IN VERTICAL AND HORIZONTAL SEPARATION FROM THOSE DESCRIBED SHALL BE REPORTED TO THE OWNER BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING VERTICAL AND HORIZONTAL SEPARATION AT ALL TIMES AND SHALL BE RESPONSIBLE FOR ANY AND ALL ENCROACHMENTS.

E. CLEARANCES TO STORM SEWERS AND SANITARY SEWERS SHALL BE EXACTLY THE SAME AS THOSE TO WATER MAINS.

STRUCTURE PROTECTION

MANHOLES AND CONDUIT TO BE PLACED ADJACENT TO EXISTING STRUCTURES SUCH AS BRIDGE BRIDGE FOOTINGS/PIERS, BUILDING FOUNDATIONS, WALLS, POWER AND TELEPHONE POLES, AND OTHER UTILITIES SHALL MAINTAIN A MINIMUM CLEARANCE AS SHOWN. THE CONTRACTOR SHALL NOT UNDERMINE ANY ADJACENT STRUCTURE WITHOUT SPECIFIC WRITTEN PERMISSION FROM THE OWNER/OPERATOR OF SUCH STRUCTURE.

SHORING USED AS FOUNDATION SUPPORT SHALL BE DESIGNED SPECIFICALLY FOR BOTH THE LIVE AND DEAD LOADS OF THE STRUCTURE. OR IF ONLY THE DEAD LOAD IS USED FOR DESIGN, THE CONTRACTOR SHALL PROVIDE A DETAILED LAYOUT AND PLAN OF THE METHOD OF ESTABLISHING AND MAINTAINING THE DESIGN LOAD CONDITIONS (I.E., ROAD DETOURS, TIEBACKS, ETC.).

SEE UTILITY CLEARANCE SECTION NOTES FOR CLEARANCE CRITERIA TO PARALLEL OR CROSS UTILITIES.

EXISTING UTILITIES EXPOSED DURING EXCAVATION SHALL BE 100% SUPPORTED BY EITHER TRENCH BRIDGING AND SUSPENSION OR BY THE USE OF LONGITUDINAL TRAYS OR PLATFORMS VERTICALLY SUPPORTED BY ADJUSTABLE BUILDING JACKS.

EXISTING SPLICE CASES AND CABLES SHALL BE SUPPORTED BY SUSPENSION FROM A CROSSING BEAM. SUPPORTS SHALL BE PLACED AT A MAXIMUM SPACING OF 4.0 FEET AND SHALL CONSIST OF A CANVAS SLING WITH NYLON BELTING OR ROPE. ALL CABLE SUPPORTS SHALL BE PLACED IN A MANNER THAN PREVENTS KINKS OR OTHER DAMAGE TO THE CABLE SHEATH.

AN ACCEPTABLE ALTERNATIVE TO CABLE SLINGS WOULD BE THE UTILIZATION OF A WIDE FLANGE "I" BEAM OR CHANNEL AS A "CABLE TRAY" WITH THE CABLES/CASES BANDED IN PLACE.

SHORING

THE CONTRACTOR SHALL PROVIDE SHORING FOR CONDUIT TRENCH EXCAVATION 42" OR MORE IN DEPTH AS MEASURED FROM THE HIGH SIDE OF THE TRENCH AND FOR ALL MANHOLE EXCAVATION.

MANHOLE SHORING SHALL BE TIGHT-SHEETED.

ALL SHORING SHALL CONFORM TO THE STANDARDS AND SPECIFICATIONS OF LOCAL COUNTY AND THE OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA).

SHORING SHALL BE DESIGNED TO MEET H-20 HIGHWAY LOADING.

THE CONTRACTOR SHALL PROVIDE ALL SHORING AND DESIGN CALCULATIONS TO THE PERMIT ISSUING AGENCY PRIOR TO COMMENCING ANY CONSTRUCTION ACTIVITY

CORNING

SHEET NAME:		GENERAL NOTES
PROJECT MANAGER:	----	
PROJECT ENGINEER:		
PROJECT NAME:	LUZERNE & SUSQUEHANNA RR CROSSING	
SHEET NO:	3 OF 6	



GENERAL AERIAL CONSTRUCTION DETAILS

OWNER:



ENGINEERING FIRM:

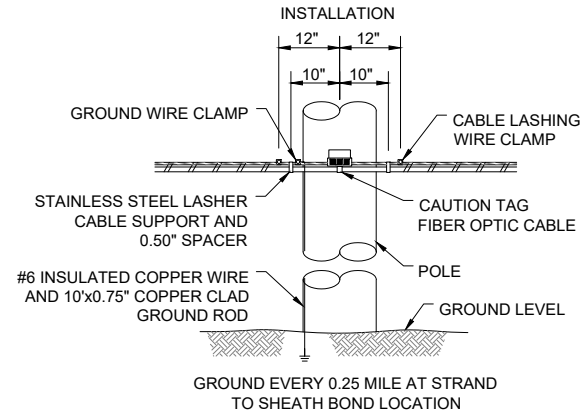


4200 CORNING PLACE
CHARLOTTE, NC 28216

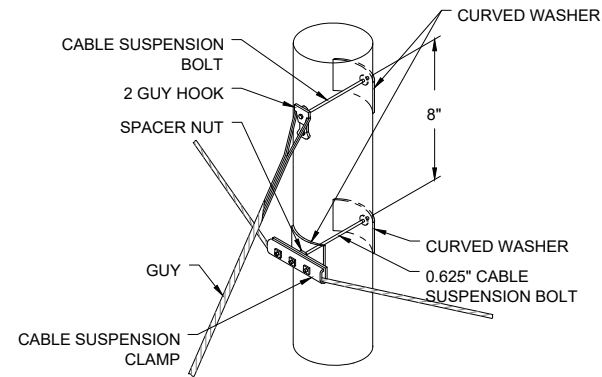
NOTES:

AERIAL CONSTRUCTION NOTES:

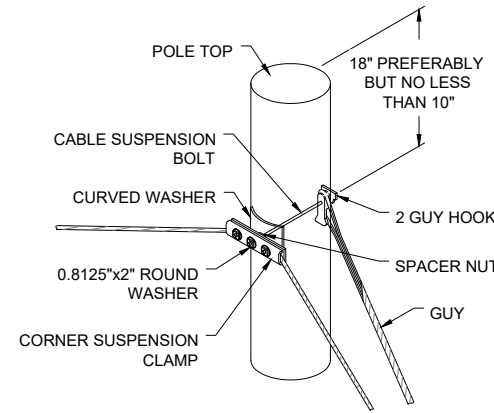
1. STRAND CONSTRUCTION SHALL CONFORM TO MATERIAL CONTAINED IN THE NATIONAL ELECTRIC SAFETY CODE (NESC), INCLUDING SECTION 25, ALONG WITH RELEVANT NEC, OSHA, AND UTILITY RULES AND STANDARDS. ALL DESIGN SHALL INSURE THE APPROPRIATE STORM-LOAD DISTRICTS ARE USED AS DEFINED BY THE NESC.
2. STRAND SHALL BE A MINIMUM OF 6.6M EHS (CLASS A GALVANIZED) OR STRONGER AS REQUIRED BY THE NESC OR CONTROLLING STANDARDS.
3. STRAND SHALL BE SAGGED AND TENSIONED IN ACCORDANCE WITH APPROPRIATE CODE, UTILITY AGREEMENTS, AND STANDARD INDUSTRY ACCEPTED PRACTICES. ALL DOWN GUYS SHALL HAVE A MINIMUM OF 8 FT OF YELLOW DOWN GUY MARKER INSTALLED ON THE DOWN GUY STRAND.
4. ALL ANCHORS SHALL BE SCREW TYPE ANCHORS, AND PLACEMENT, SIZE, AND DETAILS OF THE ANCHORS SHALL BE DOCUMENTED ON THE DESIGN DRAWINGS. ANCHOR SPACING MUST BE PER POWER COMPANY STANDARDS.
5. ALL AERIAL HARDWARE SHALL BE GALVANIZED STEEL.
6. GROUNDING OF THE STRAND AND ASSOCIATED CABLE SHALL BE PER POWER COMPANY STANDARDS, AND SHALL CONFORM TO GROUNDING STANDARDS OUTLINED WITHIN THE NESC, NEC, AND RELEVANT UTILITY STANDARDS.
7. ALL STRAND INSTALLATION SHALL BE IN ACCORDANCE WITH THE UTILITY LICENSE GRANTED TO DOBSON FIBER FOR THE PLACEMENT OF STRAND, INCLUDING ATTACHMENT LOCATION, GUYING, AND GROUNDING.



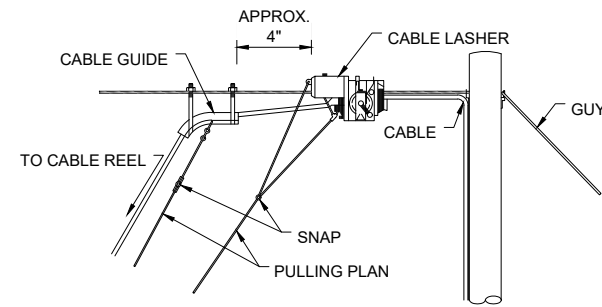
TYPICAL DETAIL "B"
TYPICAL CABLE & HARDWARE



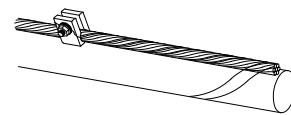
TYPICAL DETAIL "C"
SUSPENSION STRAND - PULL TOWARD
POLE - LESS THAN 5 FEET



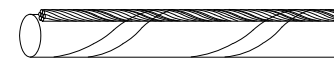
TYPICAL DETAIL "D"
SUSPENSION STRAND - PULL AWAY
FROM POLE - 5 FEET OR MORE



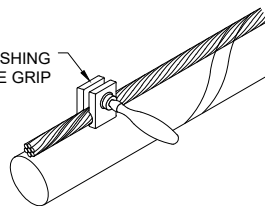
TYPICAL DETAIL "E"
TYPICAL ARRANGEMENT OF
CABLE LASHER AND CABLE GUIDE



**FORMING WIRE OVER
STUD OF CLAMP**
REMOVE ANY SLACK IN THE LASHING WIRE BY MAINTAINING A PULL ON THE WIRE AND TAPPING THE STRAND SHARPLY. THEN FORM THE WIRE OVER THE STUD AND TIGHTEN THE NUT. CUT THE FREE END OF THE LASHING WIRE OFF 0.75" BEYOND THE END OF THE CLAMP.

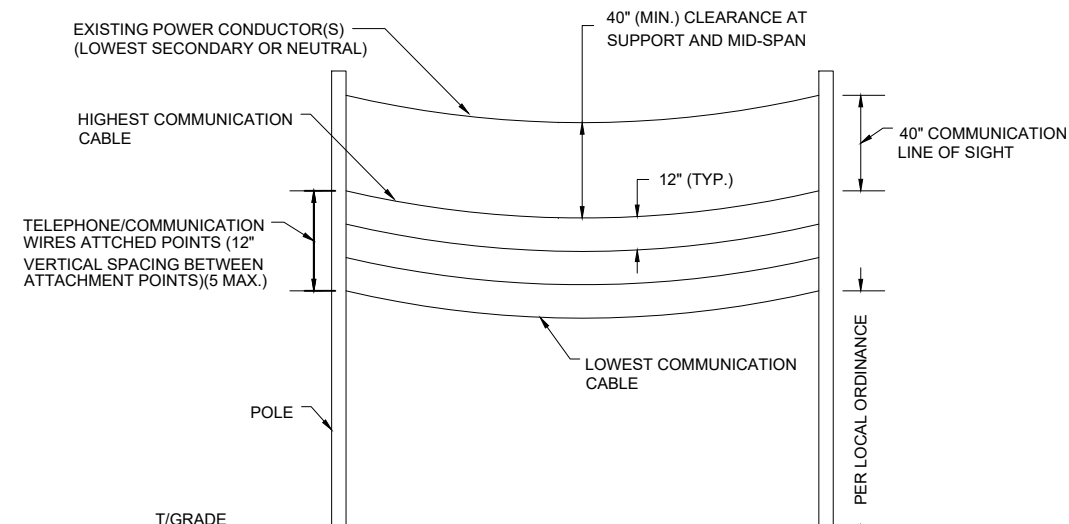


**FORMING LASHING WIRE
AROUND STRAND**
FORM THE LASHING WIRE AROUND THE STRAND AND PLACE IT BELOW THE STUD AND BETWEEN THE SECOND WASHER AND STUD SHOULDER.

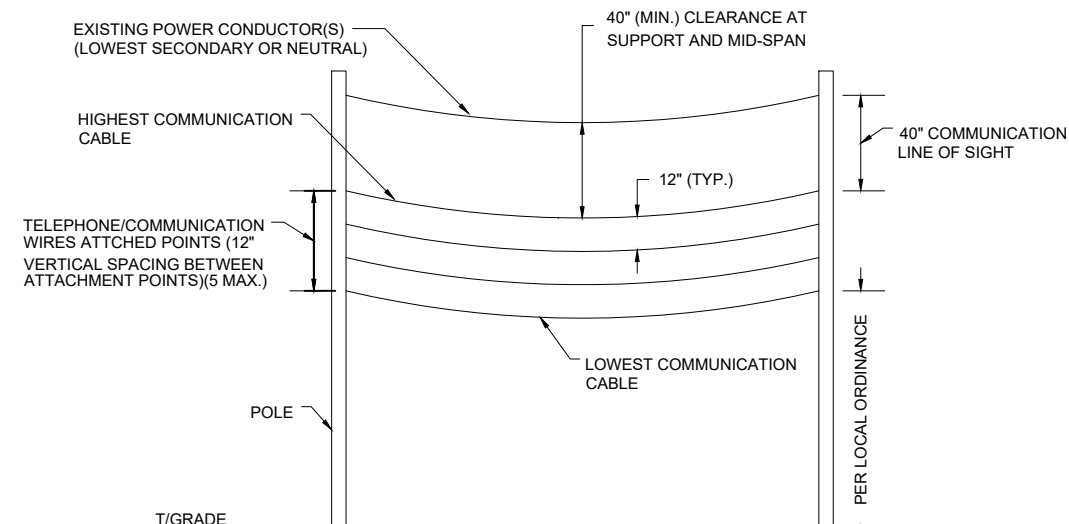


NOTE:
LASHING WIRE SHOULD FOLLOW LAY OF STRAND WIRES UNDER GRIP.

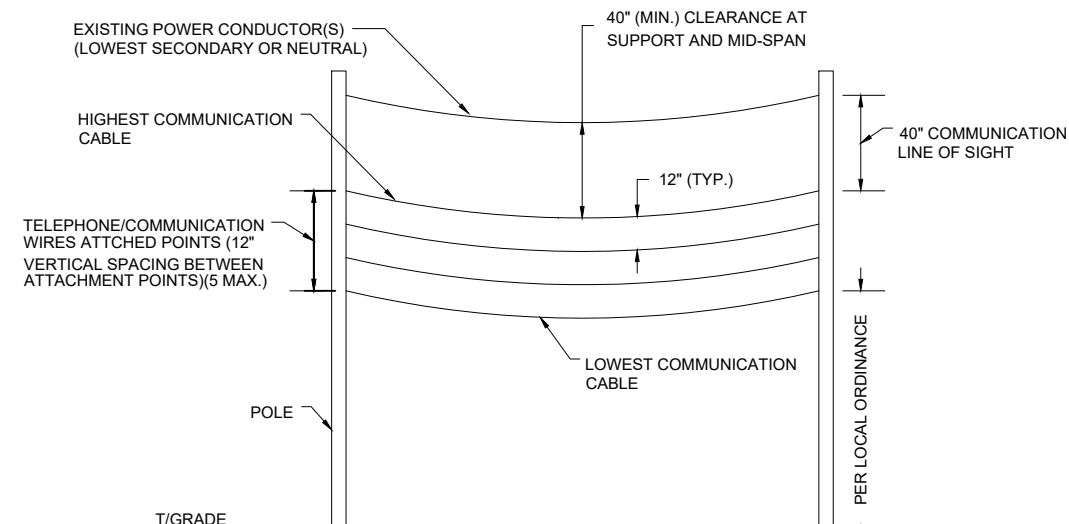
TYPICAL DETAIL "F"
JOINING 045 LASHING WIRE



TYPICAL DETAIL "G"
ANCHOR & DOWN GUY



TYPICAL DETAIL "H"
LASHING WIRE GRIP AND CLAMP



**MIN. VERTICAL CLEARANCE OF COMM.
CABLES**

SCALE:

NORTH:

REV	DATE	DESCRIPTION
2	DATE	AS-BUILT
1	DATE	REVISED
0	#####	ORIGINAL

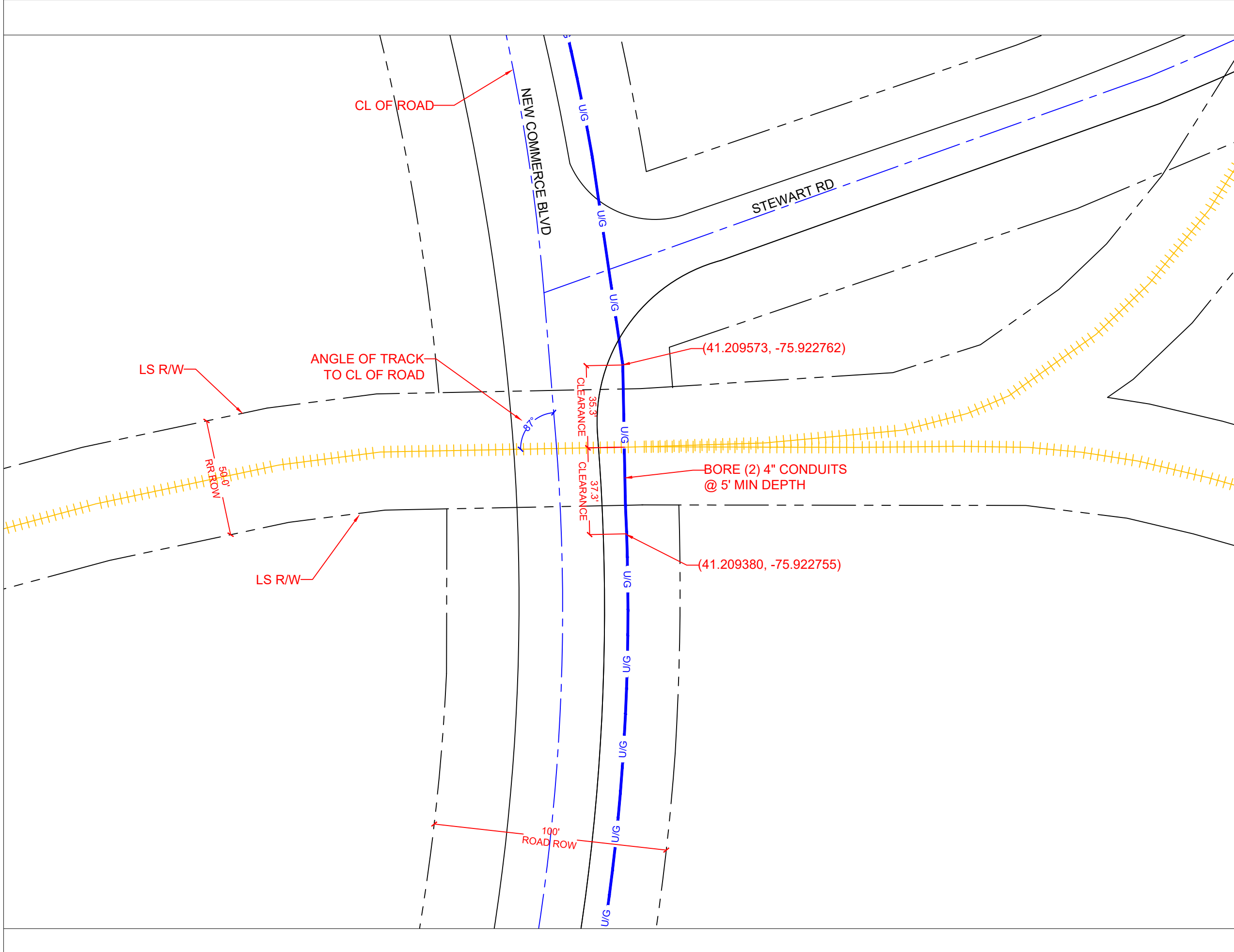
SHEET NAME:
GENERAL CONSTRUCTION DETAILS

PROJECT MANAGER: ----

PROJECT ENGINEER:

PROJECT NAME: LUZERNE & SUSQUEHANNA RR CROSSING

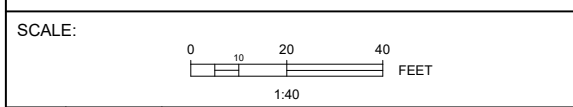
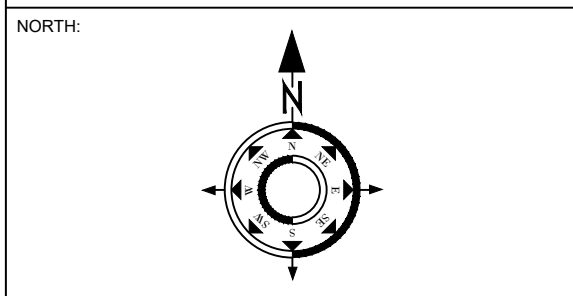
SHEET NO:
4 OF 6



OWNER:
GIGAPOWER

ENGINEERING FIRM:
CORNING
 4200 CORNING PLACE
 CHARLOTTE, NC 28216

NOTES:



2	DATE	AS-BUILT
1	DATE	REVISED
0	#####	ORIGINAL
REV	DATE	DESCRIPTION

SHEET NAME: **PLANVIEW**

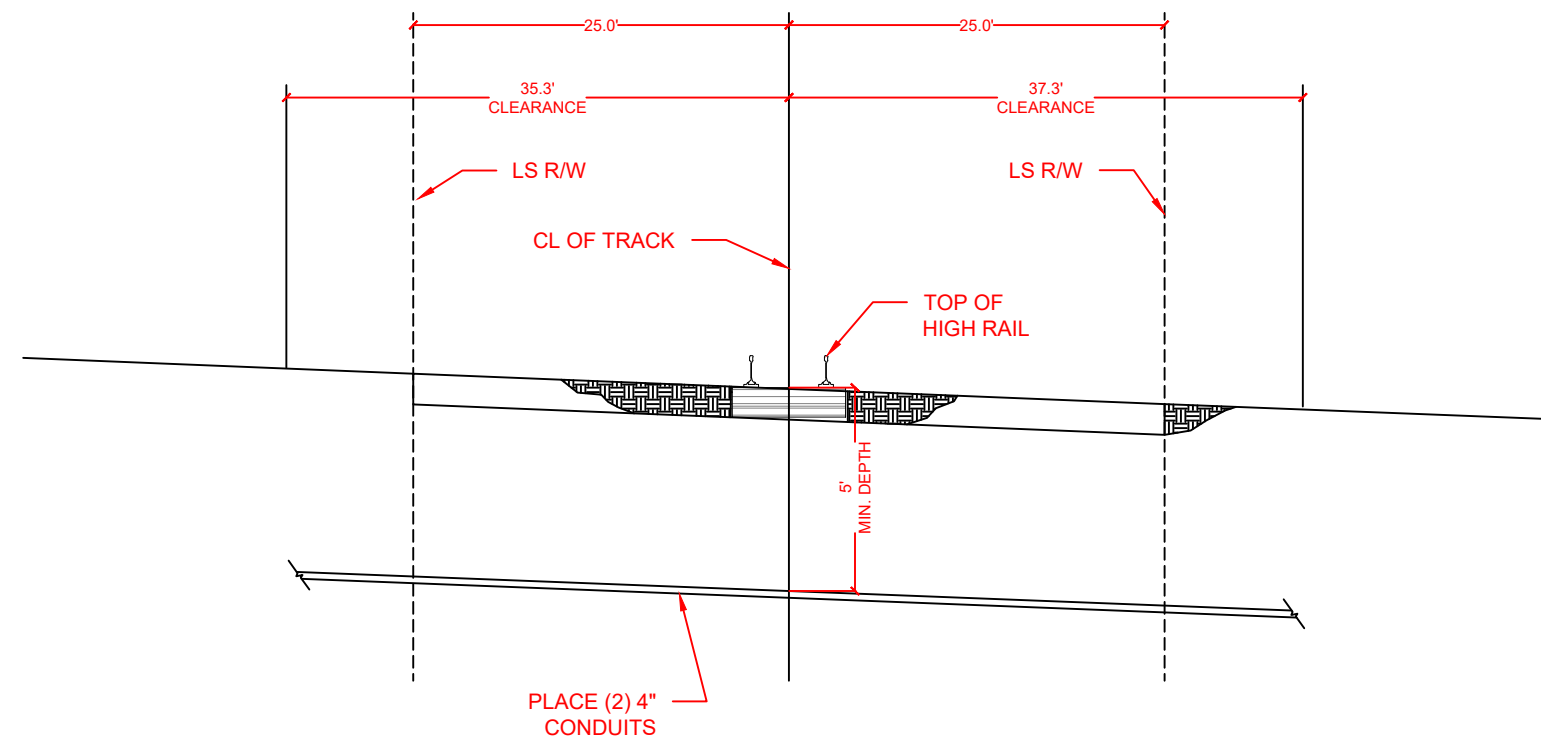
PROJECT MANAGER: ----

PROJECT ENGINEER:

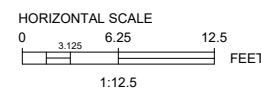
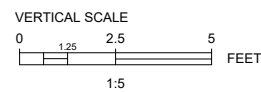
PROJECT NAME: LUZERNE & SUSQUEHANNA RR CROSSING

SHEET NO: **5 OF 6**

LUZERNE & SUSQUEHANNA RAILWAY BURIED CROSSING



PROFILE VIEW WIRELINE / CABLE



OWNER:

GIGAPOWER

ENGINEERING FIRM:

CORNING

4200 CORNING PLACE
CHARLOTTE, NC 28216

NOTES:

SCALE:

NORTH:

2	DATE	AS-BUILT
1	DATE	REVISED
0	#####	ORIGINAL
REV	DATE	DESCRIPTION

SHEET NAME: **PROFILE VIEW**

PROJECT MANAGER: ----

PROJECT ENGINEER:

PROJECT NAME: LUZERNE & SUSQUEHANNA RR CROSSING

SHEET NO: **6 OF 6**