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ENGINEERS • PLANNERS • SCIENTISTS • CONSTRUCTION MANAGERS

5001 Louise Drive, Suite 201 • Mechanicsburg, PA 17055 • Phone 717-691-1340 • Fax 717-691-3470

July 1, 2024

MCDP 0801

The Secretary of the Commission: Rosemary Chiavetta
Pennsylvania Public Utility Commission
400 North Street
Harrisburg, PA 17120

RE: APPLICATION OF MONTGOMERY COUNTY FOR APPROVAL TO RECONSTRUCT OVERHEAD CROSSING FOR NORFOLK SOUTHERN DALE SECONDARY LINE AT RIDGE PIKE, SECTION MG4, (MONTGOMERY COUNTY BRIDGE NO. 0), PLYMOUTH TOWNSHIP, MONTGOMERY COUNTY

Dear Rosemary:

Enclosed are the original and three (3) copies of the PUC submission for the above referenced project for review and approval. Our submission consists of the following items:

1. PUC Application
2. Certificate of Service
3. Project Description
4. Location/Index Map (Showing Limits of Work)
5. Typical Roadway Sections
6. Highway Plan and Profile Sheets
7. Preliminary Structures Drawings (TS&L Approved)
8. TS&L Approval from PennDOT
9. Right-of-Way Plans
10. Quad Map

In accordance with PennDOT Publication 371, Section 3.05.G the required written information is provided as follows:

Montgomery County Bridge No. 0 (L-443):

- 1) Location: Montgomery County Ridge Pike Section MG4, Station 133+43.09 over Norfolk Southern Railroad Dale Secondary Line in Plymouth Township, Montgomery County.
- 2) Purpose and Estimated Cost: Replacement of current structure. Replacement of the structure will improve vertical clearance. The estimated cost of the replacement/construction of the

Employee-Owned Since 1988

bridge is approximately \$22,071,174.00. The total cost of the project is estimated to be \$26,870,000.00.

- 3) Programs: Preliminary Engineering – 0% Federal, 80% State, 20% County
Final Design – 0% Federal, 80% State, 20% County
Right-of-Way – 0% Federal, 80% State, 20% County
Utilities – 0% Federal, 80% State, 20% County
Construction – 0% Federal, 80% State, 20% County
Tentative Letting Date – 1/9/2025
- 4) Name of Railroad: Norfolk Southern - Dale Secondary Line.
- 5) Existing Roadway: 64'-6" out-out, 49'-0" curb-curb. This includes two 12'-0" lanes, two 12'-6" lanes, two 1'-6" barriers, and two 8'-0" sidewalks. All lanes have an asphalt wearing surface.
- 6) Existing Alignment: Tangent.
Grade: 1.47% to -1.06% (Bridge located on vertical curve).
- 7) Paving in Track: N/A – Grade Separation
- 8) Protection Signals: N/A – Grade Separation
- 9) Proposed Roadway: Varies 108'-9½" to 100'-9¼" out-out, Varies 93'-3¼" to 82'-8½" curb-curb. This includes five 11'-0" lanes, one 13'-0" shoulder, one 14'-8½" shoulder (measured radially), and two 8'-0" sidewalks. Type of pavement on westbound approach is asphalt pavement (subbase, base course, binder course and wearing course). Type of pavement on eastbound approach is concrete pavement (subbase, base course and concrete).
- 10) Proposed Alignment: Tangent to Station 134+25.89. Radius of Curvature: 2,414.50'.
Grade: 7.60% to -2.00% (Bridge located on vertical curve).
- 11) Additional Protection at Crossing: No additional protection required.
- 12) Railroad Traffic: Volume – 5+ daily
Class – Freight
Max. Speed – 40 mph
- 13) Current ADT: 35,252 (2024)
Current ADTT: 2,820 (2024)
Projected ADT: 38,152 (2048)
Projected ADTT: 3,052 (2048)
Class: Principal Urban Arterial
Posted Speed: 25 mph
Design Speed: 35 mph

14) Abolish Existing Crossing: N/A

15) Clearances: Vertical – 22'-0" Existing; 23'-7¼" Proposed; 23'-6" Required
Horizontal LT – 21'-7" Existing; 28'-5" Proposed; 28'-0" Required
Horizontal RT – 7'-9¼" Existing; 23'-1¼" Proposed; 22'-0" Required

16) Signalized Intersection: N/A

17) Names and addresses of local municipality, county, public utilities, and local authorities: See enclosed Certificate of Service.

Should you have questions or comments, please contact me at (717) 516-7612.

Sincerely,
KCI TECHNOLOGIES INC.

Brian C. Pinamonti

Brian C. Pinamonti, P.E.

Enclosures

cc: PennDOT ROW & Utility Division
PennDOT Office of Chief Counsel
Mary Lang – PennDOT District 6-0
Municipality
Matthew Edmond – Montgomery County (1 Set)
Susan Guisinger-Colon – McCormick Taylor
Shawn Sterling – Norfolk Southern Railway Company
Fixed Utilities
Matthew Hickson – Traffic Planning and Design, Inc.

**BEFORE
PENNSYLVANIA PUBLIC UTILITY COMMISSION**

In re: Application of Montgomery County for approval to reconstruct overhead crossing for Norfolk Southern Dale Secondary Line at Ridge Pike, Section MG4, (Montgomery County Bridge #0), in Plymouth Township, Montgomery County.

Application
Docket No.

To the Pennsylvania Public Utility Commission:

1. The name and address of applicant:

County of Montgomery
P.O. Box 311
Norristown, PA 19404-0311

2. The name and address of attorney for the applicant:

Brian O. Phillips, Esq.,
Montgomery County Assistant Solicitor
County of Montgomery
P.O. Box 311
Norristown, PA 19404-0311

3. Montgomery County desires to replace County Bridge #0 over Norfolk Southern on Ridge Pike in Plymouth Township, Montgomery County, PA. The intended purpose of the proposed bridge replacement and Ridge Pike improvements is to provide a consistent five lane cross section along Ridge Pike, adequate under-clearance for the bridge carrying Ridge Pike over Norfolk Southern, and to accommodate the proposed Pennsylvania Turnpike Commission's Lafayette Street interchange ramp infrastructure. The bridge reconstruction will provide multiple traffic and pedestrian benefits in the area and will provide accommodations for future business development and economic growth.
4. With PUC and Norfolk Southern approval, Montgomery County and its contractors shall cause to replace the Montgomery County Bridge #0, shown on the attached Structural Plan Set (56 Sheets). In coordination with Norfolk Southern and the Pennsylvania Department of Transportation, Montgomery County and its contractors, shall cause to replace County Bridge #0 with minimal interruption to traffic. No detour has been proposed for this project. The western section of the bridge will be constructed first while the existing traffic is maintained and shifted to the east side of the existing bridge. Once the western section of the bridge has been constructed, traffic will be shifted to the newly constructed western half of the bridge while the east side is being constructed.

5. The names of public utilities, municipalities, or corporations concerned with, or affected by, the proposed crossing, and alterations to existing utility crossing(s) related thereto, are listed immediately below (the contact information for each of these parties is shown in the Certificate of Service):

PECO ENERGY
VERIZON PENNSYLVANIA, LLC
ENERGY TRANSFER PARTNERS, LLC
ZAYO GROUP (FORMERLY PPL TELECOM, LLC)
PLYMOUTH TOWNSHIP, MONTGOMERY COUNTY
VERIZON BUSINESS (FORMERLY MCI)
COMCAST CABLE COMMUNICATIONS MANAGEMENT, LLC
LUMEN
CROWN CASTLE
WINDSTREAM COMMUNICATIONS (FORMERLY CAVALIER TELEPHONE)
PENNSYLVANIA DEPARTMENT OF TRANSPORTATION DISTRICT 6-0
PENNSYLVANIA TURNPIKE COMMISSION

6. It is anticipated that this section of Ridge Pike, where County Bridge #0 is located, will generate approximately 37,902 vehicles per day by 2046. This is based on the current Average Daily Traffic of 34,676 and a 0.28% growth per year. Additionally, other sections of Ridge Pike that will be reconstructed in the near future will provide sidewalk connectivity and additional pedestrian facilities along the corridor that potentially increase the usage of the proposed bridge. Other factors such as time of year, weather and local events may also impact the traffic and pedestrian volumes in this area.
7. The funding for this section of Ridge Pike, as shown on the plans, will be covered by 80% State and 20% County funds. The estimated construction cost of the bridge is \$20,800,000.00. The road is classified as Urban Principal Arterial.
8. The replacement of County Bridge #0 on Ridge Pike is necessary and proper for the safety and convenience of the general public and local businesses that will use the proposed County Bridge #0 and to the construction of the Lafayette Street Interchange along the Pennsylvania Turnpike.

Wherefore, applicant prays your Honorable Commission to approve the application:

Respectfully submitted,



Brian O. Phillips, Esq.,
Montgomery County Assistant Solicitor,
County of Montgomery

**RIDGE PIKE SECTION A
CERTIFICATE OF SERVICE (NOTARIZED)**

CERTIFICATE OF SERVICE

In re: Application of Montgomery County for approval to reconstruct overhead crossing for Norfolk Southern Dale Secondary Line at Ridge Pike, Section MG4, (Montgomery County Bridge #0), in Plymouth Township, Montgomery County.

Application
Docket No.

I hereby certify that I have this day served a true copy of the foregoing document upon the participants, listed below, in accordance with the requirements of § 1.54 (*relating to service by participant*).

Shawn Starling, PE
Norfolk Southern Corporation
650 West Peachtree Street NW – Box 45
Atlanta, GA 30308

Montgomery County
Montgomery County
P.O. Box 311
Norristown, PA 19404

Karen Weiss
Plymouth Township
700 Belvoir Road
Plymouth Meeting, PA 19462

Pennsylvania Department of Transportation
Right-of-Way & Utility Division
Bureau of Design – Project Delivery
P.O. Box 3362
Harrisburg, PA 17105-3362

Pennsylvania Department of Transportation
Office of Chief Counsel
P.O. Box 8212
Harrisburg, PA 17105-8212

Mary Ann Lang
Pennsylvania Department of Transportation
7000 Geerdes Boulevard
King of Prussia, PA 19406-1525

Erika Martino
Pennsylvania Turnpike Commission
700 South Eisenhower Blvd
Middletown, PA 17057

Darren Marsteller
Comcast Cable Communications Management, LLC
234 West Main Street, Suite 103
Leola, PA 17540

David Hayward
Crown Castle (Fiber)
3200 Horizon Drive, Suite 150
King of Prussia, PA 19406

Tim Armstrong
Crown Castle (Small Cell)
3200 Horizon Drive, Suite 150
King of Prussia, PA 19406

Richard L. Williams
Lumen (Formerly Level 3 Communications, LLC/Century Link)
5095 Ritter Rd, Suite 101
Mechanicsburg, PA 17055

Pete Dettling
PECO Energy Company (Electric)
1050 West Swedesford Road
Berwyn, PA 19312

Michael J. Gabriel
PECO Energy Company (Gas)
1050 West Swedesford Road
Berwyn, PA 19312

John L. Ruskay
Sunoco Pipeline LP (Energy Transfer Partners, LLC)
525 Fritztown Road
Sinking Spring, PA 19608

John Alessandrini
Verizon Business (Formerly MCI)
630 Clark Avenue
King of Prussia, PA 19406

Scott Nubbemeyer
Verizon Pennsylvania, LLC
1050 Virginia Drive
Fort Washington, PA 19304

Jason Woodyshek
Windstream Communications (Formerly Cavalier Telephone)
14 Corporate Cir, Suite 7
E Syracuse, NY 13057

Chris Ricciuti
Zayo Bandwidth (Formerly PPL Telecom, LLC/Abovenet)
170 Robbins Road
Downingtown, PA 19335



Bridget Briddes
Executive Paralegal
Solicitor's Office, Montgomery County
Dated this 2nd Day of May 2024

Commonwealth of Pennsylvania - Notary Seal
Bridget Wosczyzna-Briddes, Notary Public
Montgomery County
My commission expires October 24, 2025
Commission number 1232014
Member, Pennsylvania Association of Notaries

**EXISTING CONDITIONS AND PROJECT
PURPOSE AND NEED**

Existing Conditions

Ridge Pike, currently maintained by the Montgomery County Department of Road and Bridges, is located in Plymouth Township in Montgomery County, Pennsylvania. The project length is approximately 1.2 miles and extends from approximately 200 feet west of the intersection of Ridge Pike and School Lane to the intersection of Ridge Pike and Belvoir Road. From School Lane to Belvoir Road, Ridge Pike is a four-lane roadway. Narrow sidewalks are provided throughout the project in both the eastbound and westbound directions. The posted speed limit is 25 MPH from School Lane to the Pennsylvania Turnpike structure and 35 MPH from the Pennsylvania Turnpike structure to Belvoir Road. Due to safety concerns, it will be required to provide a center left-turn lane along this section of Ridge Pike, which will tie into the center left-turn improvements previously constructed to the west, and the center left-turn improvements under construction to the east.

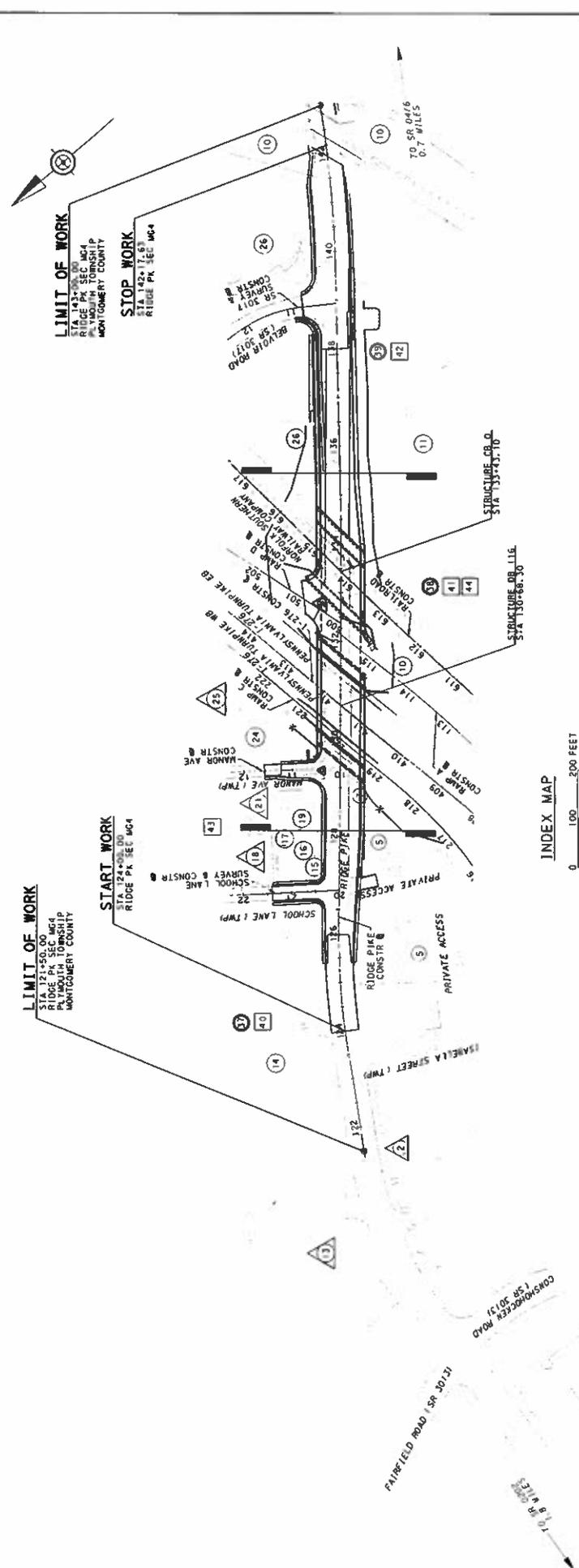
Project Purpose and Need

The purpose of the project is to provide adequate under clearance for the bridges carrying Ridge Pike over I-276 (Pennsylvania Turnpike) and Norfolk Southern and to accommodate the proposed I-276 Lafayette Street interchange ramp infrastructure, while improving congestion, safety and pedestrian facilities. The proposed roadway section between School Lane and Belvoir Road will consist of two 11-foot through lanes with an 11-foot center left-turn lane and two 13-foot curbed through lanes for a 59-foot typical section. The two structures will be further widened to provide a 13-foot westbound right turn lane to the future I-276 eastbound interchange on-ramp and over widen the structures to maintain two through lanes throughout construction. The project will require a maximum of 36 feet of widening mainly on the south side of the roadway and will require necessary right-of-way acquisitions to produce the most practical solution with the least adverse impact to the Ridge Pike corridor. Proposed construction also includes upgrades to the existing drainage and traffic safety features including guide rail, updated signage and pavement markings. Accommodations for a future traffic signal will be included at the intersection of Ridge Pike and future I-276 eastbound on/off ramps. The existing traffic signal at Belvoir Road will be upgraded and ADA compliance shall be met at the intersections where pedestrians have access. In addition to the roadway work, the Montgomery County Bridge No. 0 over Norfolk Southern - Dale Secondary Railroad Line (MV 30.51), near Station 133+43 will be replaced with this project. The bridge is currently maintained jointly by Montgomery County and the Norfolk Southern Corporation. In addition, the project scope will include the replacement of Pennsylvania Turnpike Bridge DB-116 and the construction of a retaining wall along eastbound Ridge Pike between County Bridge No. 0 and Belvoir Road.

LOCATION INDEX MAP

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-D	MONTGOMERY	104S	MC4	2 OF 44
REVISION	PL YOUTH TOWNSHIP			
DATE	BY			

LEGEND	PLAN	PROFILE	PARCEL IDENTIFICATION NUMBER	NO TAKE
○	□	○	○	△



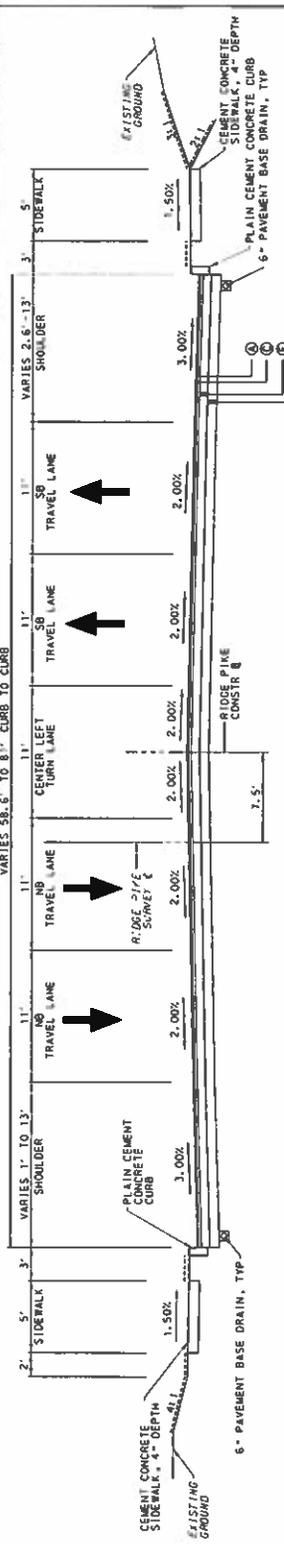
**DRAFT
NOT FOR
CONSTRUCTION
06/25/2024**

TITLE SHEET	DESCRIPTION	SHEET
INDEX MAP	INDEX MAP	1
LOCATION MAP AND GENERAL NOTES	LOCATION MAP AND GENERAL NOTES	2
SUMMARY OF PROJECT COORDINATES	SUMMARY OF PROJECT COORDINATES	3
GEOMETRY PLAN	GEOMETRY PLAN	4
TYPICAL SECTIONS	TYPICAL SECTIONS	5
INTERSECTION DETAILS	INTERSECTION DETAILS	6 TO 11
GRADING AND DRAINAGE PLANS	GRADING AND DRAINAGE PLANS	12 TO 16
CURB RAMP DETAILS	CURB RAMP DETAILS	17 TO 19
MISC. DETAILS	MISC. DETAILS	20 TO 35
SUMMARY OF QUANTITIES	SUMMARY OF QUANTITIES	36
SUBTOTAL OF QUANTITIES	SUBTOTAL OF QUANTITIES	180
PLAN AND PROFILE SHEETS	PLAN AND PROFILE SHEETS	37 TO 44

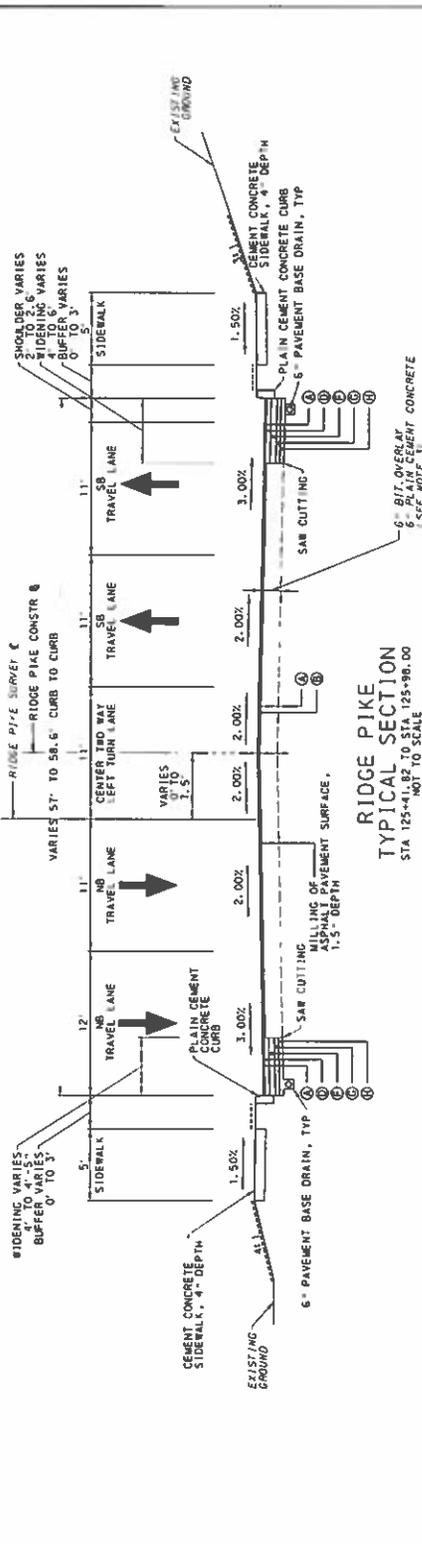
- PROPERTY OWNERS**
- ① FRANCIS P. ROMANO
 - ② ROMALD BATTAGLIA & SUSAN BATTAGLIA
 - ③ ENRICO DAGLILANTE
 - ④ BRIAN FLANNAM
 - ⑤ CARLA REID & JAMES JOHN
 - ⑥ ROBERT A. ROMANO & MARIAM D. ROMANO, M/W
 - ⑦ ROMALD B. WINTERBOTTOM AND WINTERBOTTOM PARTNERS, LP
 - ⑧ IGLESIA DE DIOS REMAER
 - ⑨ FRANCIS P. ROMANO
 - ⑩ ENRICO DAGLILANTE
 - ⑪ BRIAN FLANNAM
 - ⑫ CARLA REID & JAMES JOHN
 - ⑬ ROBERT A. ROMANO & MARIAM D. ROMANO, M/W
 - ⑭ ROMALD B. WINTERBOTTOM AND WINTERBOTTOM PARTNERS, LP
 - ⑮ IGLESIA DE DIOS REMAER
- REMSYLVANIA TURNPIKE COMMISSION**
- ⑯ PECO ENERGY COMPANY

TYPICAL ROADWAY SECTIONS

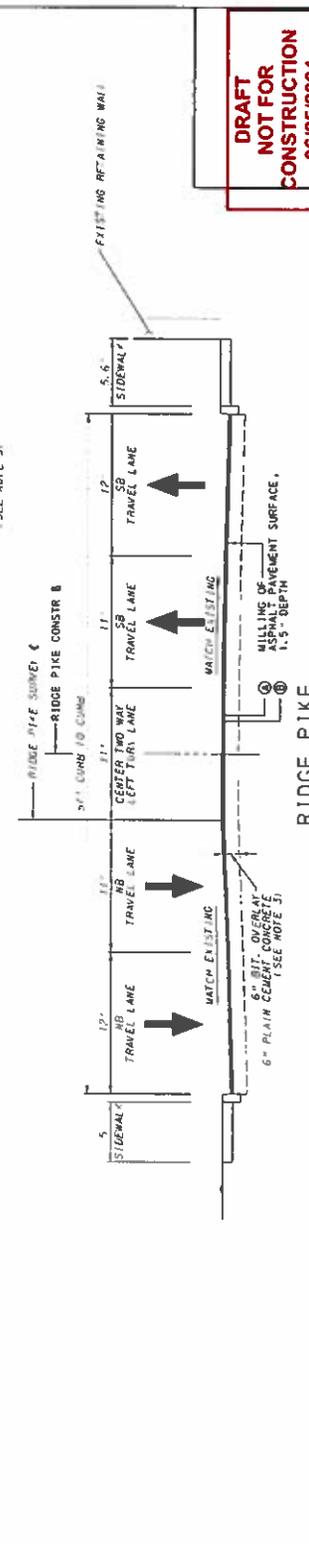
DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	PLYMOUTH	125	MC4	6 OF 41
REVISION	DATE	BY		



**RIDGE PIKE
TYPICAL SECTION**
STA 125+98.00 TO STA 125+50.00
NOT TO SCALE



**RIDGE PIKE
TYPICAL SECTION**
STA 125+41.02 TO STA 125+98.00
NOT TO SCALE

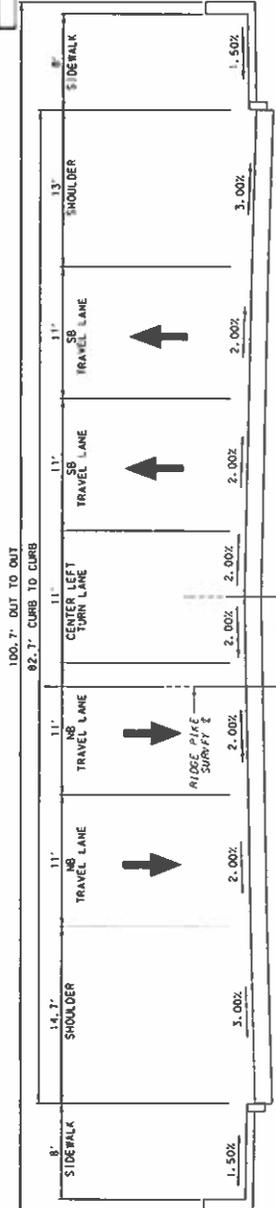


**RIDGE PIKE
TYPICAL SECTION**
STA 124+00 TO STA 123+41.02
NOT TO SCALE

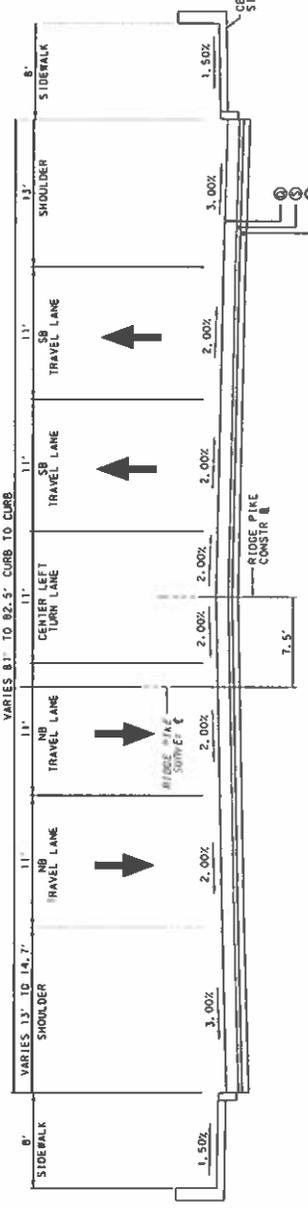
- LEGEND**
- SLOPE STEEPER THAN 3:1 FORMULA B
 - SLOPE STEEPER THAN 3:1 FORMULA C
- NOTES**
1. CURB SHALL BE 4" MAXIMUM WHEN APPLIED TO RC 5" M AND WHEN CONCRETE RATIO IS WITHIN EIGHT FEET OF THE CURB FACE.
 2. EXISTING PAVEMENT TYPES AND DEPTHS MATCH PAVEMENT DESIGN REPORT (SECTION M02).
 3. EXISTING PAVEMENT TYPES AND DEPTHS MATCH ECMS NO. 81392 (SECTION M01).
 4. SUPERPAVE ASPHALT MIXTURE DESIGN, WEARING COURSE, PG 645-22, 3 TO <10 MILLION ESALS, 5.5 MM MIX, 1.5" DEPTH, SRL-L.
 5. SUPERPAVE ASPHALT MIXTURE DESIGN, 3 TO 10 MILLION ESALS, 5.5 MM MIX, VARIABLE DEPTH, SRL-L.
 6. SUPERPAVE ASPHALT MIXTURE DESIGN, HMA BINDER COURSE, PG 645-22, 3 TO <10 MILLION ESALS, 15.0 MM MIX, 2.5" DEPTH.
 7. SUPERPAVE ASPHALT MIXTURE DESIGN, HMA BINDER COURSE, PG 645-22, 3 TO <10 MILLION ESALS, 25.0 MM MIX, 4.5" DEPTH.
 8. SUPERPAVE ASPHALT MIXTURE DESIGN, HMA BASE COURSE, PG 645-22, 1 TO <10 MILLION ESALS, 25.0 MM MIX, 8" DEPTH.
 9. PLAIN CEMENT CONCRETE PAVEMENT, 6" DEPTH.
 10. TREATED PERMEABLE BASE COURSE, 4" DEPTH.
 11. SUBBASE 4" DEPTH (NO. 2A).
 12. SUBBASE 9" DEPTH (NO. 2A).
 13. GEOTEXTILE, CLASS 4, TYPE A.

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CONSTRUCTION
06/25/2024**

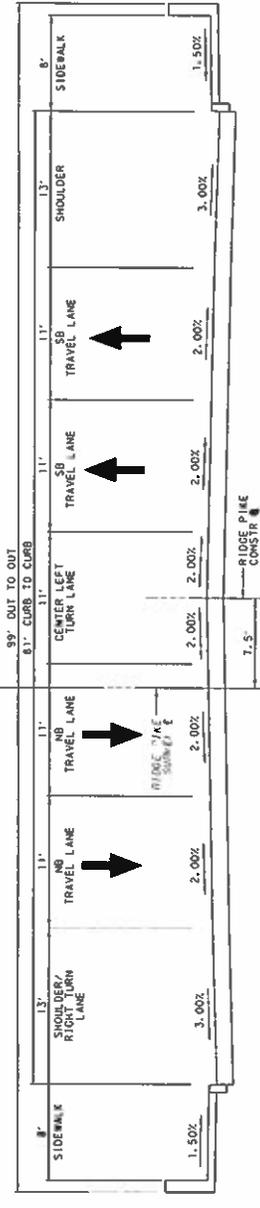
TYPICAL SECTIONS



**RIDGE PIKE BRIDGE OVER
 NORFOLK-SOUTHERN RAILROAD
 TYPICAL SECTION**
 STA 132+58.67 TO STA 133+26.75
 NOT TO SCALE



**RIDGE PIKE
 TYPICAL SECTION**
 STA 131+71.22 TO STA 132+58.67
 NOT TO SCALE



**RIDGE PIKE BRIDGE OVER PA TURNPIKE
 TYPICAL SECTION**
 STA 129+50.69 TO STA 131+71.22
 NOT TO SCALE

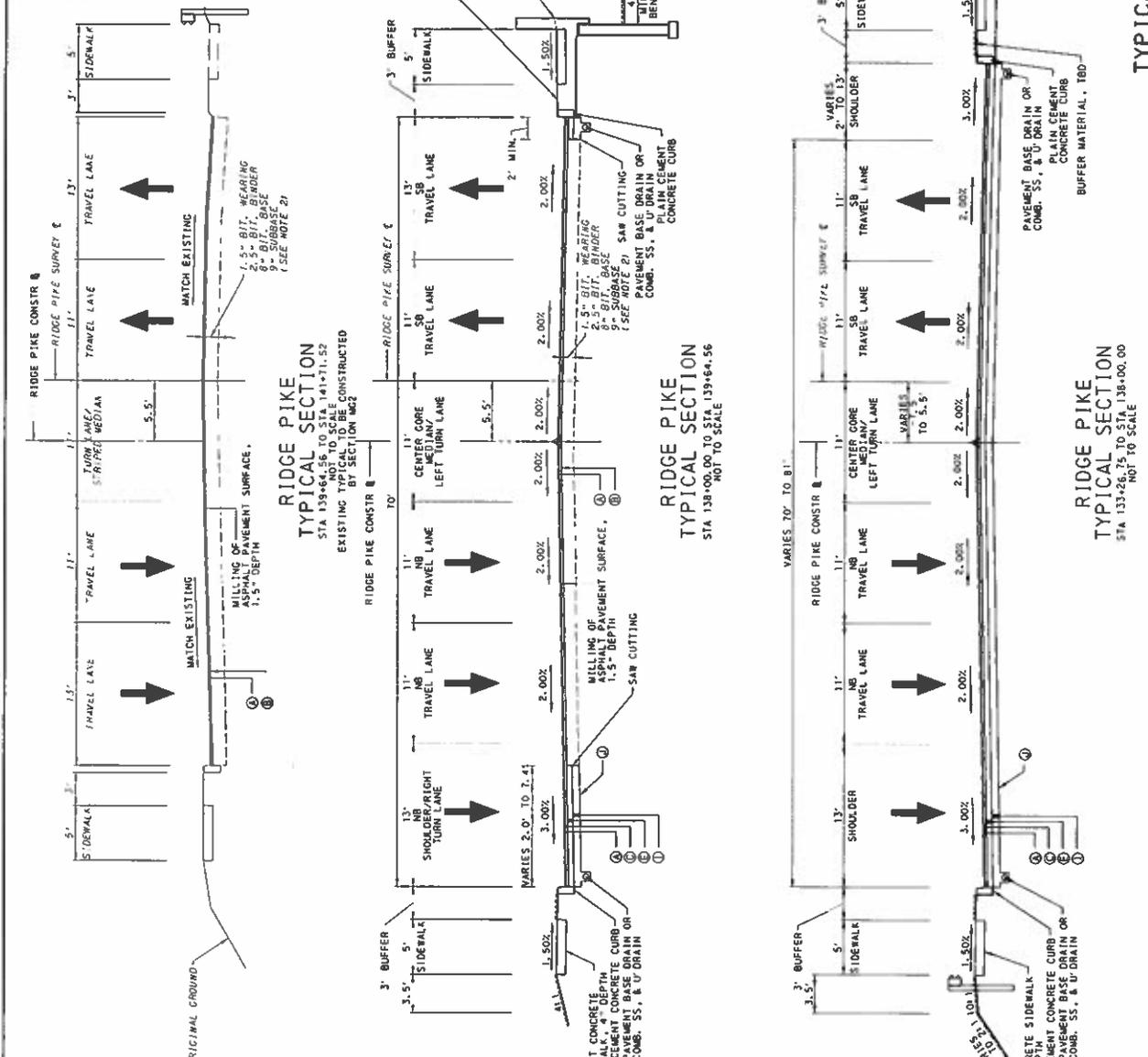
LEGEND
 - - - - - SLOPE 3:1 AND FLATTER FORMULA B
 - - - - - SLOPE STEEPER THAN 3:1 FORMULA C

- NOTES
- CURB SHALL BE 4" MAXIMUM WHEN ADJACENT TO GUIDE RAIL ACCORDING TO RC-51M AND WHEN GUIDE RAIL IS WITHIN EIGHT FEET OF THE CURB FACE.
 - EXISTING PAVEMENT TYPES AND DEPTHS MATCH PAVEMENT DESIGN REPORT (SECTION WC2)
 - EXISTING PAVEMENT TYPES AND DEPTHS MATCH EDMS NO. 87392 (SECTION WCL)
 - SUPERPAVE ASPHALT MIXTURE DESIGN, WEARING COURSE, 9.5 MM MIX, 1.5" DEPTH, SRL E
 - SUPERPAVE ASPHALT MIXTURE DESIGN, WEARING COURSE, 9.5 MM MIX, VARIABLE DEPTH, SRL-L
 - SUPERPAVE ASPHALT MIXTURE DESIGN, MMA BINDER COURSE, 14.0 MM MIX, 2.5" DEPTH, C10 MILLION ESALS, 25.0 MM MIX, 4.5" DEPTH, C10 MILLION ESALS
 - SUPERPAVE ASPHALT MIXTURE DESIGN, MMA BINDER COURSE, 14.0 MM MIX, 4.5" DEPTH, C10 MILLION ESALS
 - SUPERPAVE ASPHALT MIXTURE DESIGN, MMA BASE COURSE, PG 645-22, 3" TO C10 MILLION ESALS, 25.0 MM MIX, 8" DEPTH
 - PLAIN CEMENT CONCRETE PAVEMENT, 6" DEPTH
 - TREATED PERMEABLE BASE COURSE, 4" DEPTH
 - SUBBASE 4" DEPTH (NO. 2A)
 - SUBBASE 9" DEPTH (NO. 2A)
 - GEOTEXTILE, CLASS 4, TYPE A
 - PLAIN CEMENT CONCRETE PAVEMENT, 12" DEPTH
 - CONCRETE SHOULDER, TYPE 2 (6" DEPTH)
 - ASPHALT TREATED PERMEABLE BASE COURSE, 4" DEPTH
 - ASPHALT TREATED PERMEABLE BASE COURSE, 10" DEPTH
 - SUBBASE 6" DEPTH (NO. 2A)

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 NOT FOR
 CONSTRUCTION
 06/25/2024**

TYPICAL SECTIONS

DISTRICT	COUNTY	ROUTE SECTION	SHEET
6-0	MONTGOMERY	PA ROUTE 108	8 OF 44
REVISIONS	DATE	BY	



- LEGEND**
- SLOPE 3:1 AND FLATTER FORMULA B
 - SLOPE STEEPER THAN 3:1 FORMULA C
- NOTES**
- CURB SHALL BE 4" MAXIMUM WHEN CONCRETE TO GUIDE RAIL ACCORDING TO RC-51M AND BREN BUILD RAIL IS WITHIN EIGHT FEET OF THE CURB FACE.
 - EXISTING PAVEMENT TYPES AND DEPTHS MATCH PAVEMENT DESIGN REPORT (SECTION M02)
 - EXISTING PAVEMENT TYPES AND DEPTHS MATCH ECMS NO. 87392 (SECTION M03)
 - SUPERPAVE ASPHALT MIXTURE DESIGN, HEARING COURSE, PG 645-22, 4" DEPTH, SR-E, 15.0 MM MIX, 1.5" DEPTH, SR-E
 - SUPERPAVE ASPHALT MIXTURE DESIGN, HEARING COURSE, PG 645-22, 4" DEPTH, SR-E, 15.0 MM MIX, 1.5" DEPTH, SR-E
 - SUPERPAVE ASPHALT MIXTURE DESIGN, NMA BASE COURSE, PG 645-22, 3" TO 4" DEPTH, SR-L, 25.0 MM MIX, 8" DEPTH
 - SUPERPAVE ASPHALT MIXTURE DESIGN, NMA BINDER COURSE, PG 645-22, 1" DEPTH, SR-L, 25.0 MM MIX, 8" DEPTH
 - PLAIN CEMENT CONCRETE PAVEMENT, 6" DEPTH
 - TREATED PERMEABLE BASE COURSE, 4" DEPTH
 - CONCRETE 4" DEPTH (NO. 2A)
 - CONCRETE 9" DEPTH (NO. 2A)
 - GEOTEXTILE, CLASS 4, TYPE A

**DRAFT
NOT FOR
CONSTRUCTION
06/25/2024**

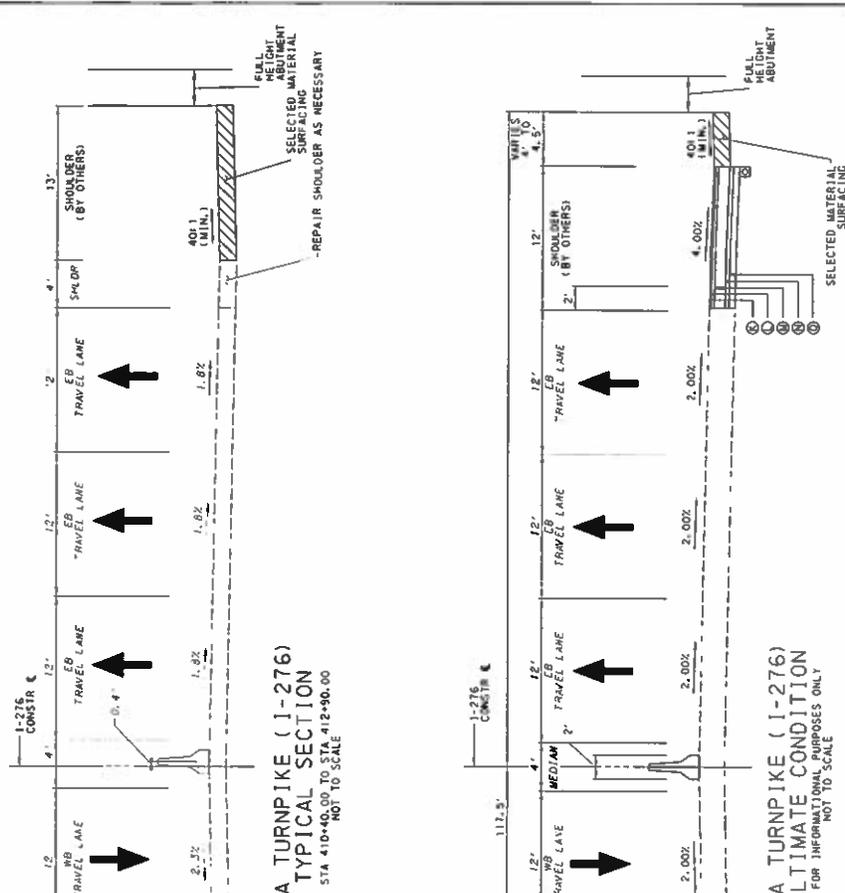
TYPICAL SECTIONS

**RIDGE PIKE
TYPICAL SECTION
STA 133+26.75 TO STA 139+00.00
NOT TO SCALE**

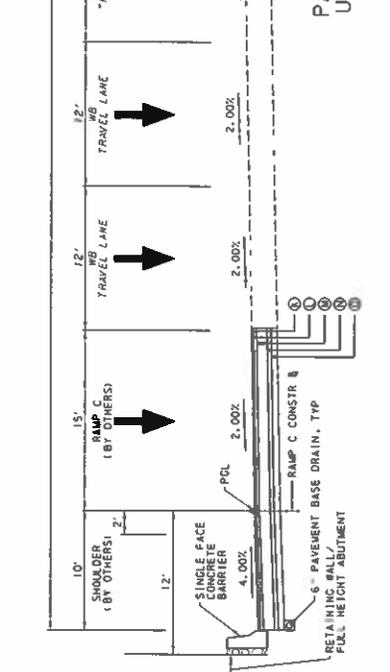
**RIDGE PIKE
TYPICAL SECTION
STA 138+00.00 TO STA 139+64.56
NOT TO SCALE**

**RIDGE PIKE
TYPICAL SECTION
STA 139+64.56 TO STA 141+11.52
EXISTING TYPICAL TO BE CONSTRUCTED
BY SECTION M02**

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	MONTGOMERY	244	MC-4	1 OF 44
PROJECT NUMBER	PLYMOUTH TOWNSHIP		REVISIONS	DATE
				BY



PA TURNPIKE (I-276)
TYPICAL SECTION
STA 410+40.00 TO STA. 412+90.00
NOT TO SCALE



PA TURNPIKE (I-276)
ULTIMATE CONDITION
FOR INFORMATIONAL PURPOSES ONLY
NOT TO SCALE

**DRAFT
NOT FOR
CONSTRUCTION
06/25/2024**

TYPICAL SECTIONS

LEGEND
SLOPE 3:1 AND FLATTER FORMULA B
SLOPE STEEPER THAN 3:1 FORMULA C

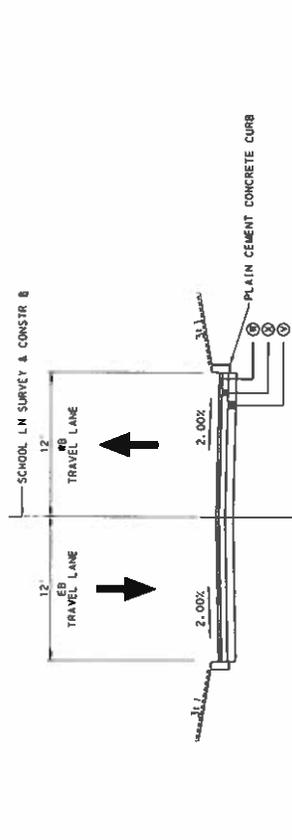
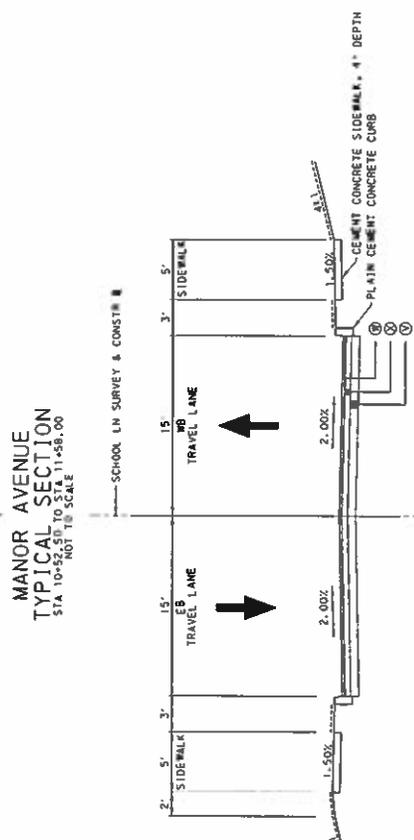
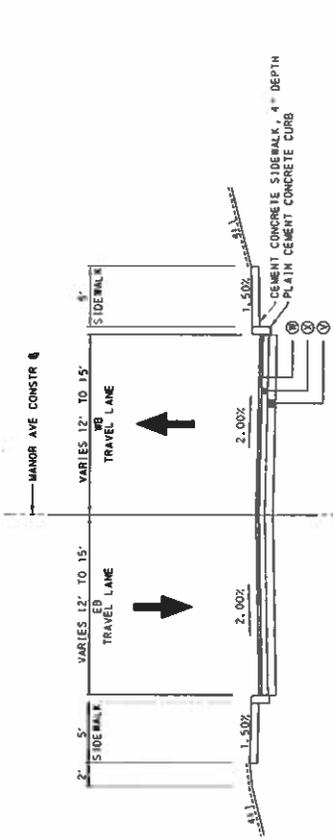
- NOTES
- PROPOSED PAVEMENT SECTION BASED ON EXISTING AS-BUILT PLANS FOR SR 3013, EXCEPT WHERE SHOWN OTHERWISE. ALL EXISTING UTILITIES AND OBSTRUCTIONS TO BE CONFIRMED IN FINAL DESIGN.
 - ROUND CUT SLOPES IN ACCORDANCE WITH RC-10M OR AS DIRECTED.
 - PAVEMENT DESIGN TO BE COMPLETED BY PTC FOR I-276, AND RAMP'S A, C & D.

- SUPERPAVE ASPHALT MIXTURE DESIGN, HMA BEARING COURSE, 155' DEPTH TO 0.10 MILLION ESALS, 12.5% BINDER MIX, 7.5' DEPTH, 5% SURF COURSE
- SUPERPAVE ASPHALT MIXTURE DESIGN, HMA BEARING COURSE, PG 645-20, 155' DEPTH TO 0.10 MILLION ESALS, 15.0% BINDER MIX, 5' DEPTH
- BITUMINOUS CONCRETE BASE COURSE, 11" DEPTH
- ASPHALT TREATED PERMEABLE BASE COURSE, 4" DEPTH
- SUBBASE 6" DEPTH (NO. 2A)
- BITUMINOUS CONCRETE BASE COURSE, 4" DEPTH

SHEET NO.	COUNTY	ROUTE	SECTION	SHEET
60	MONTGOMERY	106	MC4	
REVISION	DATE	BY		

LEGEND
 ----- SLOPE STEEPER THAN 3:1 FORMULA B
 ----- SLOPE STEEPER THAN 3:1 FORMULA C

- NOTES
- CURB SHALL BE 4" MAXIMUM WHEN EXISTING CURB IS NOT WITHIN 8" OF THE CURE FACE. IS WITHIN EIGHT FEET OF THE CURB FACE.
 - EXISTING PAVEMENT TYPES AND DEPTHS. MATCH PAVEMENT DESIGN REPORT (SECTION M02).
 - EXISTING PAVEMENT TYPES AND DEPTHS. MATCH ECMS NO. 81332 (SECTION M01).
- ① SUBBASE 6" DEPTH (NO. 2A)
 ② SUPERPAVE ASPHALT MIXTURE DESIGN, HMA WEARING COURSE, PC 645-22, 3 TO 6 MILLION ESALS, 5" MM MIX, 1.5" DEPTH, SRL-1
 ③ SUPERPAVE ASPHALT MIXTURE DESIGN, HMA BASE COURSE, PC 645-22, 3 TO 6 MILLION ESALS, 25.0 MM MIX, 8" DEPTH



**DRAFT
 NOT FOR
 CONSTRUCTION
 06/25/2024**

TYPICAL SECTIONS

HIGHWAY PLAN AND PROFILE SHEETS

DISTRICT COUNTY ROUTE SECTION SHEET
 6-0 HORTON P. NORTH TOWNSHIP REVISIONS DATE BY

PROPOSED STRUCTURE DATA
 TYPE: SINGLE SPAN ADJACENT BOX BEAM
 SPAN LENGTH: 112'-0" (112'-0" LCL RAIL)
 LATERAL CLEARANCE: 23'-0" (BT LCL RAIL)
 BRIDGE WIDTH: 82'-0" (1/2" CURB TO CURB)
 BRIDGE WIDTH: 100'-0" (1/2" OUT-TO-OUT)

PROPOSED STRUCTURE DATA
 TYPE: SINGLE SPAN COMPOSITE STEEL PLATE GIRDER
 SPAN LENGTH: 161'-0" (161'-0" LCL RAIL)
 LATERAL CLEARANCE: 25'-0" (BT LCL RAIL)
 BRIDGE WIDTH: 81'-0" (1/2" CURB TO CURB)
 BRIDGE WIDTH: 99'-0" (1/2" OUT-TO-OUT)

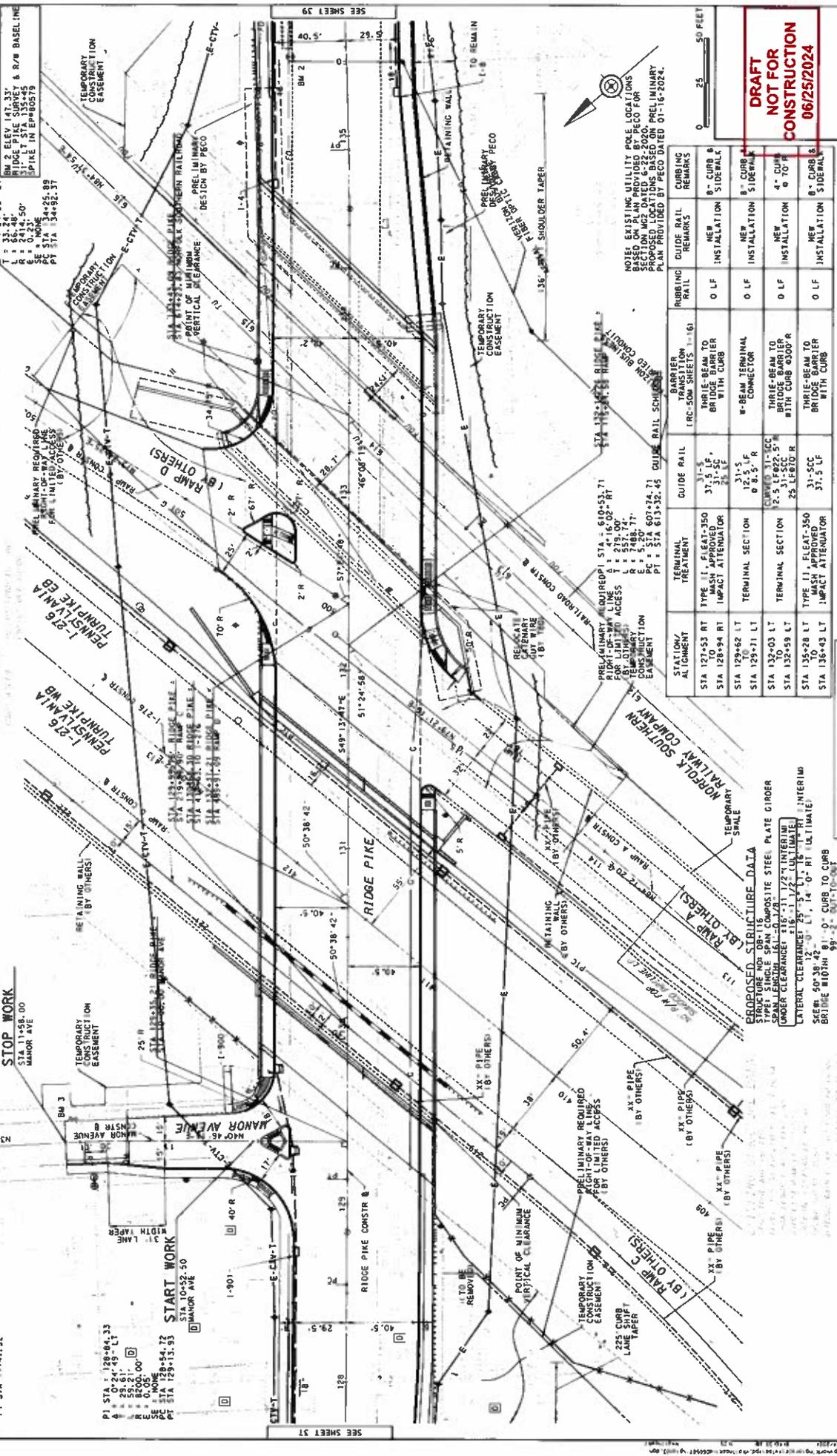
PROPOSED STRUCTURE DATA
 TYPE: SINGLE SPAN COMPOSITE STEEL PLATE GIRDER
 SPAN LENGTH: 161'-0" (161'-0" LCL RAIL)
 LATERAL CLEARANCE: 25'-0" (BT LCL RAIL)
 BRIDGE WIDTH: 81'-0" (1/2" CURB TO CURB)
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SEE SHEET 31 SEE SHEET 39

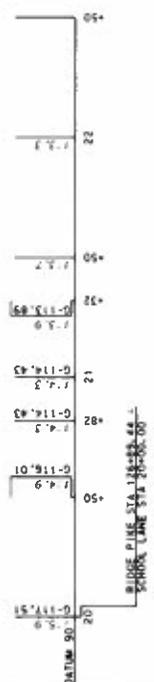
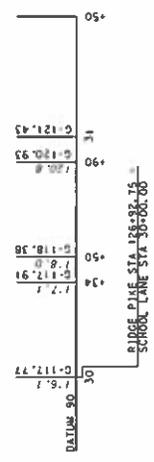
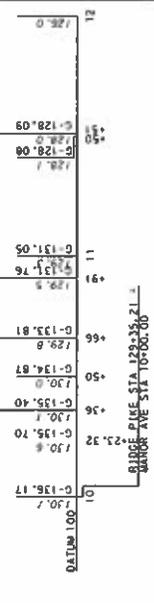
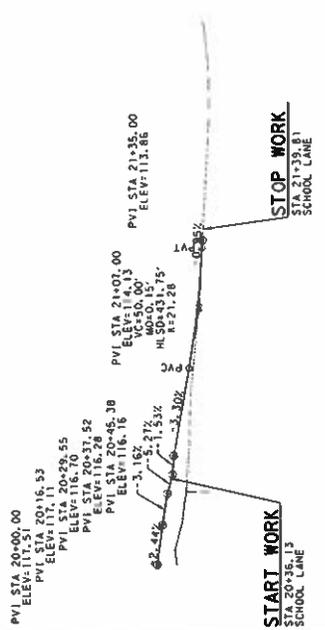
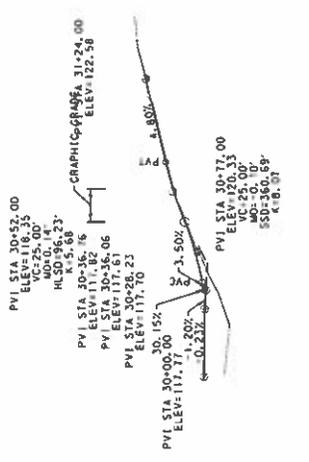
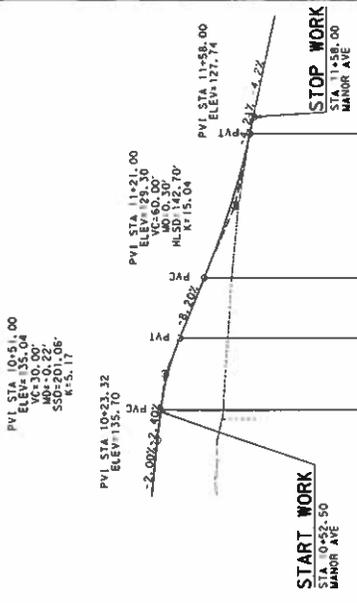
NOTE: EXISTING UTILITY POLE LOCATIONS BASED ON PLAN PROVIDED BY PECC FOR SECTION M62 DATED 6-22-2020. PRELIMINARY PLAN PROVIDED BY PECC DATED 01-16-2024.

STATION ALIGNMENT	TERMINAL TREATMENT	GUIDE RAIL	BARRIER TRANSITION (RC-50M SHEETS 1-1-16)	RUBBING RAIL	CURBING REMARKS
STA 127+53 RT	TYPE II, FLEAT-350 IMPACT ATTENUATOR	37.5 LF	THREE-BEAM TO BRIDGE BARRIER WITH CURB	0 LF	NEW 8" CURB & SIDEWALK INSTALLATION
STA 128+94 RT	TERMINAL SECTION	21.5 LF	W-BEAM TERMINAL CONNECTOR	0 LF	NEW 8" CURB & SIDEWALK INSTALLATION
STA 129+28 LT	TERMINAL SECTION	22.5 LF	THREE-BEAM TO BRIDGE BARRIER WITH CURB	0 LF	NEW 8" CURB & SIDEWALK INSTALLATION
STA 132+59 LT	TYPE II, FLEAT-350 IMPACT ATTENUATOR	31.5 LF	THREE-BEAM TO BRIDGE BARRIER WITH CURB	0 LF	NEW 8" CURB & SIDEWALK INSTALLATION

DRAFT FOR CONSTRUCTION
 06/25/2024

FOR PROFILE, SEE SHEETS 41, 43 AND 44

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	MONTGOMERY	1046	MC4	43 OF 44
TOWNSHIP	PILGRIM TOWNSHIP			
REVISIONS	DATE BY			



MANOR AVENUE

PRIVATE ACCESS

SCHOOL LANE LT

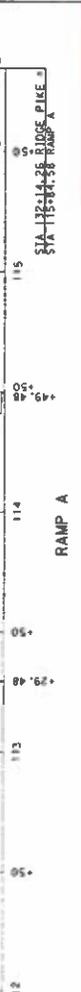
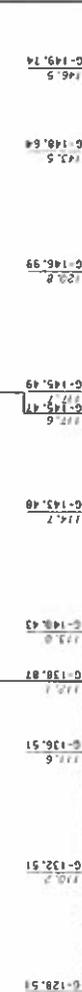
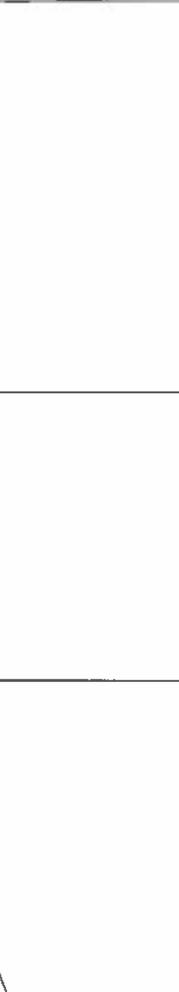
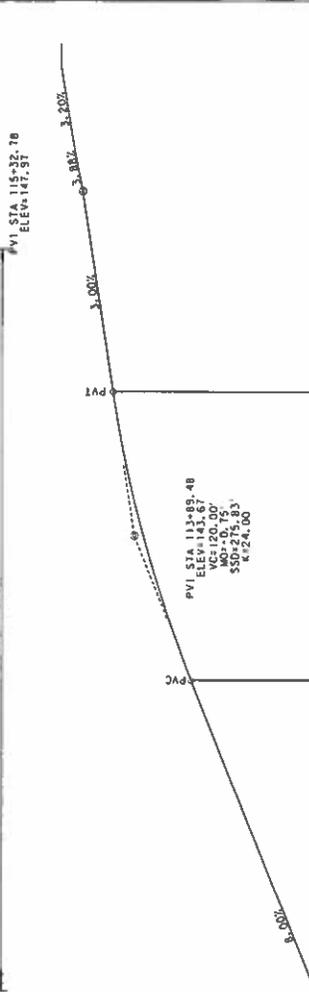


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06/25/2024**

FOR PLAN, SEE SHEET 37 AND 38

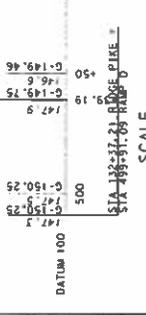
DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	MONTGOMERY	104	MC4	44 OF 44
TOWNSHIP	REVISIONS		DATE	BY
PLUMOUTH				

BY OTHERS
 PVI STA 115+32.76
 ELEV=147.31



**DRAFT
 NOT FOR
 CONSTRUCTION
 06/25/2024**

FOR PLAN, SEE SHEET 37



PVI STA 503+44.54
 ELEV=148.15
 VC=360.00'
 MLSD=176.95'
 K=30.72
 PVI=503+44.54
 VLS=10.00%
 VLS2=1.82%

PVI STA 500+68.19
 ELEV=148.15
 VC=60.00'
 MLSD=164.80'
 K=500.98
 PVI=500+68.19
 VLS=10.00%

BY OTHERS
 51+56

**RIDGE PIKE OVER NORFOLK SOUTHERN
CB #0 - TS&L DRAWINGS**

RIDGE PIKE

HORIZONTAL ALIGNMENT DATA

TANGENT AT S 49°13'47" E
 PI STA 13458.13
 PC STA 13455.89
 PT STA 13452.37
 P + 33.24'
 L + 66.48'
 E + 214.50'
 SE + NONE

RIDGE PIKE

VERTICAL ALIGNMENT DATA

PVI STA 13193.00
 ELEV + 555.79
 MD + 16.00'
 550 + 351.64'
 +1.60% @ -2.00%

NORFOLK SOUTHERN RR

VERTICAL ALIGNMENT DATA

PVI STA 61491.74
 ELEV + 500.00
 MD + 0.14'
 NSLO + 8106.91'
 -0.17% @ -0.35%

THE ELEVATIONS OF THE EXISTING TOP-OF-RAIL PROFILE SHALL BE VERIFIED BEFORE BEGINNING CONSTRUCTION. ALL DISTURBANCES SHALL BE SOUTHERN PUBLIC IMPROVEMENT ENGINEER.

EXISTING STRUCTURE DATA

STA + 33+50.00
 TYPE + STEEL THROUGHER
 L + 10'-0"
 CLR ABOVE + 49'-0" (NORMAL)
 MIN VERT CLR + 22'-0" TO TOP OF RAIL
 SFRM + 48'-17'-48"

GENERAL NOTES

DESIGN SPECIFICATIONS 2012 AND AS SUPPLEMENTED BY DESIGN MANUAL, PART 4, DECEMBER 2013 EDITION.
 LIVE LOAD DISTRIBUTION TO BEAMS IS BASED UPON PENNDOT DESIGN MANUAL, PART 4, DISTRIBUTION FACTORS.
 DESIGN IS IN ACCORDANCE WITH THE LRFD METHOD.
 DESIGN LIVE LOADS: PHL-93, P-42 (20K PERM LOAD) AND P2015-13.
DEAD LOADS
 INCLUDES 30 LBS PER SQ FT FOR FUTURE WEARING SURFACE ON THE DECK.

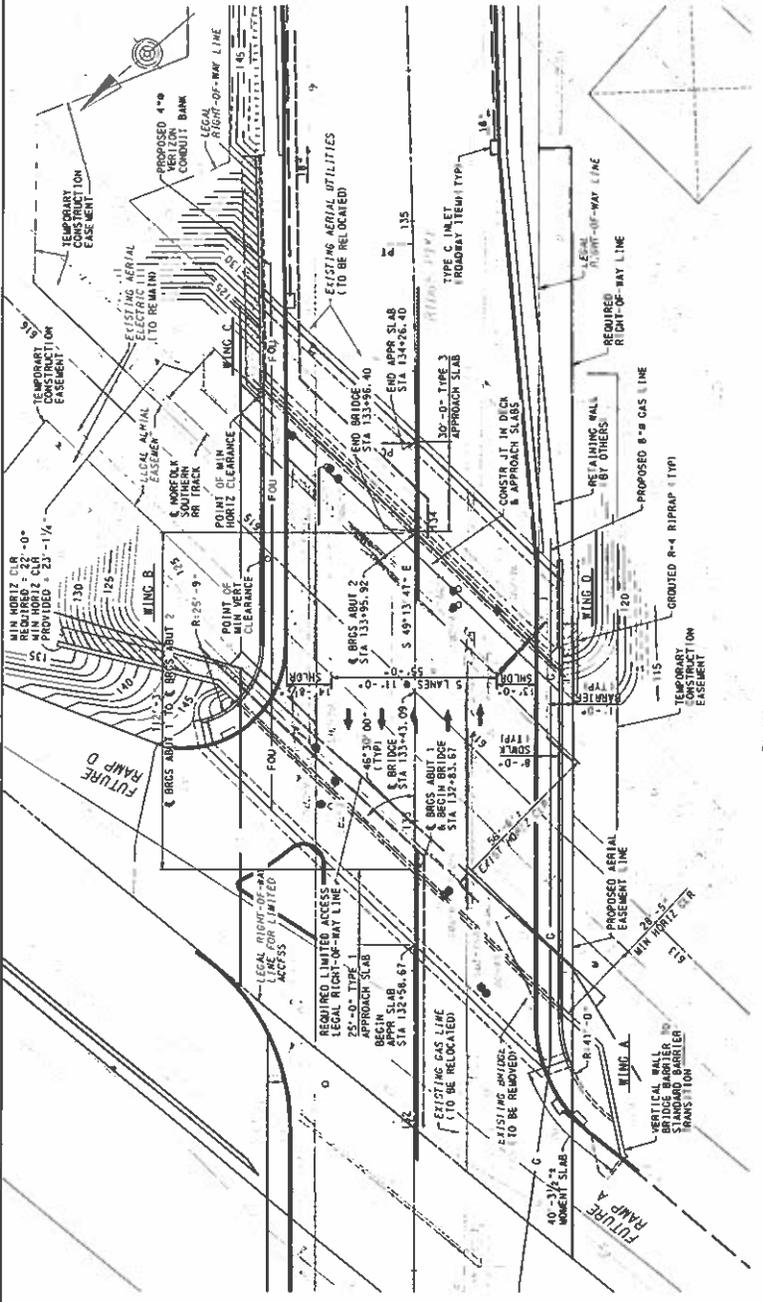
AS-DRILLED BORING LOCATIONS

BORING	STATION	OFFSET
582-1	134+28.32	40.6' L
582-2	134+14.03	26.5' L
582-3	134+11.28	28.1' L
582-4	133+11.13	12.7' R
582-5	133+16.63	12.7' R
582-6	132+15.33	9.9' R
582-7	132+16.83	11.4' R
582-8	133+13.14	26.2' L
582-9	133+05.53	30.8' L
582-10	132+24.38	32.5' L
582-15	132+42.63	24.1' R
582-15A	132+46.83	22.3' R
582-16	133+10.13	21.7' R

WINNALL LENGTHS

WING	LENGTH
A	55'-0"
B	64'-0"
C	72'-0"
D	29'-0"

WINNALL LENGTHS ARE APPROXIMATE



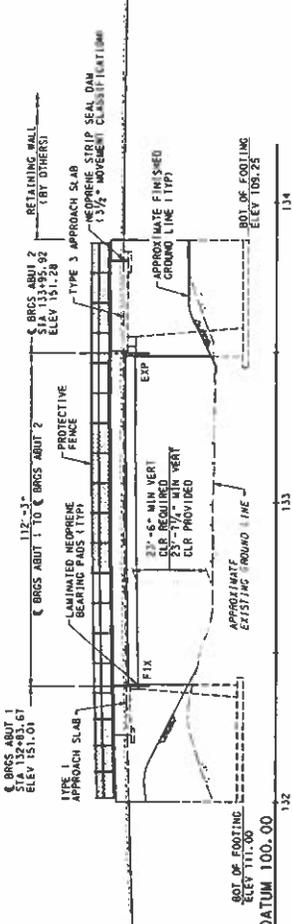
LEGEND

- TEMPORARY EXCAVATION SUPPORT AND PROTECTION SYSTEM (STAGE 1)
- TEMPORARY EXCAVATION SUPPORT AND PROTECTION SYSTEM (STAGE 2)
- DIRECTION OF TRAFFIC
- PROPOSED CONTOURS
- EXISTING CONTOURS
- EXISTING UTILITY POLE
- AS-DRILLED BORING LOCATIONS

NOTES

1. FOR RAILROAD CLEARANCE SKETCH, SEE SHEET 4.

PLAN



ELEVATION



MONTGOMERY COUNTY
 DEPARTMENT OF ASSETS AND INFRASTRUCTURE
RIDGE PIKE SECTION M64
 RIDGE PIKE STA 133+43.09
 OVER NORFOLK SOUTHERN RAILROAD (MPWV-30.51)
 SINGLE SPAN P/S COMP ADJACENT BOX BEAM BRIDGE
 TYPE, SIZE & LOCATION - 1

REVISIONS

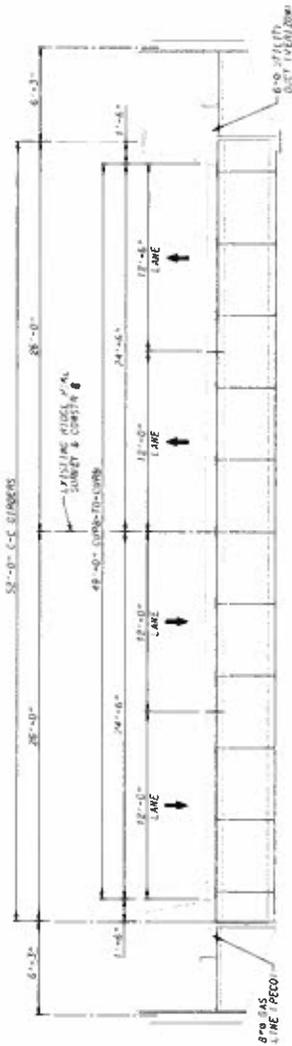
Mark	Description	By	CHK'd	Revised	Date

BMS STR ID# 46-7046-0100-0000 MPWV/EDC PROJ# 110444 BARETY 28031

PREPARED BY
KCI TECHNOLOGIES, INC
 2000 W. STATE ST., SUITE 201
 MECHANICSBURG, PA 17055
 TEL# (717) 851-1340

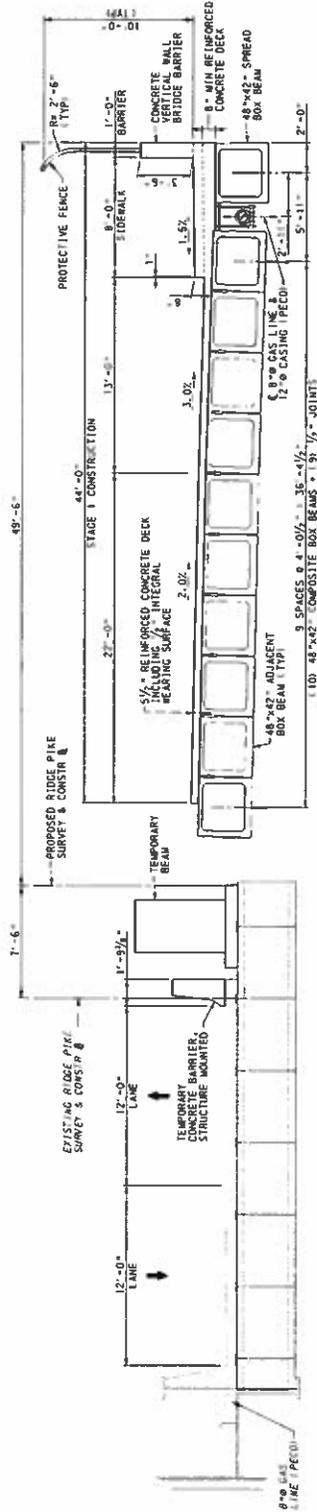
PROFESSIONAL ENGINEER
 DATE 09-02-2022

SHEET 1 OF 4
 CB #0



EXISTING TYPICAL SECTION

LOOKING AHEAD STATION
SCALE: 1/4" = 1'-0"



STAGE 1 CONSTRUCTION

LOOKING AHEAD STATION
SCALE: 1/4" = 1'-0"

Work	Description	By	CHK'd	Rec'd	Date

REVISIONS
BUS. STR. ID# 46-7046-0100-0000 MPM/EGDK PROJ# 110444 BRIEF# 28031

MONTGOMERY COUNTY
DEPARTMENT OF ASSETS AND INFRASTRUCTURE
MONTGOMERY COUNTY
RIDGE PIKE SECTION MG4
RIDGE PIKE STA 133+43.09
OVER NORFOLK SOUTHERN RAILROAD (NPMV-30.51)
SINGLE SPAN P/S COMP ADJACENT BOX BEAM BRIDGE
TYPE, SIZE & LOCATION - 2

**PRELIMINARY
NOT FOR
CONSTRUCTION**

RECOMMENDED
SHEET 2 OF 4
CB #0

PREPARED BY
ACT TECHNOLOGIES

DESIGN: MAR | DRAWN: STC | CHECK: MAR

MCB No 0
(SIGNED TS&L APPROVAL LETTER)



December 9, 2022

Montgomery County
County Bridge #0 Replacement over Norfolk Southern Railroad
Montgomery County Bridge #0
T. S. & L. Approval

Matthew I. Hickson, P.E.
Traffic Planning and Design, Inc.
2500 East High Street, Suite 650
Pottstown, PA 19464

Dear Mr. Hickson,

Reference is made to the submission dated September 2022 from the design consultant KCI Technologies, Inc. requesting our review and approval of the T. S. & L. for the subject structure. We have reviewed the submission in coordination with the NTM Engineering, Inc. consultant reviewer and approve the T. S. & L. as follows:

LOCATION:

Ridge Pike, Section MG4, over Norfolk Southern Railroad in Plymouth Township, Montgomery County, PA

TYPE:

Replace existing single span girder-floorbeam-stringer bridge with a single span adjacent box beam bridge. A minimum 5.5" reinforced concrete slab will be provided except where an 8" minimum slab is required over fascia beams and utility bays. These thicknesses include a ½" integral wearing surface.

SPAN:

One (1) span at 112'-3"

SKEW:

46°30'00" Left Ahead

CLEARANCES:

23'-6" minimum vertical clearance to be provided

STRUCTURE WIDTH:

Curb-to curb width of the roadway varies from 82'-8 1/2" at the east end to 93'-3 1/8" at the west end.

Out-to-out width of the structure varies from 100'-8 1/2" on the east end to 108'-9 1/2" on the west end which includes two 1'-0" concrete wall barriers, and 8'-0" sidewalks on both sides.

DECK PROTECTIVE SYSTEM:

Epoxy coated reinforcement bars and Class AAAP Cement Concrete in deck slab.

SUBSTRUCTURE PROTECTIVE SYSTEM:

Use epoxy coated reinforcement and a breathable sealant as per the requirements in DM-4M. Place waterproofing at the abutment stem footing construction joint in accordance with BC-788M.

DECK JOINTS: N/A

BEARING TYPE & LOCATION:

Laminated Elastomeric Bearing Pads:
Fixed (Abutment 1) & Expansion (Abutment 2)

DRAINAGE:

Scuppers will not be provided on the structure. Off-structure drainage along both approaches to the structure is adequate.

DESIGN METHODOLOGY:

Use Load and Resistance Factor Design (LRFD) Methodology

DESIGN LIVE LOAD:

PHL-93, P-82 (204 k Permit Load), or P2016-13 (230 k Permit Load)

TEST BORINGS:

Thirteen (13) test borings were performed in 2006.

OTHER COMMENTS:

The final plans review and approval for this structure is delegated to the design consultant.

We provide the following comments for incorporation in the Final Plans;

- 1) The bridge plan approval number is L-443.
- 2) T. S. & L. approval is contingent upon approval of the CEE reports and obtaining required permits.
- 3) Substructure design should not begin until foundation submission has been approved.
- 4) Permit a contractor alternate for the subject structure and outline any restrictions in Part B of the Special Provisions for the alternate structure.
- 5) Type 1 Near and Type 3 Far Approach Slabs must be incorporated in the Final Design.
- 6) Constructed bridge using staged construction, maintaining one lane of traffic in each direction, and a sidewalk for pedestrians in each stage.
- 7) Provide a consultant reviewer block on the first sheet of the final design construction plans in accordance with DM-4M (pp) Section 1.3.4 criteria.
- 8) All affected utilities and bridge attachments must be coordinated with the District Utility Unit per DM-4M PP 3.5 to assure compliance and license requirements.

If you have any questions or comments concerning this matter, please contact Alexa Harper, PE of Gannett Fleming at 610-783-3783.

Sincerely,



dabazi
2022.12.09
13:15:32-05'00'

Din Abazi, P.E.
District Bridge Engineer

Montgomery County
County Bridge #0 over Norfolk Southern Railroad
T. S. & L. Approval
Page 4
December 9, 2022

cc: A. Harper
S. Badane
K. Langer

ROW PLAN

RECORDER OF DEEDS
MONTGOMERY COUNTY PENNSYLVANIA

Jeanne Sorg

One Montgomery Plaza
Swede and Airy Streets ~ Suite 303
P.O. Box 311 ~ Norristown, PA 19404
Office: (610) 278-3289 ~ Fax (610) 278-3869

**I hereby certify that the following is a true and correct
copy of the original document
recorded in Montgomery County, PA**



Jeanne Sorg
Jeanne Sorg, Recorder of Deeds

DATE	12/20/2023	TIME	12:07:01 PM
PROJECT	MONTICO PLANT		
CLIENT	MONTICO PLANT		
LOCATION	MONTICO PLANT		
SCALE	AS SHOWN		
DRAWN BY	[Name]		
CHECKED BY	[Name]		
DATE	[Date]		

DATE	12/20/2023	TIME	12:07:01 PM
PROJECT	MONTICO PLANT		
CLIENT	MONTICO PLANT		
LOCATION	MONTICO PLANT		
SCALE	AS SHOWN		
DRAWN BY	[Name]		
CHECKED BY	[Name]		
DATE	[Date]		

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LOCATION	MONTICO PLANT		
SCALE	AS SHOWN		
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DATE	[Date]		

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CLIENT	MONTICO PLANT		
LOCATION	MONTICO PLANT		
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CLIENT	MONTICO PLANT		
LOCATION	MONTICO PLANT		
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CLIENT	MONTICO PLANT		
LOCATION	MONTICO PLANT		
SCALE	AS SHOWN		
DRAWN BY	[Name]		
CHECKED BY	[Name]		
DATE	[Date]		

SUMMARY OF PROJECT COORDINATES
 BASED ON PENNSYLVANIA STATE PLANE
 COORDINATE SYSTEM (SPCS 8311 SOUTH ZONE)

STATION	POINT	NORTH	EAST	BEARING
121-37.49	POT	291,359,3049	2,649,453,3701	S59°21'39"E
123-63.78	PC	291,241,1982	2,649,046,6290	S59°21'39"E
124-93.56	P1	291,175,7462	2,649,052,9590	S48°48'57"E
126-74.16	P1	291,093,5940	2,649,246,7646	S48°48'57"E
128-54.72	PC	290,954,2552	2,649,426,8681	S49°13'47"E
130-35.33	P1	290,814,7577	2,649,451,1403	S49°13'47"E
132-15.94	P1	290,675,7535	2,649,473,3703	S49°13'47"E
134-02.37	P1	290,535,9538	2,649,481,2884	S50°48'26"E
135-49.62	PC	290,395,9882	2,649,485,4657	S50°48'26"E
137-36.62	P1	290,255,9882	2,650,271,1885	S72°50'21"E
139-24.62	P1	289,922,9479	2,650,642,5001	S72°50'21"E
141-12.62	P1	289,589,4076	2,651,013,4417	S72°50'21"E
143-00.62	POT	291,043,0915	2,649,304,4739	N37°52'24"E
144-88.62	POT	291,222,6241	2,649,444,1025	N37°52'24"E
146-76.62	PC	290,974,3464	2,649,565,7185	N40°46'13"E
148-64.62	P1	290,993,7245	2,649,571,8051	N37°55'59"E
150-52.62	P1	290,993,2814	2,649,585,4174	N37°55'59"E
152-40.62	P1	290,987,5883	2,649,656,8539	S58°21'59"E
154-28.62	P1	290,981,0478	2,649,692,5777	S58°21'59"E
156-16.62	P1	290,974,8261	2,649,742,8089	S49°13'47"E
158-04.62	P1	290,968,5418	2,650,223,6214	S49°13'47"E
160-00.00	POT	290,979,0055	2,650,606,7261	S72°50'21"E
162-00.00	POT	290,877,1625	2,649,494,7503	N37°55'59"E
164-00.00	POT	291,070,0888	2,649,649,0148	N37°55'59"E

NOTE: FOUR (4) PLACE COORDINATES ARE USED FOR COMPUTATIONAL PURPOSES ONLY AND DO NOT IMPLY A PRECISION BEYOND TWO (2) PLACES.
 AVERAGE COMBINED SCALE FACTOR 0.9999768041.

TABLATION OF REQUIRED RIGHT-OF-WAY BREAK POINT COORDINATES
 BASED ON PENNSYLVANIA STATE PLANE
 COORDINATE SYSTEM (SPCS 8311 SOUTH ZONE)

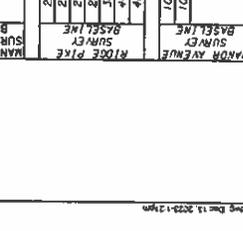
RTE	STATION	OFF-SET	NORTH	EAST
125+06	35.00' RT	291,133,2000	2,649,142,6517	
126+27.00	35.00' RT	291,120,5517	2,649,139,1421	
126+75.00	45.00' RT	291,018,7308	2,649,263,9773	
127+45.00	54.00' RT	290,985,8638	2,649,310,7328	
128+54.72	54.00' RT	290,833,6149	2,649,330,3041	
128+95	54.00' RT	290,667,1004	2,649,423,7624	
131+54	53.00' RT	290,698,4080	2,649,620,8956	
132+35.00	52.00' RT	290,643,0089	2,649,685,1433	
133+35	44' RT	290,647,7413	2,649,733,4509	
133+27	53.00' RT	290,596,8336	2,649,751,6669	
135+23.00	54.00' RT	290,457,1809	2,649,901,8448	
135+23.00	43' RT	290,455,4931	2,649,906,6223	
135+98	45' RT	290,419,1548	2,649,962,3618	
137+88.00	51.00' RT	290,265,7246	2,650,116,9723	
138+49.00	36' RT	290,158,8665	2,650,171,3823	
138+49.00	36' RT	290,158,8665	2,650,171,3823	
137+14.00	36.00' LT	291,054,0164	2,649,346,6565	
137+14.00	36.00' LT	291,055,2994	2,649,356,5428	
137+09.77	133.55' LT	291,130,7203	2,649,406,8303	
137+09.77	141.90' LT	291,005,5689	2,649,583,1342	
128+58.59	67.26' LT	290,917,1990	2,649,553,3476	
128+58.59	67.26' LT	290,936,6179	2,649,553,3476	
129+82	53.00' LT	290,890,8265	2,649,560,0641	
132+45	56.00' LT	290,723,0262	2,649,762,3223	
133+45.00	58.00' LT	290,657,8515	2,649,837,9068	
133+21	32' LT	290,651,4383	2,649,802,7565	
134+21.00	33' LT	290,588,9867	2,649,878,8779	
134+46.00	58.00' LT	290,592,2735	2,649,914,0043	
134+72	51.00' LT	290,575,6594	2,649,933,8391	
135+31	51.00' LT	290,432,6759	2,650,098,0559	
136+26.85	51.00' LT	290,347,3358	2,650,201,9842	
139+21.81	82.00' LT	290,395,8629	2,650,246,0855	
139+64	86.00' LT	290,350,4883	2,650,294,2412	
20+35	22' RT	291,087,0519	2,649,143,7096	
20+35	27' RT	291,054,0104	2,649,143,7096	
20+50	27' RT	291,065,2994	2,649,356,8498	
21+32	27' RT	291,130,7203	2,649,406,8303	
21+32.08	22.00' RT	291,133,8487	2,649,402,9258	
10+67.26	16.67' LT	290,943,3523	2,649,620,8916	
10+67	22' LT	290,946,6214	2,649,517,1903	
11+43	25' LT	291,005,5689	2,649,563,1342	
11+42.90	15.98' LT	291,002,4435	2,649,567,0376	

NOTE: FOUR (4) PLACE COORDINATES ARE USED FOR COMPUTATIONAL PURPOSES ONLY AND DO NOT IMPLY A PRECISION BEYOND TWO (2) PLACES.
 AVERAGE COMBINED SCALE FACTOR 0.9999768041.

AERIAL EASEMENT NOTES

WHERE AN AERIAL EASEMENT IS ACQUIRED, IT SHALL INCLUDE AN EASEMENT IN THE AIR FOR THE STRUCTURE TO BE ACCOMMODATED BY THE SURFACE EASEMENT UNLIMITED IN VERTICAL DIMENSION ABOVE THE ACCOMMODATION OF PIERS AND INCLUDING THE EASEMENT OF MAINTENANCE AND REPAIR OF THE STRUCTURE AND THE FOLLOWING LIMITATIONS SHALL BE IMPOSED ON THE PROPERTY BENEATH THE AREA AFFECTED BY THE AERIAL EASEMENT.

- NO USE SHALL BE MADE OF THE PROPERTY WHICH SHALL ENHANCE THE STRUCTURE OR THE HEALTH, SAFETY OR WELFARE OF THE TRAVELING PUBLIC.
- NO FLAMMABLE, EXPLOSIVE, DANGEROUS OR HAZARDOUS MATERIAL SHALL BE USED, PLACED OR STORED ON THE PROPERTY.
- NO BUILDING OR OTHER FACILITY SHALL BE CONSTRUCTED ON THE PROPERTY WITHOUT PRIOR AUTHORITY OF THE COUNTY. IF AND WHEN SUCH AUTHORITY IS GRANTED, THE BUILDING OR FACILITY AND CONSTRUCTION METHODS SHALL BE SUBJECT TO THE APPROVAL OF THE COUNTY.
- NO INTERFERENCE SHALL BE MADE WITH THE RIGHT OF THE COUNTY TO ENTER THE PROPERTY FOR THE PURPOSES OF INSPECTION, MAINTENANCE, REPAIR, PAINTING, RECONSTRUCTION OR REMOVAL OF THE STRUCTURE OR FACILITY DURING SOME OR ALL OF THE ABOVE OPERATIONS.
- ANY SUBSTANTIAL CHANGE IN PROPERTY USE TO BE MADE SUBSEQUENT TO THE ACQUISITION OF THE EASEMENT SHALL BE SUBJECT TO THE APPROVAL OF THE COUNTY.
- THE NOTES ON THESE DRAWINGS SHALL NOT BE CONSTRUED AS LIMITING OR INTERFERING IN ANY WAY WITH THE PRESENT AND FUTURE USE, MAINTENANCE, REPAIR, REHAIR, CHANGE, ADDITION, BETTERMENT OR ALTERATION OF THE RAILROAD AND ITS SUPPORTING FACILITIES.



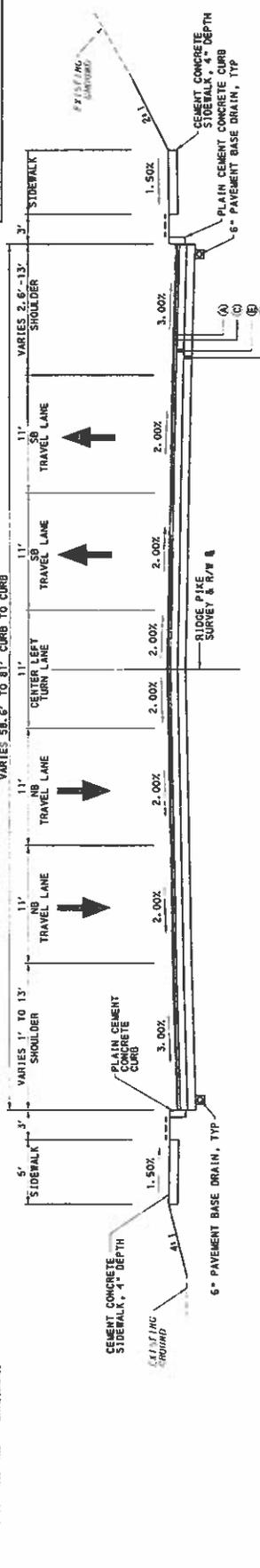
LEGAL R/W LINE
 SURVEY R/W P/R
 RIDGE PIKE
 AERIAL EASEMENT
 AERIAL EASEMENT LINE

PRELIMINARY STATE TO BE ACQUIRED FOR AN AERIAL EASEMENT FROM RIDGE PIKE SURVEY & P/R BASELINE FROM STA 132+35.00 TO STA 134+46.00. USE OF AIRSPACE BENEATH THE ESTABLISHED GRADELINE OF THE HIGHWAY SHALL PROVIDE SUFFICIENT VERTICAL CLEARANCE FOR THE VERIFICATION AND SAFETY OF THE HIGHWAY FACILITY. THE EASEMENT REQUIRED FOR THE ESTABLISHMENT OF THE HIGHWAY SHALL BE ENTERED ON BY MOVING VEHICLES SUCH AS TRUCKS OR RAILROAD ROLLING STOCK.

DATE	12/20/2023	TIME	12:07:01 PM
PROJECT	MONTICO PLANT		
CLIENT	MONTICO PLANT		
LOCATION	MONTICO PLANT		
SCALE	AS SHOWN		
DRAWN BY	[Name]		
CHECKED BY	[Name]		
DATE	[Date]		

PLN BK 0060-00454
 PROJECT: 1200002, UO 01 PA
 DATE: 12/20/2023
 DRAWING: 00454

PROJECT NO.	1200002
PROJECT NAME	FLYWOOD TOWNSHIP
DATE	12/20/2023
SCALE	AS SHOWN
BY	MM
CHECKED	MM
APPROVED	MM

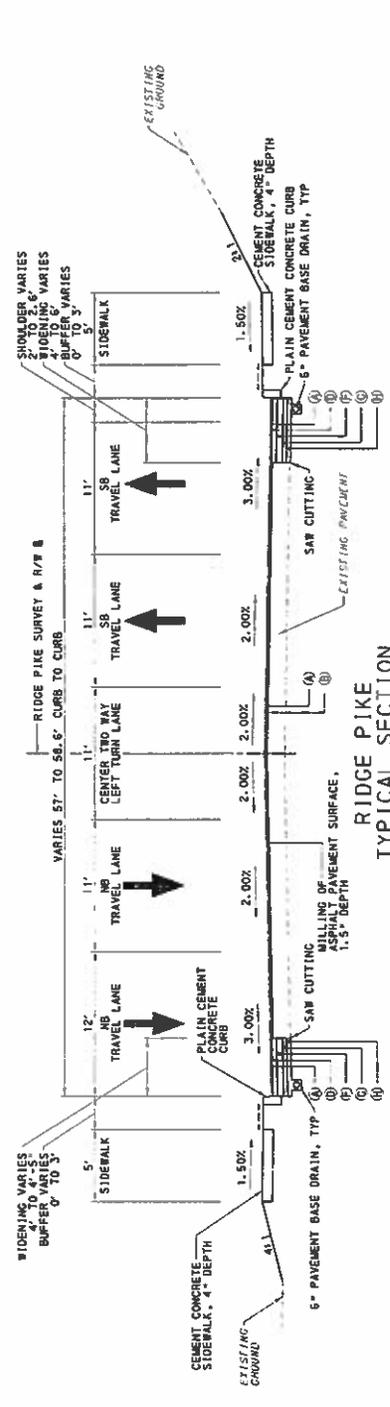


**RIDGE PIKE
 TYPICAL SECTION**
 STA 125+96.00 TO STA 125+14.00
 NOT TO SCALE

LEGEND:
 --- SLOPE 3:1 AND FLATTER FORMULA B
 --- SLOPE STEEPER THAN 3:1 FORMULA C

NOTES:

1. CURB SHALL BE 4" MAXIMUM WHEN ADJACENT TO GUIDE RAIL ACCORDING TO RC-SIM AND WHEN GUIDE RAIL IS WITHIN EIGHT FEET OF THE CURB FACE.
2. EXISTING PAVEMENT TYPES AND DEPTHS MATCH PAVEMENT DESIGN REPORT (SECTION M2)
3. EXISTING PAVEMENT TYPES AND DEPTHS MATCH ECHS NO. 87392 (SECTION M2)
4. SUPERPAVE ASPHALT MIXTURE DESIGN (WEARING COURSE) PC 845-22, 3 TO 4" (10" MILL) ON ESALS, 5.0 MM MIX, 1.5" DEPTH, SILE FILL (ON ESALS, 25.0 MM MIX, 8" DEPTH)
5. SUPERPAVE ASPHALT MIXTURE DESIGN (SUBBASE) PC 845-22, 3 TO 4" (10" MILL) ON ESALS, 5.0 MM MIX, 1.5" DEPTH, SILE FILL (ON ESALS, 25.0 MM MIX, 8" DEPTH)
6. SUPERPAVE ASPHALT MIXTURE DESIGN (WMA BINDER COURSE) PC 845-22, 3 TO 4" (10" MILL) ON ESALS, 5.0 MM MIX, 1.5" DEPTH, SILE FILL (ON ESALS, 25.0 MM MIX, 8" DEPTH)
7. SUPERPAVE ASPHALT MIXTURE DESIGN (WMA BASE COURSE) PC 845-22, 3 TO 4" (10" MILL) ON ESALS, 5.0 MM MIX, 1.5" DEPTH, SILE FILL (ON ESALS, 25.0 MM MIX, 8" DEPTH)
8. PLAIN CEMENT CONCRETE PAVEMENT, 6" DEPTH
9. TREATED PERMEABLE BASE COURSE, 4" DEPTH
10. SUBBASE 4" DEPTH (NO. 2A)
11. SUBBASE 9" DEPTH (NO. 2A)
12. GEOTEXTILE, CLASS 4, TYPE A



**RIDGE PIKE
 TYPICAL SECTION**
 STA 125+00.00 TO STA 125+98.00
 NOT TO SCALE

LEGEND:
 --- SLOPE 3:1 AND FLATTER FORMULA B
 --- SLOPE STEEPER THAN 3:1 FORMULA C

NOTES:

1. CURB SHALL BE 4" MAXIMUM WHEN ADJACENT TO GUIDE RAIL ACCORDING TO RC-SIM AND WHEN GUIDE RAIL IS WITHIN EIGHT FEET OF THE CURB FACE.
2. EXISTING PAVEMENT TYPES AND DEPTHS MATCH PAVEMENT DESIGN REPORT (SECTION M2)
3. EXISTING PAVEMENT TYPES AND DEPTHS MATCH ECHS NO. 87392 (SECTION M2)
4. SUPERPAVE ASPHALT MIXTURE DESIGN (WEARING COURSE) PC 845-22, 3 TO 4" (10" MILL) ON ESALS, 5.0 MM MIX, 1.5" DEPTH, SILE FILL (ON ESALS, 25.0 MM MIX, 8" DEPTH)
5. SUPERPAVE ASPHALT MIXTURE DESIGN (SUBBASE) PC 845-22, 3 TO 4" (10" MILL) ON ESALS, 5.0 MM MIX, 1.5" DEPTH, SILE FILL (ON ESALS, 25.0 MM MIX, 8" DEPTH)
6. SUPERPAVE ASPHALT MIXTURE DESIGN (WMA BINDER COURSE) PC 845-22, 3 TO 4" (10" MILL) ON ESALS, 5.0 MM MIX, 1.5" DEPTH, SILE FILL (ON ESALS, 25.0 MM MIX, 8" DEPTH)
7. SUPERPAVE ASPHALT MIXTURE DESIGN (WMA BASE COURSE) PC 845-22, 3 TO 4" (10" MILL) ON ESALS, 5.0 MM MIX, 1.5" DEPTH, SILE FILL (ON ESALS, 25.0 MM MIX, 8" DEPTH)
8. PLAIN CEMENT CONCRETE PAVEMENT, 6" DEPTH
9. TREATED PERMEABLE BASE COURSE, 4" DEPTH
10. SUBBASE 4" DEPTH (NO. 2A)
11. SUBBASE 9" DEPTH (NO. 2A)
12. GEOTEXTILE, CLASS 4, TYPE A



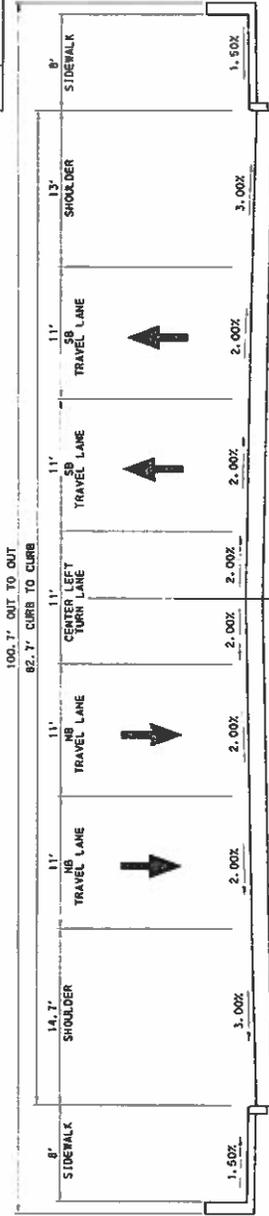
TYPICAL SECTIONS

**RIDGE PIKE
 TYPICAL SECTION**
 STA 124+00.00 TO STA 125+09.70
 NOT TO SCALE

00-454

DISTRICT	COURT	JUDGE	SEC
6-0	MONTGOMERY	TOAB	LM4
MONTGOMERY COUNTY		PL. SOUTH TOWNSHIP	
SECTION	DATE		

FILE: BK 0060 - 00455 - 12/20/2023 12:07:01 PM
 PROJECT: MONTGOMERY COUNTY PL. SOUTH TOWNSHIP
 DRAWING: PLN BK 0060 PG 00455
 MONTGOMERY COUNTY

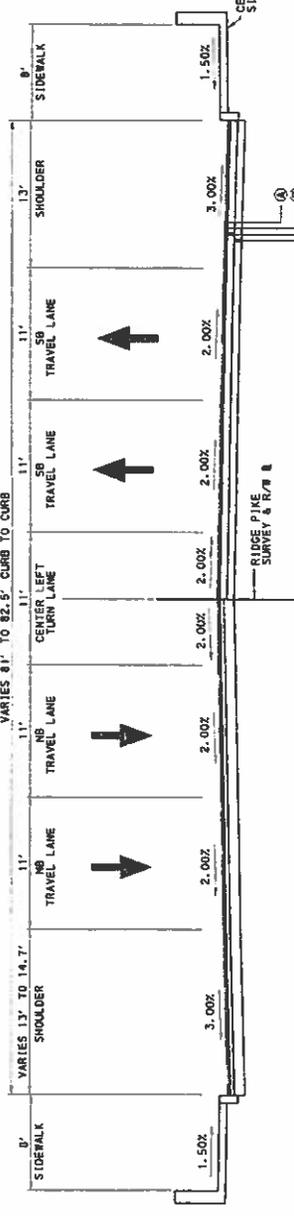


**RIDGE PIKE BRIDGE OVER
 NORFOLK-SOUTHERN RAILROAD
 TYPICAL SECTION**
 STA 132+21.75 TO STA 133+86.50
 VARIES 8'1" TO 82.5' CURB TO CURB
 RIDGE PIKE SURVEY & R/R

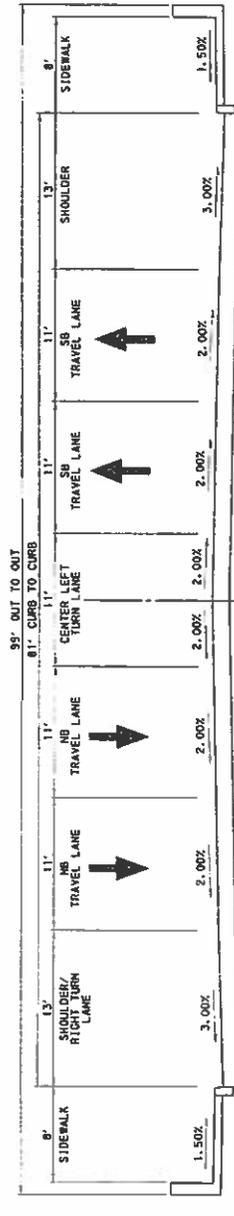
LEGEND
 --- SLOPE 3:1 AND FLATTER FORMULA B
 --- SLOPE STEEPER THAN 3:1 FORMULA C

NOTES

- CURB SHALL BE 4" MAXIMUM WHEN CONCRETE. WHEN NOT CONCRETE, CURB SHALL BE 6" MAXIMUM. CURB SHALL BE WITHIN EIGHT FEET OF THE CURB FACE.
- EXISTING PAVEMENT TYPES AND DEPTHS MATCH PAVEMENT DESIGN REPORT (SECTION 402)
- EXISTING PAVEMENT TYPES AND DEPTHS MATCH EGAS NO. 87192 (SECTION 401)
- SUPERPAVE ASPHALT MIXTURE DESIGN, WEARING COURSE, PG 645-22, 3 TO 10 MILLION ESALS, 9.5 MM MIX, 11.5" DEPTH, SRL-E
- SUPERPAVE ASPHALT MIXTURE DESIGN, WEARING COURSE (LEVELING), PG 645-22, 3 TO 10 MILLION ESALS, 9.5 MM MIX, 11.5" DEPTH, SRL-L
- SUPERPAVE ASPHALT MIXTURE DESIGN, HMA BINDER COURSE, PG 645-22, 3 TO 10 MILLION ESALS, 19.0 MM MIX, 2.5" DEPTH
- SUPERPAVE ASPHALT MIXTURE DESIGN, HMA BINDER COURSE, PG 645-22, 3 TO 10 MILLION ESALS, 25.0 MM MIX, 4.5" DEPTH
- SUPERPAVE ASPHALT MIXTURE DESIGN, HMA BASE COURSE, PG 645-22, 3 TO 10 MILLION ESALS, 25.0 MM MIX, 5" DEPTH
- PLAIN CEMENT CONCRETE PAVEMENT, 6" DEPTH
- TREATED PERMEABLE BASE COURSE, 4" DEPTH
- SUBBASE 4" DEPTH (MO. 2A)
- SUBBASE 9" DEPTH (MO. 2A)
- GEOTEXTILE, CLASS 4, TYPE A



**RIDGE PIKE
 TYPICAL SECTION**
 STA 131+95.6 TO STA 132+41.75
 VARIES 8'1" TO 82.5' CURB TO CURB
 RIDGE PIKE SURVEY & R/R



**RIDGE PIKE BRIDGE OVER PA TURNPIKE
 TYPICAL SECTION**
 STA 129+46.00 TO STA 131+95.61
 VARIES 8'1" TO 82.5' CURB TO CURB
 RIDGE PIKE SURVEY & R/R

TYPICAL SECTIONS

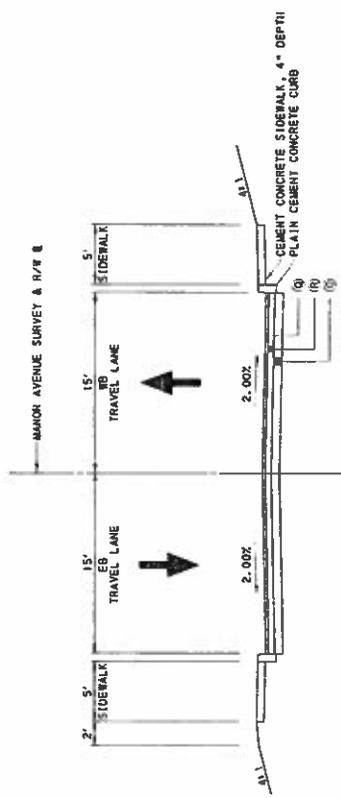


PROJECT NO.	6-0	COUNTY	MONTGOMERY	TOWNSHIP	TOAG	SHEET NO.	104
PROJECT NAME	PLYWOOD SHIP						
DATE	08/11/2023						
DESIGNER	MONTGOMERY COUNTY PUBLIC WORKS						
CHECKER	MONTGOMERY COUNTY PUBLIC WORKS						
APPROVED	MONTGOMERY COUNTY PUBLIC WORKS						

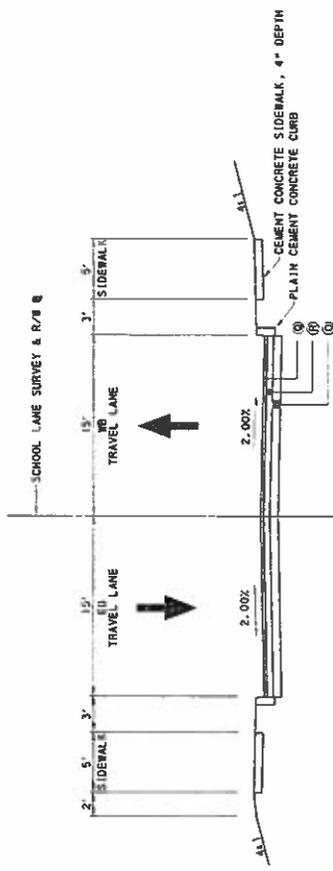


LEGEND
 --- SLOPE STEEPER THAN 3:1 FORMULA A
 --- SLOPE FLATTER FORMULA B

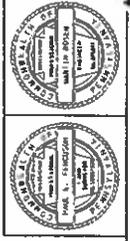
- NOTES
- CURB SHALL BE 4" MAXIMUM WHEN ADJACENT TO GUIDE RAIL ACCORDING TO RC-51M AND WHEN GUIDE RAIL IS WITHIN EIGHT FEET OF THE CURB FACE.
 - EXISTING PAVEMENT TYPES AND DEPTHS MATCH PAVEMENT DESIGN REPORT (SECTION M02).
 - EXISTING PAVEMENT TYPES AND DEPTHS MATCH EGAS NO. 87392 (SECTION M01).
 - ① SUBBASE 6" DEPTH (NO. 2A)
 - ② SUPERPAVE ASPHALT MIXTURE DESIGN, HMA WEARING COURSE, PG 64S-22, 3" TO 1.0 MILLION ESAL'S, 5.5" MIN MIX, 1.5" DEPTH, SBL-L
 - ③ SUPERPAVE ASPHALT MIXTURE DESIGN, SUPERPAVE COURSE, PG 64S-22, 3" TO 1.0 MILLION ESAL'S, 5.5" MIN MIX, 6" DEPTH



MANOR AVENUE
 TYPICAL SECTION
 STA 10+00.00 TO STA 11+57.91
 NOT TO SCALE



SCHOOL LANE
 TYPICAL SECTION
 STA 20+00.00 TO STA 22+27.44
 NOT TO SCALE



TYPICAL SECTIONS

400-457

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PLN BK 0060 PG 00461

MONTCO

EXISTING STRUCTURE DATA
 STA: 131+50.00
 TYPE: 3 SPAN I BEAM
 SPAN: 30'-0" (60'-0" NORMAL)
 SKEW: 45°-30'-45" (TO TOP OF RAIL)
 CLEAR ROADWAY WIDTH: 49'-0"
 CLEAR ROADWAY WIDTH (CURB TO CURB): 49'-0"

PROPOSED STRUCTURE DATA
 TYPE: STEEL PLATE GIRDER
 SPAN: 161'-0" (178'-0" MIN UNDERCLEARANCE)
 SKEW: 50°-33'-22.5" (81'-0" (CURB TO CURB)
 CLEAR ROADWAY WIDTH: 99'-2" (100'-0" (OUT TO OUT))
 STRUCTURE NO: L-442

EXISTING STRUCTURE DATA
 STA: 137+50.00
 TYPE: SINGLE SPAN P/S COMP.
 SPAN: 107'-7" (112'-0" MIN UNDERCLEARANCE)
 SKEW: 45°-13'-47" (87'-8" (1/2" (CURB TO CURB)
 CLEAR ROADWAY WIDTH: 100'-8" (1/2" (OUT TO OUT))
 STRUCTURE NO: L-443

PROPOSED STRUCTURE DATA
 TYPE: SINGLE SPAN P/S COMP.
 SPAN: 107'-7" (112'-0" MIN UNDERCLEARANCE)
 SKEW: 45°-13'-47" (87'-8" (1/2" (CURB TO CURB)
 CLEAR ROADWAY WIDTH: 100'-8" (1/2" (OUT TO OUT))
 STRUCTURE NO: L-443

EXISTING STRUCTURE DATA
 STA: 143+50.00
 TYPE: SINGLE SPAN P/S COMP.
 SPAN: 107'-7" (112'-0" MIN UNDERCLEARANCE)
 SKEW: 45°-13'-47" (87'-8" (1/2" (CURB TO CURB)
 CLEAR ROADWAY WIDTH: 100'-8" (1/2" (OUT TO OUT))
 STRUCTURE NO: L-443

PROPOSED STRUCTURE DATA
 TYPE: SINGLE SPAN P/S COMP.
 SPAN: 107'-7" (112'-0" MIN UNDERCLEARANCE)
 SKEW: 45°-13'-47" (87'-8" (1/2" (CURB TO CURB)
 CLEAR ROADWAY WIDTH: 100'-8" (1/2" (OUT TO OUT))
 STRUCTURE NO: L-443

EXISTING STRUCTURE DATA
 STA: 149+50.00
 TYPE: SINGLE SPAN P/S COMP.
 SPAN: 107'-7" (112'-0" MIN UNDERCLEARANCE)
 SKEW: 45°-13'-47" (87'-8" (1/2" (CURB TO CURB)
 CLEAR ROADWAY WIDTH: 100'-8" (1/2" (OUT TO OUT))
 STRUCTURE NO: L-443

PROPOSED STRUCTURE DATA
 TYPE: SINGLE SPAN P/S COMP.
 SPAN: 107'-7" (112'-0" MIN UNDERCLEARANCE)
 SKEW: 45°-13'-47" (87'-8" (1/2" (CURB TO CURB)
 CLEAR ROADWAY WIDTH: 100'-8" (1/2" (OUT TO OUT))
 STRUCTURE NO: L-443

EXISTING STRUCTURE DATA
 STA: 155+50.00
 TYPE: SINGLE SPAN P/S COMP.
 SPAN: 107'-7" (112'-0" MIN UNDERCLEARANCE)
 SKEW: 45°-13'-47" (87'-8" (1/2" (CURB TO CURB)
 CLEAR ROADWAY WIDTH: 100'-8" (1/2" (OUT TO OUT))
 STRUCTURE NO: L-443

PROPOSED STRUCTURE DATA
 TYPE: SINGLE SPAN P/S COMP.
 SPAN: 107'-7" (112'-0" MIN UNDERCLEARANCE)
 SKEW: 45°-13'-47" (87'-8" (1/2" (CURB TO CURB)
 CLEAR ROADWAY WIDTH: 100'-8" (1/2" (OUT TO OUT))
 STRUCTURE NO: L-443

EXISTING STRUCTURE DATA
 STA: 161+50.00
 TYPE: SINGLE SPAN P/S COMP.
 SPAN: 107'-7" (112'-0" MIN UNDERCLEARANCE)
 SKEW: 45°-13'-47" (87'-8" (1/2" (CURB TO CURB)
 CLEAR ROADWAY WIDTH: 100'-8" (1/2" (OUT TO OUT))
 STRUCTURE NO: L-443

PROPOSED STRUCTURE DATA
 TYPE: SINGLE SPAN P/S COMP.
 SPAN: 107'-7" (112'-0" MIN UNDERCLEARANCE)
 SKEW: 45°-13'-47" (87'-8" (1/2" (CURB TO CURB)
 CLEAR ROADWAY WIDTH: 100'-8" (1/2" (OUT TO OUT))
 STRUCTURE NO: L-443

EXISTING STRUCTURE DATA
 STA: 167+50.00
 TYPE: SINGLE SPAN P/S COMP.
 SPAN: 107'-7" (112'-0" MIN UNDERCLEARANCE)
 SKEW: 45°-13'-47" (87'-8" (1/2" (CURB TO CURB)
 CLEAR ROADWAY WIDTH: 100'-8" (1/2" (OUT TO OUT))
 STRUCTURE NO: L-443

PROPOSED STRUCTURE DATA
 TYPE: SINGLE SPAN P/S COMP.
 SPAN: 107'-7" (112'-0" MIN UNDERCLEARANCE)
 SKEW: 45°-13'-47" (87'-8" (1/2" (CURB TO CURB)
 CLEAR ROADWAY WIDTH: 100'-8" (1/2" (OUT TO OUT))
 STRUCTURE NO: L-443

EXISTING STRUCTURE DATA
 STA: 173+50.00
 TYPE: SINGLE SPAN P/S COMP.
 SPAN: 107'-7" (112'-0" MIN UNDERCLEARANCE)
 SKEW: 45°-13'-47" (87'-8" (1/2" (CURB TO CURB)
 CLEAR ROADWAY WIDTH: 100'-8" (1/2" (OUT TO OUT))
 STRUCTURE NO: L-443

PROPOSED STRUCTURE DATA
 TYPE: SINGLE SPAN P/S COMP.
 SPAN: 107'-7" (112'-0" MIN UNDERCLEARANCE)
 SKEW: 45°-13'-47" (87'-8" (1/2" (CURB TO CURB)
 CLEAR ROADWAY WIDTH: 100'-8" (1/2" (OUT TO OUT))
 STRUCTURE NO: L-443

EXISTING STRUCTURE DATA
 STA: 179+50.00
 TYPE: SINGLE SPAN P/S COMP.
 SPAN: 107'-7" (112'-0" MIN UNDERCLEARANCE)
 SKEW: 45°-13'-47" (87'-8" (1/2" (CURB TO CURB)
 CLEAR ROADWAY WIDTH: 100'-8" (1/2" (OUT TO OUT))
 STRUCTURE NO: L-443

PROPOSED STRUCTURE DATA
 TYPE: SINGLE SPAN P/S COMP.
 SPAN: 107'-7" (112'-0" MIN UNDERCLEARANCE)
 SKEW: 45°-13'-47" (87'-8" (1/2" (CURB TO CURB)
 CLEAR ROADWAY WIDTH: 100'-8" (1/2" (OUT TO OUT))
 STRUCTURE NO: L-443

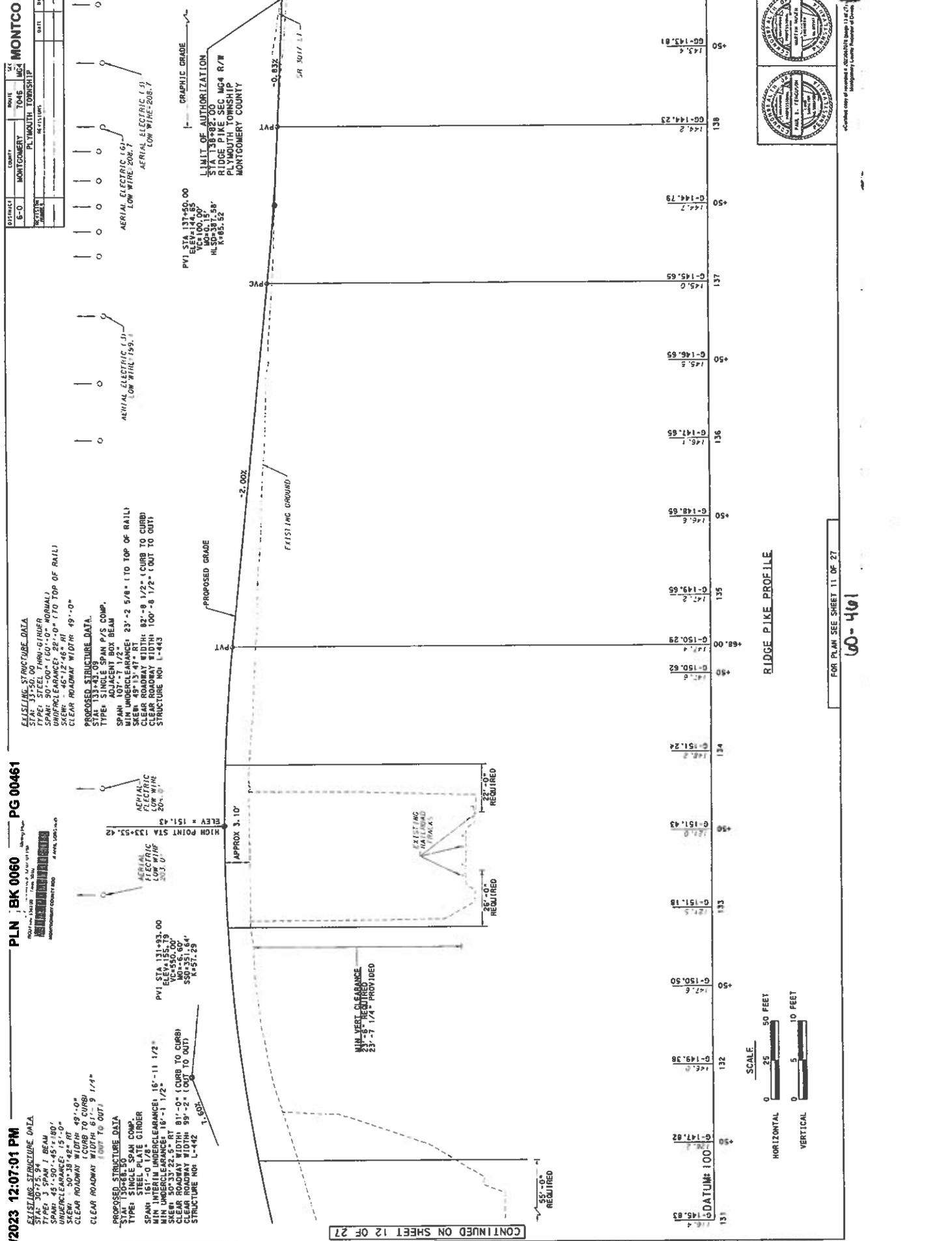
EXISTING STRUCTURE DATA
 STA: 185+50.00
 TYPE: SINGLE SPAN P/S COMP.
 SPAN: 107'-7" (112'-0" MIN UNDERCLEARANCE)
 SKEW: 45°-13'-47" (87'-8" (1/2" (CURB TO CURB)
 CLEAR ROADWAY WIDTH: 100'-8" (1/2" (OUT TO OUT))
 STRUCTURE NO: L-443

PROPOSED STRUCTURE DATA
 TYPE: SINGLE SPAN P/S COMP.
 SPAN: 107'-7" (112'-0" MIN UNDERCLEARANCE)
 SKEW: 45°-13'-47" (87'-8" (1/2" (CURB TO CURB)
 CLEAR ROADWAY WIDTH: 100'-8" (1/2" (OUT TO OUT))
 STRUCTURE NO: L-443

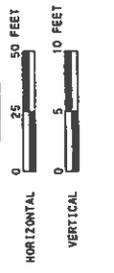
EXISTING STRUCTURE DATA
 STA: 191+50.00
 TYPE: SINGLE SPAN P/S COMP.
 SPAN: 107'-7" (112'-0" MIN UNDERCLEARANCE)
 SKEW: 45°-13'-47" (87'-8" (1/2" (CURB TO CURB)
 CLEAR ROADWAY WIDTH: 100'-8" (1/2" (OUT TO OUT))
 STRUCTURE NO: L-443

PROPOSED STRUCTURE DATA
 TYPE: SINGLE SPAN P/S COMP.
 SPAN: 107'-7" (112'-0" MIN UNDERCLEARANCE)
 SKEW: 45°-13'-47" (87'-8" (1/2" (CURB TO CURB)
 CLEAR ROADWAY WIDTH: 100'-8" (1/2" (OUT TO OUT))
 STRUCTURE NO: L-443

EXISTING STRUCTURE DATA
 STA: 197+50.00
 TYPE: SINGLE SPAN P/S COMP.
 SPAN: 107'-7" (112'-0" MIN UNDERCLEARANCE)
 SKEW: 45°-13'-47" (87'-8" (1/2" (CURB TO CURB)
 CLEAR ROADWAY WIDTH: 100'-8" (1/2" (OUT TO OUT))
 STRUCTURE NO: L-443



CONTINUED ON SHEET 12 OF 27



RIDGE PIKE PROFILE

FOR PLAN SEE SHEET 11 OF 27

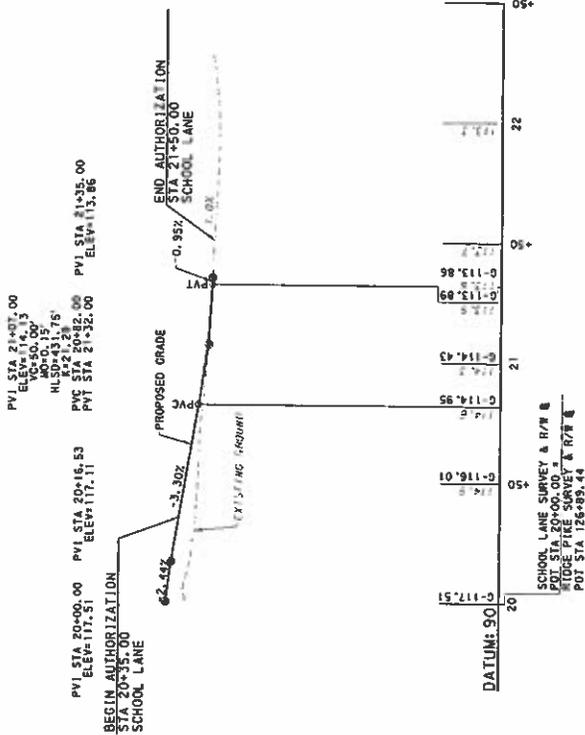
60-401



Professional Engineer License No. 1220230101

DISTRICT	COUNTY	STATE	SHEET
6-0	MONTGOMERY	MD	14 OF 27
MONTGOMERY COUNTY			
TOWNSHIP			
MOUNTAIN VIEW			
PROJECT			
SCHOOL LANE			

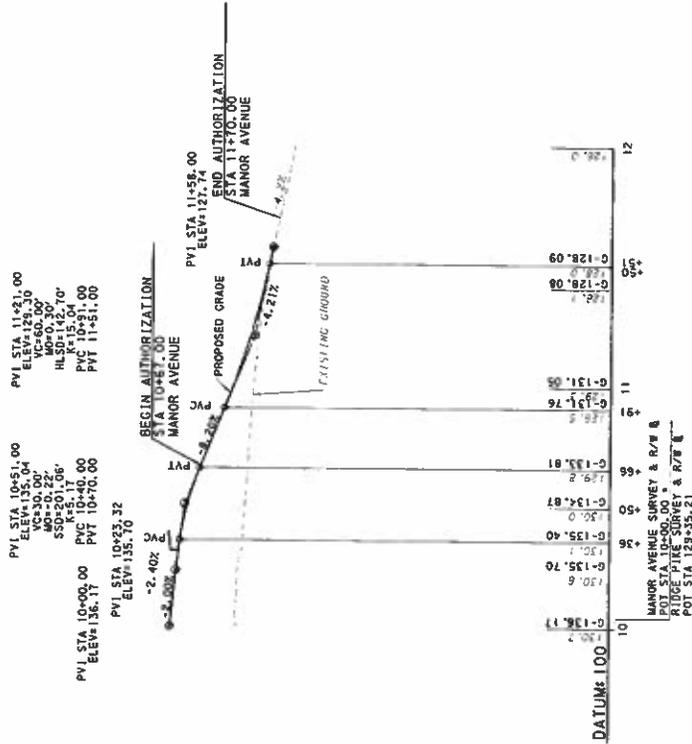
PLN BK 0060, SHEET 14 OF 27, PROJECT: SCHOOL LANE, DATE: 12/20/2023, TIME: 12:07:01 PM, DRAWN BY: [Redacted], CHECKED BY: [Redacted], APPROVED BY: [Redacted], TITLE: [Redacted]



FOR PLAN SEE SHEET 10 OF 27
 60-402

PROJECT NO.	5-0	TOWN	MONTGOMERY	COUNTY	MONTGOMERY
PROJECT NAME	MONTGOMERY TOWNSHIP				
DATE	11/13/2023				
SCALE	AS SHOWN				

PLN BK 0060, SHEET 00463, MONTGOMERY TOWNSHIP
 PROJECT NO. 5-0, DATE 11/13/2023
 PROJECT NAME MONTGOMERY TOWNSHIP
 COUNTY MONTGOMERY



FOR PLAN SEE SHEET 10 OF 21

60-463



12/20/2023 12:07:01 PM

PLN BK 0060 PG 00466

MONTGOMERY COUNTY

SEC 046 MONTGOMERY COUNTY

ADJACENT PROPERTY LINES ARE PLOTTED FROM THE DEED OF RECORD, INCLUDING SUBDIVISION OR LOT PLANS. EXISTING TOPOGRAPHICAL FEATURES AND LIMITED FIELD DATA. PRIVATE PROPERTY LINES WERE NOT SURVEYED BY THE PROFESSIONAL LAND SURVEYOR RESPONSIBLE FOR THE PROJECT.

THIS PROPERTY PLOT PLAN IS NOT TO BE SUBSTITUTED FOR A BOUNDARY SURVEY. NECESSARY CONSTRUCTION EASEMENT. AN EASEMENT TO USE THE LAND AS NECESSARY DURING CONSTRUCTION OR WORK INDICATED BY THE PLAN IS REQUIRED ONLY UNTIL THE CONSTRUCTION OR WORK INDICATED BY THE PLAN IS COMPLETED, UNLESS SOONER RELINQUISHED IN WRITING BY THE COUNTY.

UNDERGROUND STRUCTURE SUPPORT EASEMENT. AN EASEMENT FOR THE CONSTRUCTION, INSPECTION, MAINTENANCE, REPAIR, RECONSTRUCTION AND ALTERATION OF UNDERGROUND STRUCTURE SUPPORT ELEMENTS. THE EASEMENT SHALL NOT PREVENT THE OWNER FROM MAKING USE OF THE SURFACE FOR FARMING, INCLUDING THE CONSTRUCTION OF ANY STRUCTURE OF ANY KIND, OR THE EXERCISE THEREOF, NOR IS ANY EXCAVATION OR DRILLING ALLOWED WITHOUT ADVANCED WRITTEN APPROVAL BY THE DEPARTMENT OF TRANSPORTATION.

NORFOLK SOUTHERN RAILWAY COMPANY, SUCCESSOR THROUGH MERGER WITH PENNSYLVANIA LINES, LLC, DATED AUGUST 26, 2004. 49-00-1045-00-1. 49-00-1065-00-2.

Table with columns: DISTRICT, COUNTY, SEC, TOWNSHIP, RANGE, SECTION. Values: 5-0, MONTGOMERY, 046, MOUNTAIN, 046, 01.

Table with columns: SURVEY, DATE, BY. Values: SURVEY, 12/20/2023, [Name].

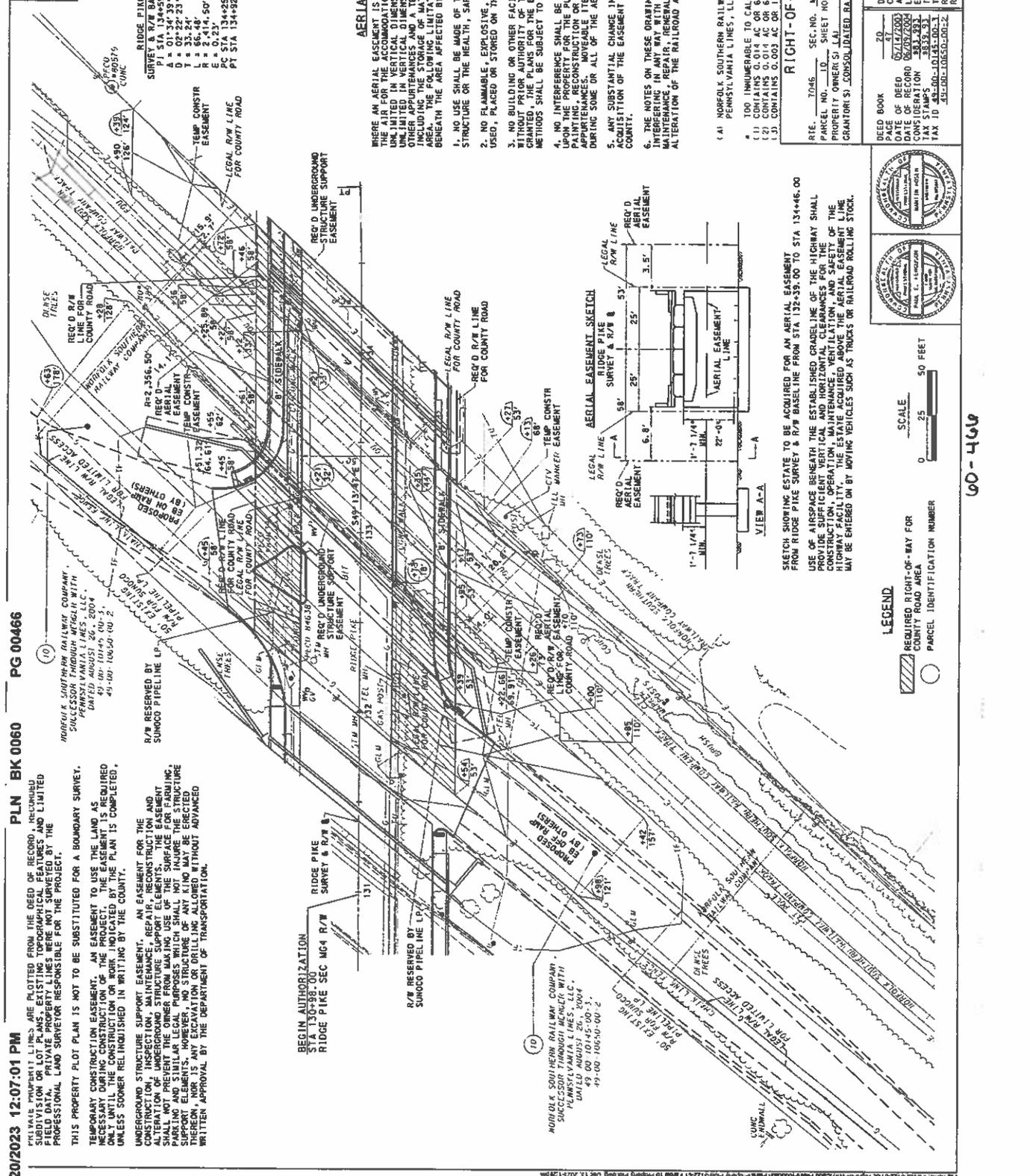


PLN BK 0060, 0066, 0067, 0068, 0069, 0070, 0071, 0072, 0073, 0074, 0075, 0076, 0077, 0078, 0079, 0080, 0081, 0082, 0083, 0084, 0085, 0086, 0087, 0088, 0089, 0090, 0091, 0092, 0093, 0094, 0095, 0096, 0097, 0098, 0099, 0100.

MONTEGOMERY COUNTY DEPARTMENT OF TRANSPORTATION

STATIONING: STA 130+00 TO STA 134+00

LEGAL R/W LINE FOR COUNTY ROAD



RIDGE PIKE SURVEY & R/W BASELINE
PI STA 134+59.13
A = 01'-34'-39" LT
D = 02'-22'-23" LT
L = 66'-48"
R = 2'-114'-50"
E = 0'-23"
PT STA 134+92.31

TEMP CONSTR EASEMENT

LEGAL R/W LINE FOR COUNTY ROAD

AERIAL EASEMENT NOTES

- WHERE AN AERIAL EASEMENT IS ACQUIRED, IT SHALL INCLUDE AN EASEMENT IN THE AIR FOR THE ACCOMMODATION OF THE ELEVATED HIGHWAY STRUCTURE... 1. NO USE SHALL BE MADE OF THE PROPERTY WHICH SHALL ENDANGER THE STRUCTURE OR THE HEALTH, SAFETY OR WELFARE OF THE TRAVELING PUBLIC... 2. NO FLAMMABLE, EXPLOSIVE, DANGEROUS OR HAZARDOUS MATERIAL SHALL BE USED, PLACED OR STORED ON THE PROPERTY... 3. NO BUILDING OR OTHER FACILITY SHALL BE CONSTRUCTED ON THE PROPERTY... 4. NO INTERFERENCE SHALL BE MADE WITH THE RIGHT OF THE COUNTY TO ENTER UPON THE PROPERTY FOR THE PURPOSES OF INSPECTION, MAINTENANCE, REPAIR, PAINTING, RECONSTRUCTION OR ALTERATION OF THE STRUCTURE OR APURTANCES... 5. ANY SUBSTANTIAL CHANGE IN PROPERTY USE TO BE MADE SUBSEQUENT TO THE ACQUISITION OF THE EASEMENT SHALL BE SUBJECT TO THE APPROVAL OF THE COUNTY... 6. THE NOTES ON THESE DRAWINGS SHALL NOT BE CONSTRUED AS LIMITING OR INTERFERING IN ANY WAY WITH THE PRESENT AND FUTURE OPERATION, USE, MAINTENANCE, REPAIR, RENEWAL, CHANGE, ADDITION, BETTERMENT OR ALTERATION OF THE RAILROAD AND ITS SUPPORTING FACILITIES.

(C) NORFOLK SOUTHERN RAILWAY COMPANY, SUCCESSOR THROUGH MERGER WITH PENNSYLVANIA LINES, LLC, DATED AUGUST 26, 2004

- 1. TO UNIMPROVABLE TO CALCULATE
- 2. CONTAINS 0.014 AC OR 612 SF OF UNDERGROUND STRUCTURE SUPPORT EASEMENT.
- 3. CONTAINS 0.011 AC OR 612 SF OF REQUIRED AERIAL EASEMENT.
- 4. CONTAINS 0.003 AC OR 147 SF OF UNDERGROUND STRUCTURE SUPPORT EASEMENT.

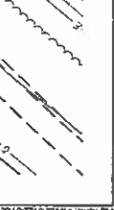
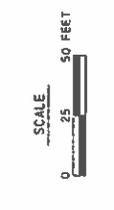
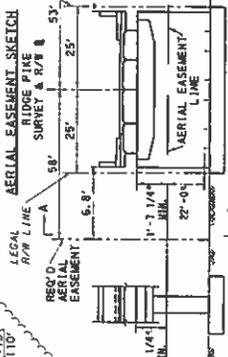
RIGHT-OF-WAY CLAIM INFORMATION

FILE NO. 7046 SEC. NO. M64 R/W MONTGOMERY COUNTY

PARCEL NO. 10 SHEET NO. 10.1.1 CLAIM NO. GRANTOR(S) CONSOLIDATED RAIL CORPORATION

Table with columns: DEED BOOK, DEED NO., DATE OF DEED, DATE OF RECORD, CONSIDERATION, TAX ID, REQUIRED AREA, ACRES, AERIAL EASEMENT, UNDERGROUND STR. SUPPORT, LEGAL R/W, EFFECTIVE AREA, TOTAL REQ'D R/W, VERIFICATION DATE, DRAWN BY, SCALE.

SKETCH SHOWN ESTIMATE TO BE ACQUIRED FOR AN AERIAL EASEMENT FROM RIDGE PIKE SURVEY & R/W BASELINE FROM STA 132+39.00 TO STA 134+46.00 USE OF AIRSPACE BENEATH THE ESTABLISHED GRADELINE OF THE HIGHWAY SHALL PROVIDE SUFFICIENT VERTICAL AND HORIZONTAL CLEARANCES FOR THE PROPOSED UNDERGROUND STRUCTURE SUPPORT ELEMENTS AND SAFETY OF THE HIGHWAY FACILITY. THE EASEMENT ACQUIRED ABOVE THE LEGAL R/W LINE MAY BE ENTERED ON BY MOVING VEHICLES SUCH AS TRUCKS OR RAILROAD ROLLING STOCK.



SCALE 0 25 50 FEET

LEGEND
REQUIRED RIGHT-OF-WAY FOR COUNTY ROAD AREA
PARCEL IDENTIFICATION NUMBER

60-466

**NS RR PARCEL 10
METES-BOUNDS**

Plymouth Township, Montgomery County
County Route Ridge Pike Section MG4
PUC No.

Norfolk Southern Railway Company
c/o Consolidated Rail Corporation
1200 Peachtree Street, N.E.
Atlanta, Georgia 30309-3579

ALL THAT CERTAIN tract of land situated in the Township of Plymouth, County of Montgomery, Commonwealth of Pennsylvania, bound and described as follows:

TRACT NO. 1
(Required Right-of-Way)

Beginning at a point 17.74 feet right of Ridge Pike Survey & R/W Baseline at Station 131+83± (Coordinates Northing 290706.2291, Easting 2649665.8265); thence South 49°18'39" East a distance of 89.71 feet to a point 18± feet right of Ridge Pike Survey & R/W Baseline at Station 132+73± thence South 84°27'02" West a distance of 48.94 feet to a point 53.00 feet right of Ridge Pike Survey & R/W Baseline at Station 132+39.00 thence North 49°13'47" West a distance of 84.83 feet to a point 53.00 feet right of Ridge Pike Survey & R/W Baseline at Station 131+54± thence North 80°07'32" East a distance of 45.61 feet to a point 18± feet right of Ridge Pike Survey & R/W Baseline at Station 131+83± and the POINT OF BEGINNING.

The above-described parcel contains 3081 sq. ft., more or less (0.071 acres±).

The above tract of land shown on the Department's Drawings Authorizing Acquisition of Right of Way for Ridge Pike Section MG4 R/W, as required right-of-way, and designated and delineated as being a portion of the same property of Norfolk Southern Railway Company, by deed dated May 14, 2003, and recorded on June 09, 2004, in the Department of Real Estate, in Montgomery County, by DBV 20 PG 47, granted and conveyed unto Consolidated Rail Corporation, the grantor herein.

TRACT NO. 2
(Required Right-of-Way)

Beginning at a point 32± feet left of Ridge Pike Survey & R/W Baseline at Station 132+24± (Coordinates Northing 290717.3313, Easting 2649729.6065); thence North 80°07'32" East a distance of 33.21 feet to a point 58.00 feet left of Ridge Pike Survey & R/W Baseline at Station 132+45± thence South 49°13'47" East a distance of 99.80 feet to a point 58.00 feet left of Ridge Pike Survey & R/W Baseline at Station 133+45.00 thence South 84°27'02" West a distance of 35.32 feet to a point 32± feet left of Ridge Pike Survey & R/W Baseline at Station 133+21± thence North 49°18'39" West a distance of 96.47 feet to a point 32± feet left of Ridge Pike Survey & R/W Baseline at Station 132+24± and the POINT OF BEGINNING.

The above-described parcel contains 2512 sq. ft., more or less (0. 058 acres±).

The above tract of land shown on the Department's Drawings Authorizing Acquisition of Right of Way for Ridge Pike Section MG4 R/W, as required right-of-way, and designated and delineated as being a portion of the same property of Norfolk Southern Railway Company, by deed dated May 14, 2003, and recorded on June 09, 2004, in the Department of Real Estate, in Montgomery County, by DBV 20 PG 47, granted and conveyed unto Consolidated Rail Corporation, the grantor herein.

TRACT NO. 3
(Required Right-of-Way)

Beginning at a point 33± feet left of Ridge Pike Survey & R/W Baseline at Station 134+21.00 (Coordinates Northing 290588.9867, Easting 2649878.8779); thence North 84°39'59" East a distance of 35.36 feet to a point 58.00 feet left of Ridge Pike Survey & R/W Baseline at Station 134+46.00 thence along an arc 25.86 feet to the left, having a radius of 2356.50 feet, the chord of which is South 50°01'16" East for a distance of 25.86 feet, to a point 58.00 feet left of Ridge Pike Survey & R/W Baseline at Station 134+72± thence South 84°02'56" West a distance of 35.44 feet to a point 33± feet left of Ridge Pike Survey & R/W Baseline at Station 134+47± thence North 49°18'39" West a distance of 26.08 feet to a point 33± feet left of Ridge Pike Survey & R/W Baseline at Station 134+21.00 and the POINT OF BEGINNING.

The above-described parcel contains 660 sq. ft., more or less (0.015 acres±).

The above tract of land shown on the Department's Drawings Authorizing Acquisition of Right of Way for Riherein.ke Section MG4 R/W, as required right-of-way, and designated and delineated as being a portion of the same property of Norfolk Southern Railway Company, by deed dated May 14, 2003, and recorded on June 09, 2004, in the Department of Real Estate, in Montgomery County, by DBV 20 PG 47, granted and conveyed unto Consolidated Rail Corporation, the grantor herein.

TRACT NO. 4
(Required Aerial Easement)

Beginning at a point 18± feet right of Ridge Pike Survey & R/W Baseline at Station 132+73± (Coordinates Northing 290647.7413, Easting 2649733.8509); thence South 49°18'39" East a distance of 87.68 feet to a point 17.48 feet right of Ridge Pike Survey & R/W Baseline at Station 133+60± thence South 84°13'37" West a distance of 48.93 feet to a point 53.00 feet right of Ridge Pike Survey & R/W Baseline at Station 133+27± thence North 49°13'47" West a distance of 87.83 feet to a point 53.00 feet right of Ridge Pike Survey & R/W Baseline at Station 132+39.00 thence North 84°27'02" East a distance of 48.94 feet to a point 18± feet right of Ridge Pike Survey & R/W Baseline at Station 132+73± and the POINT OF BEGINNING.

The above-described parcel contains 3109 sq. ft., more or less (0.071 acres±).

The above tract of land shown on the Department's Drawings Authorizing Acquisition of Right of Way for Ridge Pike Section MG4 R/W, as required aerial easement, and designated and delineated as being a portion of the same property of Norfolk Southern Railway Company, by deed dated May 14, 2003, and recorded on June 09, 2004, in the Department of Real Estate, in

Montgomery County, by DBV 20 PG 47, granted and conveyed unto Consolidated Rail Corporation, the grantor herein.

TRACT NO. 5
(Required Aerial Easement)

Beginning at a point 32± feet left of Ridge Pike Survey & R/W Baseline at Station 133+21± (Coordinates Northing 290654.4363, Easting 2649802.7566); thence North 84°27'02" East a distance of 35.32 feet to a point 58.00 feet left of Ridge Pike Survey & R/W Baseline at Station 133+45.00 thence South 49°13'47" East a distance of 77.00 feet to a point 58.00 feet left of Ridge Pike Survey & R/W Baseline at Station 134+22.00 thence South 49°13'47" East a distance of 3.89 feet to a point 58.00 feet left of Ridge Pike Survey & R/W Baseline at Station 134+25.89 thence along an arc 19.63 feet to the left, having a radius of 2356.50 feet, the chord of which is South 49°28'06" East for a distance of 19.63 feet, to a point 58.00 feet left of Ridge Pike Survey & R/W Baseline at Station 134+46.00 thence South 84°39'59" West a distance of 35.36 feet to a point 33± feet left of Ridge Pike Survey & R/W Baseline at Station 134+21.00 thence North 49°18'39" West a distance of 100.39 feet to a point 32± feet left of Ridge Pike Survey & R/W Baseline at Station 133+21± and the POINT OF BEGINNING.

The above-described parcel contains 2556 sq. ft., more or less (0.059 acres±).

The above tract of land shown on the Department's Drawings Authorizing Acquisition of Right of Way for Ridge Pike Section MG4 R/W, as required aerial easement, and designated and delineated as being a portion of the same property of Norfolk Southern Railway Company, by deed dated May 14, 2003, and recorded on June 09, 2004, in the Department of Real Estate, in Montgomery County, by DBV 20 PG 47, granted and conveyed unto Consolidated Rail Corporation, the grantor herein.

TRACT NO. 6
(Required Underground Structure Support Easement)

Beginning at a point 33± feet left of Ridge Pike Survey & R/W Baseline at Station 134+12.00 (Coordinates Northing 290594.8543, Easting 2649872.0536); thence North 83°49'14" East a distance of 34.80 feet to a point 58.00 feet left of Ridge Pike Survey & R/W Baseline at Station 134+36.00 thence along an arc 9.76 feet to the left, having a radius of 2356.50 feet, the chord of which is South 49°35'18" East for a distance of 9.76 feet, to a point 58.00 feet left of Ridge Pike Survey & R/W Baseline at Station 134+46.00 thence South 84°39'59" West a distance of 35.36 feet to a point 33± feet left of Ridge Pike Survey & R/W Baseline at Station 134+21.00 thence North 49°18'39" West a distance of 9.00 feet to a point 33± feet left of Ridge Pike Survey & R/W Baseline at Station 134+12.00 and the POINT OF BEGINNING.

The above-described parcel contains 238 sq. ft., more or less (0.005 acres±).

The above tract of land shown on the Department's Drawings Authorizing Acquisition of Right of Way for Ridge Pike Section MG4 R/W, as required underground structure support easement,

and designated and delineated as being a portion of the same property of Norfolk Southern Railway Company, by deed dated May 14, 2003, and recorded on June 09, 2004, in the Department of Real Estate, in Montgomery County, by DBV 20 PG 47, granted and conveyed unto Consolidated Rail Corporation, the grantor herein.

TRACT NO. 7
(Required Underground Structure Support Easement)

Beginning at a point 53.00 feet left of Ridge Pike Survey & R/W Baseline at Station 132+39.00 (Coordinates Northing 290643.0088, Easting 2649685.1433); thence North 84°27'02" East a distance of 153.48 feet to a point 58.00 feet left of Ridge Pike Survey & R/W Baseline at Station 133+45.00 thence North 84°29'00" East a distance of 9.14 feet to a point 64.61 feet left of Ridge Pike Survey & R/W Baseline at Station 133+51.32 thence South 13°53'02" East a distance of 4.51 feet to a point 62.00 feet left of Ridge Pike Survey & R/W Baseline at Station 133+55.00 thence South 84°28'06" West a distance of 186.72 feet to a point 73.00 feet right of Ridge Pike Survey & R/W Baseline at Station 132+26.00 thence North 6°27'22" West a distance of 4.55 feet to a point 69.91 feet right of Ridge Pike Survey & R/W Baseline at Station 132+22.66 thence North 84°47'21" East a distance of 23.52 feet to a point 53.00 feet right of Ridge Pike Survey & R/W Baseline at Station 132+39.00 and the POINT OF BEGINNING.

The above-described parcel contains 829 sq. ft., more or less (0.019 acres±).

The above tract of land shown on the Department's Drawings Authorizing Acquisition of Right of Way for Ridge Pike Section MG4 R/W, as required underground structure support easement, and designated and delineated as being a portion of the same property of Norfolk Southern Railway Company, by deed dated May 14, 2003, and recorded on June 09, 2004, in the Department of Real Estate, in Montgomery County, by DBV 20 PG 47, granted and conveyed unto Consolidated Rail Corporation, the grantor herein.

TRACT NO. 8
(Temporary Construction Easement)

Beginning at a point 53.00 feet right of Ridge Pike Survey & R/W Baseline at Station 131+54± (Coordinates Northing 290698.4080, Easting 2649620.8956); thence South 49°13'47" East a distance of 100.83 feet to a point 53.00 feet right of Ridge Pike Survey & R/W Baseline at Station 132+55.00 thence South 84°44'51" West a distance of 79.21 feet to a point 110.00 feet right of Ridge Pike Survey & R/W Baseline at Station 132+00.00 thence North 49°13'47" West a distance of 15.00 feet to a point 110.00 feet right of Ridge Pike Survey & R/W Baseline at Station 131+85.00 thence South 83°13'32" West a distance of 63.70 feet to a point 157.00 feet right of Ridge Pike Survey & R/W Baseline at Station 131+42.00 thence North 9°41'01" West a distance of 56.54 feet to a point 121.00 feet right of Ridge Pike Survey & R/W Baseline at Station 130+98± thence North 80°07'32" East a distance of 87.94 feet to a point 53.00 feet right of Ridge Pike Survey & R/W Baseline at Station 131+54± and the POINT OF BEGINNING.

The above-described parcel contains 7738 sq. ft., more or less (0.178 acres±).

The above tract of land shown on the Department's Drawings Authorizing Acquisition of Right of Way for Ridge Pike Section MG4 R/W, as temporary construction easement, and designated and delineated as being a portion of the same property of Norfolk Southern Railway Company, by deed dated May 14, 2003, and recorded on June 09, 2004, in the Department of Real Estate, in Montgomery County, by DBV 20 PG 47, granted and conveyed unto Consolidated Rail Corporation, the grantor herein.

TRACT NO. 9
(Temporary Construction Easement)

Beginning at a point 58.00 feet left of Ridge Pike Survey & R/W Baseline at Station 132+45± (Coordinates Northing 290723.0262, Easting 2649762.3223); thence North 80°07'32" East a distance of 152.99 feet to a point 176± feet left of Ridge Pike Survey & R/W Baseline at Station 133+42± thence South 54°52'28" East a distance of 21.21 feet to a point 178± feet left of Ridge Pike Survey & R/W Baseline at Station 133+63± thence South 11°15'54" East a distance of 81.90 feet to a point 128.00 feet left of Ridge Pike Survey & R/W Baseline at Station 134+28.00 thence South 84°28'04" West a distance of 96.82 feet to a point 58.00 feet left of Ridge Pike Survey & R/W Baseline at Station 133+61.00 thence North 49°13'47" West a distance of 115.80 feet to a point 58.00 feet left of Ridge Pike Survey & R/W Baseline at Station 132+45± and the POINT OF BEGINNING.

The above-described parcel contains 12063 sq. ft., more or less (0.277 acres±).

The above tract of land shown on the Department's Drawings Authorizing Acquisition of Right of Way for Ridge Pike Section MG4 R/W, as temporary construction easement, and designated and delineated as being a portion of the same property of Norfolk Southern Railway Company, by deed dated May 14, 2003, and recorded on June 09, 2004, in the Department of Real Estate, in Montgomery County, by DBV 20 PG 47, granted and conveyed unto Consolidated Rail Corporation, the grantor herein.

TRACT NO. 10
(Temporary Construction Easement)

Beginning at a point 53.00 feet right of Ridge Pike Survey & R/W Baseline at Station 133+27± (Coordinates Northing 290585.6557, Easting 2649751.6569); thence South 84°13'37" West a distance of 78.52 feet to a point 110.00 feet right of Ridge Pike Survey & R/W Baseline at Station 132+73± thence North 49°13'47" West a distance of 2.82 feet to a point 110.00 feet right of Ridge Pike Survey & R/W Baseline at Station 132+70.00 thence North 80°16'41" East a distance of 73.88 feet to a point 53.00 feet right of Ridge Pike Survey & R/W Baseline at Station 133+17.00 thence South 49°13'47" East a distance of 9.83 feet to a point 53.00 feet right of Ridge Pike Survey & R/W Baseline at Station 133+27± and the POINT OF BEGINNING.

The above-described parcel contains 360 sq. ft., more or less (0.008 acres±).

The above tract of land shown on the Department's Drawings Authorizing Acquisition of Right of Way for Ridge Pike Section MG4 R/W, as temporary construction easement, and designated

and delineated as being a portion of the same property of Norfolk Southern Railway Company, by deed dated May 14, 2003, and recorded on June 09, 2004, in the Department of Real Estate, in Montgomery County, by DBV 20 PG 47, granted and conveyed unto Consolidated Rail Corporation, the grantor herein.

TRACT NO. 11
(Temporary Construction Easement)

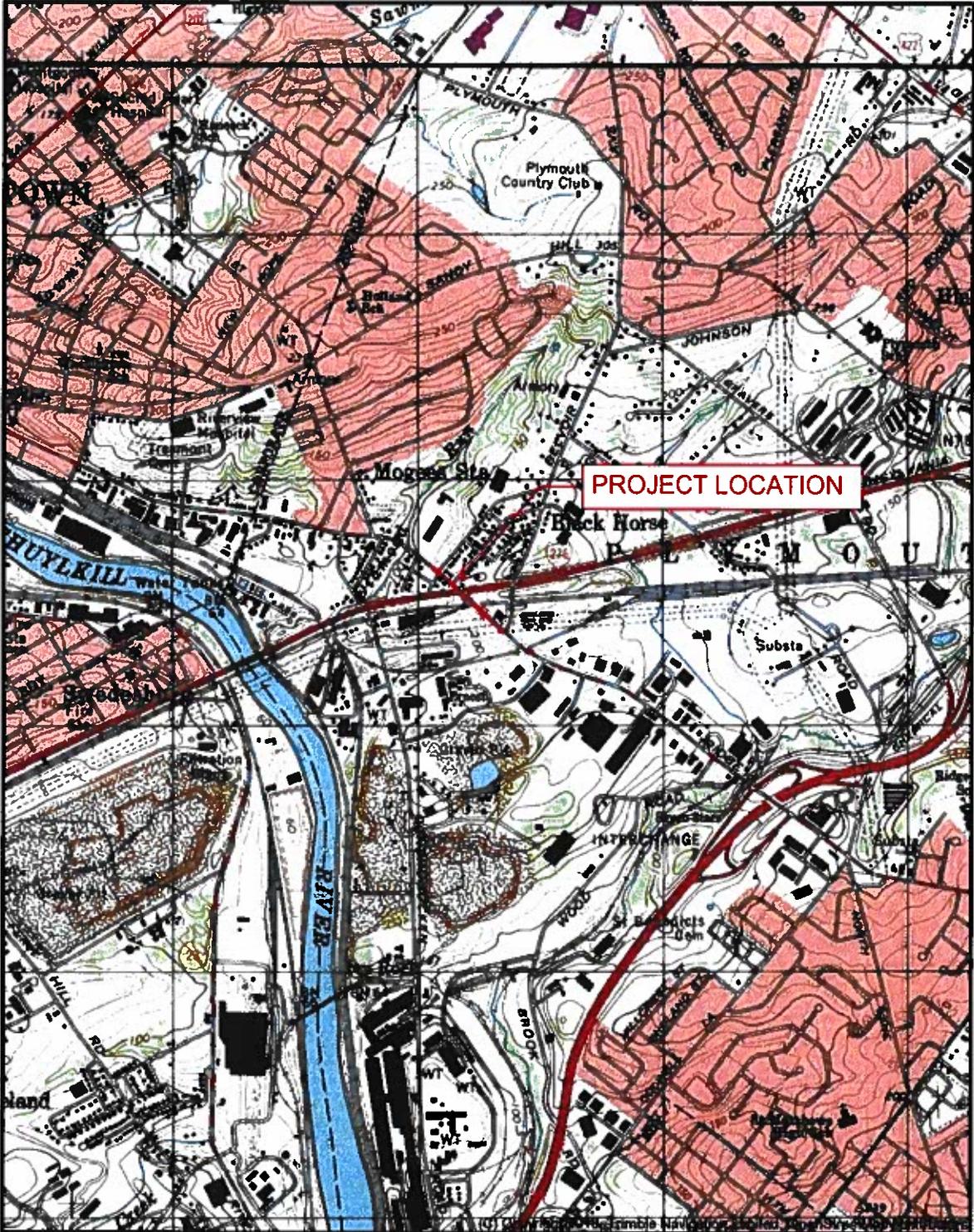
Beginning at a point 58.00 feet left of Ridge Pike Survey & R/W Baseline at Station 134+72± (Coordinates Northing 290575.6594, Easting 2649933.8991); thence along an arc 45.49 feet to the right, having a radius of 2356.50 feet, the chord of which is North 49°46'57" West for a distance of 45.49 feet, to a point 58.00 feet left of Ridge Pike Survey & R/W Baseline at Station 134+25.89 thence North 49°13'47" West a distance of 3.89 feet to a point 58.00 feet left of Ridge Pike Survey & R/W Baseline at Station 134+22.00 thence North 83°59'07" East a distance of 94.41 feet to a point 126.00 feet left of Ridge Pike Survey & R/W Baseline at Station 134+90.00 thence South 48°26'30" East a distance of 48.48 feet to a point 124.00 feet left of Ridge Pike Survey & R/W Baseline at Station 135+39± thence South 84°02'56" West a distance of 92.99 feet to a point 58.00 feet left of Ridge Pike Survey & R/W Baseline at Station 134+72± and the POINT OF BEGINNING.

The above-described parcel contains 3351 sq. ft., more or less (0.077 acres±).

The above tract of land shown on the Department's Drawings Authorizing Acquisition of Right of Way for Ridge Pike Section MG4 R/W, as temporary construction easement, and designated and delineated as being a portion of the same property of Norfolk Southern Railway Company, by deed dated May 14, 2003, and recorded on June 09, 2004, in the Department of Real Estate, in Montgomery County, by DBV 20 PG 47, granted and conveyed unto Consolidated Rail Corporation, the grantor herein.

Prepared by: Monaloh Basin Engineers
300 Business Center Drive
Suite 304
Pittsburgh, PA 15205
(412)-788-2433
MBE Job Number: 3122
February 14, 2024
File Name: Parcel 10 - Metes-Bounds.doc

PROJECT LOCATION MAP



Source: Norristown USGS 7.5 Minute
Quadrangle

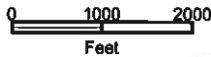


FIGURE 1
PROJECT LOCATION MAP
RIDGE PIKE - SCHOOL LANE TO
BELVOIR ROAD (CB#0) AND TPK
BD-116)/INTERCHANGE AREA
PLYMOUTH TOWNSHIP,
MONTGOMERY COUNTY, PA

CBO RR COST ESTIMATE 20240612

OPINION OF PROBABLE COST

**Project: Ridge Pike Section MG4 - School Lane to Belvoir Road
Bridge CB#0 over Norfolk-Southern Railway**

Prepared By: Traffic Planning and Design, Inc.

Matthew Hickson, P.E.

Date: 6/12/2024

Category	Cost	Notes
Design of CB#0	\$1,425,000	Includes \$380,000 for Preliminary Engineering plus \$1,045,000 for Final Design
Utility Relocation	\$440,000	40% of \$1,100k estimated cost, incl. PECO Gas line and Fiber Optic Attachments
Right-of-Way	\$130,000	(Acquisition from N-S Parcel for Ridge Pike for CB#0)
- Prof. Services Reimbursement Cost	\$4,000	
- Property Valuation	\$6,000	
- Required Right-of-Way	\$69,000	Approx. 0.144 ac
- Underground Str Support Easement	\$12,000	Approx. 0.025 ac
- Aerial Easement	\$16,600	Approx 0.130 ac
- Temporary Construction Easement	\$25,900	Approx 0.540 ac for 2 years
Construction (CB#0)	\$22,071,174	
-Structure (CB#0)	\$13,890,413	**See attached estimate-90% estimate dated 6/11/24 plus 10% contingency
-Removal of Existing Bridge	\$770,000	**
-Roadway Approach Work	\$7,410,762	40% of Section MG4 Roadway Approach Work (\$18,526,904) -see attached estimate
Construction Inspection	\$2,207,117	10% of Construction Cost Estimate per PennDOT Publication 352
Railroad Flagging	\$500,000	Assumes \$1,250/day for 400 work days (2 years for two-stage construction)
Railroad Engineering	\$100,000	Extrapolated from smaller project
Total *	\$26,870,000	
-*Rounded to nearest \$10,000		

DESIGNERS OPINION OF PROBABLE COST

TYPE OF ESTIMATE: PRE-FINAL - 90% ESTIMATE

PROJECT: **RIDGE PIKE, SECTION MG4 (MPMS 110444)**
MUNICIPALITY: Plymouth Township, Montgomery County, PA
TPD JOB #: MOCO.00060

PLAN TITLE: 90% ESTIMATE
PLAN DATE: - June 2024
ROAD(S): RIDGE PIKE
DESCRIPTION OF WORK: Widen Ridge Pike to 5 lanes between School Ln and Belvoir Ave and construct 5' sdwk & 3' buffer
 Overwiden CB#0 and DB#116 to 81' to accommodate future auxillary lanes
 Construct intersection terminals at Ramp A/D intersection
 Modify traffic signal at Ridge and Belvoir
 Reconstruct intersection terminal of Ridge & Manor Ave as Right-In Right-Out
 Reconstruct intersection terminal of Ridge & School Lane

ESTIMATE DATE: 6/11/2024
PREPARED BY: MR
CHECKED BY: MIH
SOURCE FOR UNIT COSTS: Penn DOT Publication 287, Bulletin 50 Construction Cost Catalog (ECMS)
 OR: Web Site: www.BidHistory.com

NOTES: Unit prices last revised 04/17/23, no escalation based on inflation between June 22-June 24
 Unit Prices further modified based on Ridge Pk Section MG2 ECMS 48175 Bid 4/27/2023
 Construction Year of 2024, Completion in 2026
 Ex Truss Sign Structure on I-276 will be removed, but replaced by others (Lafayette)
 Pavement depths on Ridge Pike match Section MGL to North and Section MG2 to South
 Guide Signage for new interchange on Ridge Pike not included in estimate.
 Estimate assumes only minor shoulder repair work on I-276. I-276 widening included in PTC est.
 Estimate does not include retaining walls along Future Ramps A, C & D that may require partial construction under this contract.

DISCLAIMER: TPD IS FURNISHING THIS COST ESTIMATE AS REQUESTED BY THE CLIENT. PLEASE NOTE THAT ESTIMATED COSTS ARE SUBJECT TO CHANGE BASED ON FIELD CONDITIONS, LOCAL OR REGIONAL DIFFERENCES, CHANGES TO THE PLANS, AND/ OR CHANGES IN UNIT COSTS. COST ESTIMATES ARE PROVIDED FOR USE IN BUDGETING, BUT IN NO WAY SHOULD THIS ESTIMATE BE CONSTRUED AS A FINAL COST FOR THE PROJECT. FINAL COSTS ARE CONTINGENT ON ACTUAL BIDS FROM CONTRACTORS. TPD WILL NOT BE HELD RESPONSIBLE FOR DIFFERENCES BETWEEN THIS COST ESTIMATE AND BID COSTS.

DESCRIPTION	UNIT	QUANTITY	UNIT PRICE	COST
GRADING AND PREPARATION				
CLEARING AND GRUBBING	AC	4.2	\$40,000.00	\$166,942
CLASS 1 EXCAVATION	CY	11.315	\$42.00	\$475,224
SAWCUT	LF	540	\$8.00	\$4,320
COMPLETED EMBANKMENT (FILL)	CY	6,000	\$0.00	\$0
BORROW EXCAVATION (FILL)	CY	0	\$15.00	\$0
FOREIGN BORROW EXCAVATION (FILL)	CY	3,600	\$0.00	\$0
UNFORSEEN WASTE MATERIAL SAMPLING	DOLLA	50,000	\$1.00	\$50,000
SOIL TESTING	EACH	20	\$1,500.00	\$30,000
UNEXPECTED REGULATED RESIDUAL OR HAZARDOUS WASTE	DOLLA	40,000	\$1.00	\$40,000
SUBTOTAL GRADING AND PREPARATION				\$766,487

PAVEMENT ITEMS				
SUPERPAVE WEARING COURSE, 1.5" DEPTH, SRL-E	SY	10,074	\$14.00	\$141,036
SUPERPAVE WEARING COURSE, 1.5" DEPTH, SRL-L	SY	940	\$12.00	\$11,280
SUPERPAVE BINDER COURSE, 2.5" DEPTH	SY	5,698	\$23.00	\$131,054
SUPERPAVE BASE COURSE, 4.5" DEPTH	SY	69	\$33.00	\$2,277
SUPERPAVE BASE COURSE, 6" DEPTH	SY	856	\$36.00	\$30,816
SUPERPAVE BASE COURSE, 8" DEPTH	SY	5,698	\$53.00	\$301,994
PLAIN CEMENT CONCRETE PAVEMENT, 6" DEPTH	SY	69	\$120.00	\$8,280
PLAIN CEMENT CONCRETE PAVEMENT, 12" DEPTH	SY	946	\$180.00	\$170,280
ASPHALT TREATED PERMEABLE BASE COURSE, 4" DEPTH	SY	1,015	\$36.00	\$36,540
CEMENT TREATED PERMEABLE BASE COURSE, 4" DEPTH	SY	69	\$40.00	\$2,760
CONCRETE SHOULDERS, TYPE 2	SY	269	\$160.00	\$43,040
WEARING LEVELING COURSE	TON	126	\$190.00	\$23,940
SUBBASE 4" DEPTH (NO. 2A)	SY	69	\$11.00	\$759
SUBBASE 6" DEPTH (NO. 2A)	SY	2,138	\$18.00	\$38,484
SUBBASE 9" DEPTH (NO. 2A)	SY	5,698	\$21.00	\$119,658
ASPHALT TACK COAT	SY	22,028	\$0.74	\$16,301
BRIDGE APPROACH SLAB	SY	1,712	\$0.00	\$0
GEOTEXTILE, CLASS 4, TYPE A	SY	7,905	\$3.20	\$25,296
MILLING OF BITUMINOUS PAVEMENT SURFACE, 1 1/2" DEPTH	SY	4,391	\$6.40	\$28,102
PAVED SHOULDERS TYPE 1-SP	SY	0	\$37.00	\$0
SUBTOTAL PAVEMENT ITEMS				\$1,131,897
CURBING ITEMS				
PLAIN CEMENT CONCRETE CURB (VARIOUS HEIGHTS)	LF	3,321	\$59.00	\$195,939
CONCRETE MOUNTABLE CURB	LF	100	\$83.00	\$8,300
SINGLE FACE BARRIER, 41" HEIGHT	LF	260	\$120.00	\$31,200
PLAIN CEMENT CONCRETE PAVEMENT, 4" DEPTH	SY	50	\$190.00	\$9,500
CONCRETE SIDEWALK	SY	1,966	\$160.00	\$314,560
SIDEWALKS AND DRIVEWAY APRONS THROUGH DRIVEWAYS	SY	22	\$280.00	\$6,160
GUIDERAIL (TYPE 31-SC)	LF	250	\$60.00	\$15,000
TYPICAL GUIDE RAIL TRANSITION TO CONCRETE BRIDGE	LF	5	\$2,400.00	\$12,000
GUIDERAIL END TREATMENT (MASH)	EA	2	\$4,600.00	\$9,200
SUBTOTAL CURBING ITEMS				\$601,859
DRAINAGE ITEMS				

COST ESTIMATE

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6" PAVEMENT BASE DRAIN	LF	1,478	\$26.00	\$38,428
SUBGRADE DRAINS	LF	480	\$32.00	\$15,360
18" REINFORCED CONCRETE PIPE	LF	1,178	\$180.00	\$212,040
GEOTEXTILE, CLASS I	LF	1,178	\$3.40	\$4,005
GRATE ADJUSTMENT OF EXISTING INLET	SET	3	\$1,500.00	\$4,500
TYPE C INLET TOP UNIT W/ BICYCLE SAFE GRATE	SET	16	\$1,900.00	\$30,400
STANDARD INLET BOX <10'	EA	16	\$3,800.00	\$60,800
SUBTOTAL DRAINAGE ITEMS				\$365,533
SIGNS AND LANE MARKINGS				
LUMP SUM ESTIMATE FOR SIGNS AND LANE MARKINGS	LS	1	\$50,000.00	\$50,000
MAST ARM FOR OVERHEAD SIGN (45') (PRE INSTALL FOR LAFAYETTE)	EA	2	\$40,000.00	\$80,000
SUBTOTAL SIGNS AND LANE MARKINGS				\$130,000
LANDSCAPING ITEMS				
RIGHT-OF-WAY FENCE, TYPE 1 (CHAIN LINK)	LF	270	\$50.00	\$13,500
TOPSOIL FURNISHED AND PLACED, 4" DEPTH	CY	2,000	\$70.00	\$140,000
SEEDING & SOIL SUPPLEMENTS INCLUDING MULCH	LB	639	\$75.00	\$47,925
TREES ON PARCEL 15-19	EA	30	\$400.00	\$12,000
SUBTOTAL LANDSCAPING ITEMS				\$213,425
STREETSCAPE ITEMS				
SUBTOTAL STREETSCAPE ITEMS				\$0
LIGHTING ITEMS				
RELOCATE STREET LIGHT MAST ARM & LUMINAIRE	EA	10	\$6,000.00	\$60,000
SUBTOTAL LIGHTING ITEMS				\$60,000
MISCELLANEOUS CONSTRUCTION ITEMS				
NON-QUANTIFIED ITEMS (5%)	LS	1	\$163,460	\$163,460
RETAINING WALL 490L X 19 H (AVG)	LS	1	\$4,399,063	\$4,399,063
BUILDING DEMOLITION	EACH	2	\$50,000	\$100,000
CB#0 BRIDGE REPLACEMENT 100.7W X 107.6L (see 6/11/24 estimate)	LS	1	\$12,627,648	\$12,627,648
CB#0 PECO GAS LINE	LS	1	\$400,000	\$400,000
CB#0 FUTURE FIBER OPTIC FACILITIES	LS	1	\$100,000	\$100,000
DB#116 BRIDGE REPLACEMENT 99W X 161L (see TS&L estimate)	LS	1	\$16,608,659	\$16,608,659
DB#116 PECO GAS LINE	LS	1	\$400,000	\$400,000

COST ESTIMATE

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DB#116 FUTURE FIBER OPTIC FACILITIES	LS	1	\$100,000	\$100,000
REMOVAL OF EXISTING BRIDGE CB#0	EACH	1	\$700,000	\$700,000
REMOVAL OF EXISTING BRIDGE DB-116	EACH	1	\$766,500	\$766,500
SIGNALIZATION RIDGE PK & RAMP A/D-EQUIPMENT-CONDUIT	LS	1	\$300,000	\$300,000
TSEPS (RAMP A)	LS	1	\$500,000	\$500,000
RAILROAD PROTECTIVE SERVICES	LS	1	\$500,000	\$500,000
SIGNAL MODIFICATION/INSTALLATION RIDGE PK & BELVOIR	LS	1	\$300,000	\$300,000
TEMP SIGNAL DURING CONSTR RIDGE PK & BELVOIR (2 STAGES)	LS	1	\$100,000	\$100,000
SIGNAL INTERCONNECT CONNECT RAMP A/D & BELVOIR AERIAL	LS	1	\$6,000	\$6,000
PENNDOT AERIAL FIBER RELOCATION SCHOOL TO BELVOIR	LS	1	\$30,000	\$30,000
CONSTRUCTION TRAILER (0.6%)	LS	1	\$300,000	\$300,000
EQUIPMENT PACKAGE (0.1%)	LS	1	\$50,000	\$50,000
SUBTOTAL MISCELLANEOUS ITEMS				\$38,451,331
SUBTOTAL PAY ITEMS				\$41,720,532
OTHER ITEMS				
MOBILIZATION (5.5%)	LS	1	\$2,294,629	\$2,294,629
MAINTENANCE AND PROTECTION OF TRAFFIC (8%) - INCLUDES PAY ITEMS	LS	1	\$3,337,643	\$3,337,643
CONSTRUCTION SURVEY & STAKEOUT (0.5%)	LS	1	\$208,603	\$208,603
POST CONSTRUCTION STORMWATER MANAGEMENT (0.5%)-AMMENDED SOIL	LS	1	\$208,603	\$208,603
EROSION AND SEDIMENTATION CONTROL (0.5%)	LS	1	\$208,603	\$208,603
SUBTOTAL OTHER ITEMS				\$6,258,080
TOTAL: PAY ITEMS + OTHER ITEMS				\$47,978,611
CB#0 AND DB#116 SUBTRACTED COSTS				(\$32,192,807)
APPROACH ROADWAY TOTAL: PAY ITEMS + OTHER ITEMS (SUBTRACT CB#0 and DB#116)				\$18,742,304
UNIT PRICE ESTIMATE INFLATION FACTOR (0%) JUNE 2022 TO JUNE 2024				\$0
TOTAL WITH INFLATION FACTOR				\$18,742,304
ESCALATION (3% FOR 2 YEARS)				\$966,836
ESTIMATE TO MID-CONSTRUCTION YEAR (2026)				\$16,842,640
CONTINGENCY (10% FOR MAJOR-COMPLEX 90%)				\$1,684,264
TOTAL ESTIMATE FOR APPROACH ROADWAY CONSTRUCTION*				\$18,526,904
CONSTRUCTION INSPECTION Estimated at 10% of Estimate for Construction	LS	1	\$1,852,690	\$1,852,690
TOTAL FOR CONSTRUCTION INCLUDING INSPECTION				\$20,379,595

* Does not include:

Surveying/Engineering/Permitting/Legal Fees

Acquisition Costs for Right-of-Way/Easements/Releases

Utility pole relocation, underground utility relocation/exploration

Note: Estimated number of utility poles to be relocated = 15

Construction Management

or any other items not specifically listed above.



SUBJECT Ridge Pike over Norfolk Southern, L-443

90% Cost Estimate

JOB NO. 231904972.02

DESIGN MAR DATE 6/10/2024

CHECK CEW DATE 6/10/2024

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Item Number	Description	Unit	Cost Calculation		
			Total on Tab	Cost Per Unit	Total Cost
8010-0001	Bridge Structure, As Designed, L-443 (1)	LS	-	-	\$ 12,627,648.00
(1)	Class 3 Excavation	CY	28197	\$ 45.00	\$ 1,268,865.00
(1)	Membrane Waterproofing System Installed on Other Surfaces	SY	293	\$ 70.00	\$ 20,510.00
(1)	Class AAAP Cement Concrete (2)	CY	466	\$ 2,200.00	\$ 1,025,200.00
(1)	Class AA Cement Concrete (3)(4)(5)(6)	CY	383	\$ 1,450.00	\$ 555,350.00
(1)	Class A Cement Concrete	CY	3340	\$ 825.00	\$ 2,755,500.00
(1)	Class C Cement Concrete	CY	196	\$ 225.00	\$ 44,100.00
(1)	Barrier Protective Fence, Vinyl-Coated Steel	LF	220	\$ 175.00	\$ 38,500.00
(1)	Prestressed Concrete Spread Box Beam, 48'x42"	LF	231	\$ 519.00	\$ 119,889.00
(1)	Prestressed Concrete Adjacent Box Beam, 48'x42"	LF	2533	\$ 519.00	\$ 1,314,627.00
(1)	Neoprene Strip Seal, 3.5" Movement	LF	144	\$ 350.00	\$ 50,400.00
(1)	Structure Backfill	CY	16919	\$ 100.00	\$ 1,691,900.00
(1)	Selected Borrow Excavation, Rock, Class R-4	CY	13	\$ 150.00	\$ 1,950.00
(1)	Selected Borrow Excavation, Rock, Class R-4 Grouted	CY	60	\$ 175.00	\$ 10,500.00
(1)	Protective Coating for Reinforced Concrete Surfaces (Penetrating Sealers, Bridge Superstructure)	SY	2495	\$ 10.00	\$ 24,950.00
(1)	Fabricated Structural Steel	LB	1348	\$ 4.00	\$ 5,392.00
AND	Reinforcement Bars, Epoxy Coated	LB	608768	\$ 2.50	\$ 1,521,920.00
1002-0052					
AND	Mechanical Splice System for No. 4 Bars, Epoxy Coated	EA	8	\$ 65.00	\$ 520.00
1002-0138					
AND	Mechanical Splice System for No. 5 Bars, Epoxy Coated	EA	123	\$ 55.00	\$ 6,765.00
1002-0140					
AND	Mechanical Splice System for No. 6 Bars, Epoxy Coated	EA	27	\$ 70.00	\$ 1,890.00
1002-0142					
AND	Mechanical Splice System for No. 7 Bars, Epoxy Coated	EA	30	\$ 120.00	\$ 3,600.00
1002-0146					
AND	Mechanical Splice System for No. 8 Bars, Epoxy Coated	EA	36	\$ 170.00	\$ 6,120.00
1002-0139					
AND	Mechanical Splice System for No. 11 Bars, Epoxy Coated	EA	36	\$ 200.00	\$ 7,200.00
1002-0158					
x	Pedestrian Crossing Facilities	LS	1	\$ 20,000.00	\$ 20,000.00
x	Temporary Support of Existing Structure	LS	1	\$ 350,000.00	\$ 350,000.00
9203-0101	Temporary Excavation Support and Protection System	LS	1	\$ 1,782,000	\$ 1,782,000.00



SUBJECT Ridge Pike over Norfolk Southern, L-443
90% Cost Estimate

JOB NO. 231904972.02

DESIGN MAR DATE 6/10/2024

CHECK CEW DATE 6/10/2024

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			Cost Calculation		
Item Number	Description	Unit	Total on Tab	Cost Per Unit	Total Cost
ROADWAY ITEMS					
1018-0001	Removal of Existing Bridge Structure	LS	1	\$ 700,000.00	\$ 700,000.00
9000-0001	Future Fiber Optic Facilities	LS	1	\$ 100,000.00	\$ 100,000.00
9000-0001	PECO Gas Line	LS	1	\$ 400,000.00	\$ 400,000.00