

Rosemary Chiavetta, Secretary
Public Utility Commission
400 North Street
Harrisburg, PA 17120

April 14, 2025

RE: **Application of Southeastern Transportation Authority (“SEPTA”)
For Rehabilitation of Bridges at DOT Crossing No. 592988Y (Easton Road) and
DOT Crossing No. 592989Y (Keswick Avenue) around Abington Township,
Montgomery County**
Application Docket No. _____

Dear Secretary Chiavetta,

I submit the enclosed application for approval of rehabilitation of (2) two crossings owned by SEPTA: one over Easton Road near Glenside Station (DOT Crossing No. 592988Y) and the other over Keswick Avenue (DOT Crossing No. 592989Y).

The scope of work for the Easton Road bridge includes removing and reinstalling station platforms, repairing and waterproofing the bridge deck, fixing the retaining wall, and performing concrete repairs on the stairs. For the Keswick Avenue bridge, the scope involves removing and replacing the existing superstructure, waterproofing the bridge deck, and conducting repair work on the abutment and wingwall.

Utility relocation is necessary only for the Keswick Avenue bridge. This process includes removing existing utility attachments from poles, digging a trench along the designated side of the road, placing a temporary split-duct conduit within the trench, and relocating existing wirelines into this conduit. There will be temporary disconnection and reconnection of utilities as needed throughout the Easton Road bridge and platform work. This will include disconnecting and reconnecting some lighting.

Very importantly, we are hoping to perform this work over Memorial Day weekend, that being May 24-26, 2025. This work will require full track outage and a shutdown of Keswick Avenue for about two (2) weeks. To the extent that timetable requires an expediated process by PUC, we request such process be initiated as soon as possible. These bridges have been identified as poorly rated structures that required significant rehabilitation to bring them into a state of good repair. We have carefully coordinated with all the interested parties to try to schedule times for this work, including the local municipality, and simply stated, this was the only time available in the near future that worked for everyone. If you have questions about this, please be sure to contact me to discuss this further.

All bridge work will be funded by SEPTA, excluding some work in relocating the utilities. The total cost for the project is estimated to exceed \$13,000,000.00.

A complete list of the enclosures with this letter is as follows:

1. Rail Safety Division Application – Type G;
2. Certificate of Service;
3. Bridge 11.62 Rehabilitation Civil Utility Plan (Appendix A)
4. Easton Road Bridge Temporary plan (Appendix B)
5. Bridge 11.83 over Easton Road Rehabilitation Drawing (Appendix C)
6. Bridge 11.62 over Keswick Ave Rehabilitation Drawing (Appendix D)

Sincerely,

A handwritten signature in blue ink, appearing to read 'Fuad Khan', is written over a horizontal line.

Fuad Khan, Senior Project Engineer
Southern Pennsylvania Transportation Authority
1234 Market Street, 13th Floor
Philadelphia, PA 19107
(215) 580 3332
mkhan@septa.org

Rail Safety Division Application Type (check all that apply):

- E-Installation, removal or substitution of warning devices at a public crossing
- G-Construction, alteration, relocation, abolition or suspension of a public crossing
- P-Installation of traffic signals with preemption on the approach to a public crossing
- W-Installation of a fixed utility company's facilities at a public crossing (Pipe & Wire)
- X-Exemption from the Commission's railroad clearance requirement

Application Docket No.
To Be Assigned

For Applications E/G/P/W use caption 1, for X use caption 2 (delete unused caption):

In re:

Application of Southeastern Transportation Authority ("SEPTA")
For Rehabilitation of Bridges at DOT Crossing No. 592988Y (Easton Road) and DOT Crossing No. 592989Y (Keswick Avenue) around Abington Township, Montgomery County

To Pennsylvania Public Utility Commission:

1. The name, address, telephone number, and e-mail address of the applicant are:

Southern Pennsylvania Transportation Authority (SEPTA)
Fuad Khan
Senior Project Engineer
1234 Market Street, 13th Floor
Philadelphia, PA 19107
(215) 580 3332
mkhan@septa.org

2. The name, address, telephone number, and e-mail address of the applicant's attorney are:

John P. Ciocco
Special Counsel - Corporate
1234 Market Street, 5th Floor
Philadelphia, PA 19107
2155807496
jciocco@septa.org

3. SEPTA is a body corporate and politic which exercises the public powers of the Commonwealth of Pennsylvania as an agency and instrumentality thereof. SEPTA requests approval for the rehabilitation of two (2) crossings owned by SEPTA: one over Easton Road near Glenside Station (DOT Crossing No. 592988Y) and the other over Keswick Avenue (DOT Crossing No. 592989Y).

4. The proposed alteration involves the rehabilitation of the abutments and the replacement of the superstructure of a SEPTA-owned bridge over Keswick Ave in Cheltenham and Abington Townships, Montgomery County, PA. The project will utilize accelerated bridge construction methods and is scheduled to occur over the memorial day weekend in 2025. The project also includes the removal and reinstallation of station platforms at Glenside station and concrete repairs for stairs, Bridge Deck repairs and waterproofing, repair of the retaining wall of another SEPTA owned bridge over Easton Road in Cheltenham Township, Montgomery County, PA.

Street Name: Keswick
DOT Crossing No: 592989Y
SEPTA Rail Line: Main Line
Municipality: Abington Township

Easton Road
DOT Crossing No. 592988Y
SEPTA Main Line
Cheltenham Twp

5. The parties that may be affected by the proposed alteration are as follows (see attached Certificate of Service):

The Pennsylvania Department of Transportation, Cheltenham Township, Abington Township, Montgomery County, PECO Energy Company, Comcast Cable communications management LLC, Crown Castle, Lumen, Pennsylvania Northeastern Railroad

6. N/A

7. The preliminary cost estimate for the SEPTA bridge rehabilitation project, encompassing Bridge 11.83 and Bridge 11.62, is approximately \$13.1 million. The proposed work for the bridge over Keswick Avenue (Bridge 11.62) is included within this total cost. The funding for this project is proposed to come from SEPTA's capital funding sources. The utility companies will pay their own share of the cost of relocating their utilities. The parties responsible for installing, maintaining, and inspecting any proposed facilities include Comcast Cable Communications Management, LLC, PECO Energy Company, Crowncastle, Lumen Network Implementation Program, Walsh Construction, and Johnson, Mirmiran & Thompson, Inc.

8. The application is necessary or proper for the following reason(s):

The application is necessary to facilitate construction work for the bridge rehabilitation work. These two bridges have been identified as poorly rated structures that require significant rehabilitation to bring them into a state of good repair.

Wherefore, we pray your Honorable Commission to approve the application:



(Signature of Applicant)

VERIFICATION

I, Fuad Khan, hereby state that the facts above set forth are true and correct (or are true and correct to the best of my knowledge, information and belief) and that I expect to be able to prove the same at a hearing held in this matter. I understand that the statements herein are made subject to the penalties of 18 Pa. C.S. § 4904 (relating to unsworn falsification to authorities).

04/14/2025



Date

(Signature)

CERTIFICATE OF SERVICE

I hereby certify that I have this day served a true copy of the foregoing document upon the participants, listed below, in accordance with the requirements of § 1.54 (*relating to service by participant*).

Mary Ann Lang

PennDOT District 6-0

7000 Geerdes Boulevard

King of Prussia, PA 19406

malang@pa.gov

K. Michael Anderson

PennDOT District 6-0

7000 Geerdes Boulevard

King of Prussia, PA 19406

kmanderson@pa.gov

Ashley McIlvaine

Township of Abington

1176 Old York Road

Abington, PA 19001

amcilvaine@AbingtonPA.gov

C. Pete Dettling

PECO Energy Company

Program Manager

1050 West Swedesford Road

Berwyn, PA 19312

Charles.Dettling@exeloncorp.com

Richard Williams

*Lumen Network Implementation Program
Manager*

5095 Ritter Road

Mechanicsburg, PA 17055

Richard.L.Williams@lumen.com

Brian Bonsall

Montgomery County PA

Brian.Bonsall@montgomerycountypa.gov

*Comcast Cable Communications
Management, LLC*

Attn: Americomm LLC

21 State Avenue, Suite 1

Carlisle, PA 17013

dmarsteller@americomm-llc.com

Claudia Wade

Crown Castle

1500 Corporate Drive

Canonsburg, PA 15317

Claudia.Wade@crowncastle.com

Abiy Abebe

PennDOT District 6-0

7000 Geerdes Boulevard

King of Prussia, PA 19406

aabebe@pa.gov

Peter Kuntz

Operations Manager

Pennsylvania Northeastern Railroad

pkuntz@pnrailroad.com



(Continues next page)

CERTIFICATE OF SERVICE

I hereby certify that I have this day served a true copy of the foregoing document upon the participants, listed below, in accordance with the requirements of § 1.54 (*relating to service by participant*).

Bradley Hanes

Managing Partner

PRECISION FIBER DESIGN, LLC

P.O. Box 11, Uwchland, PA 19480

m: 610.960.2574

bhanes@precisionfd.com

*Pennsylvania Department of Transportation
(PennDOT)*

ROW & Utility Division, Bureau of Design

P.O. Box 3362

Harrisburg, PA 17105-3362

Pennsylvania Department of Transportation

Office of Chief Counsel

P.O. 8212

Harrisburg, PA 17105-8212

Joe Stuckert

Cheltenham Township

8230 Old York Rd.

Elkins Park, Pa. 19027

jstuckert@cheltenhampa.gov

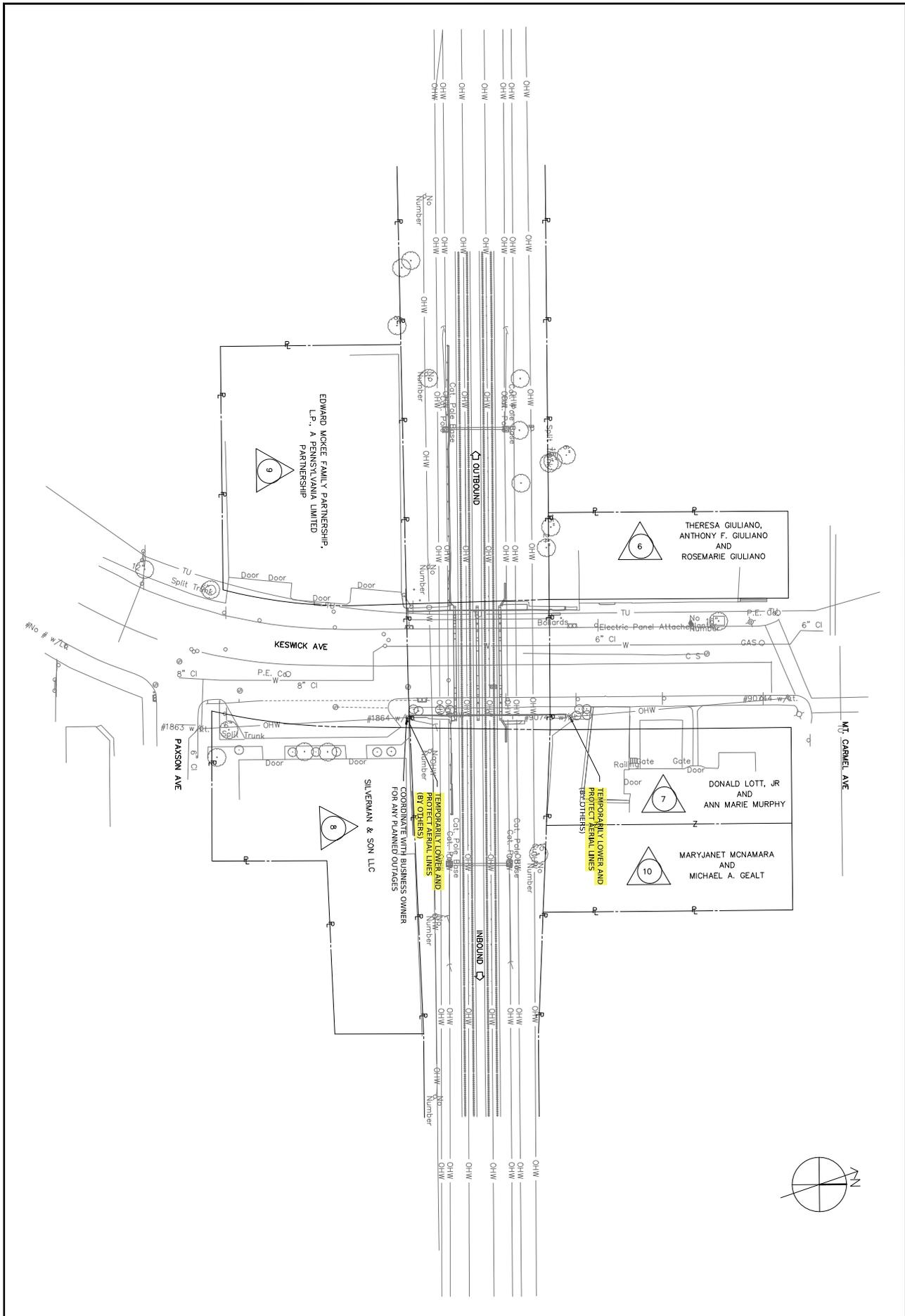
215-635-4600

Dated this 14 day of April ,20 25



(Signature)

APPENDIX A



JACOBS

INCORPORATED
 1000 MARKET STREET, SUITE 100
 PHILADELPHIA, PA 19102

BRIDGE 11.62 REHABILITATION
 CIVIL
 UTILITY PLAN

DATE: 7/26/2024
 TIME: 1:00:23 PM

PROJECT NO: C203

DATE: 7/26/2024

SCALE: 1" = 25'

DATE: 7/26/2024

PROJECT: BRIDGE 11.62 REHABILITATION

PROJECT NO: C203

DATE: 7/26/2024

TIME: 1:00:23 PM

REV	DATE	DESCRIPTION
01	7/26/24	60% SUBMISSION

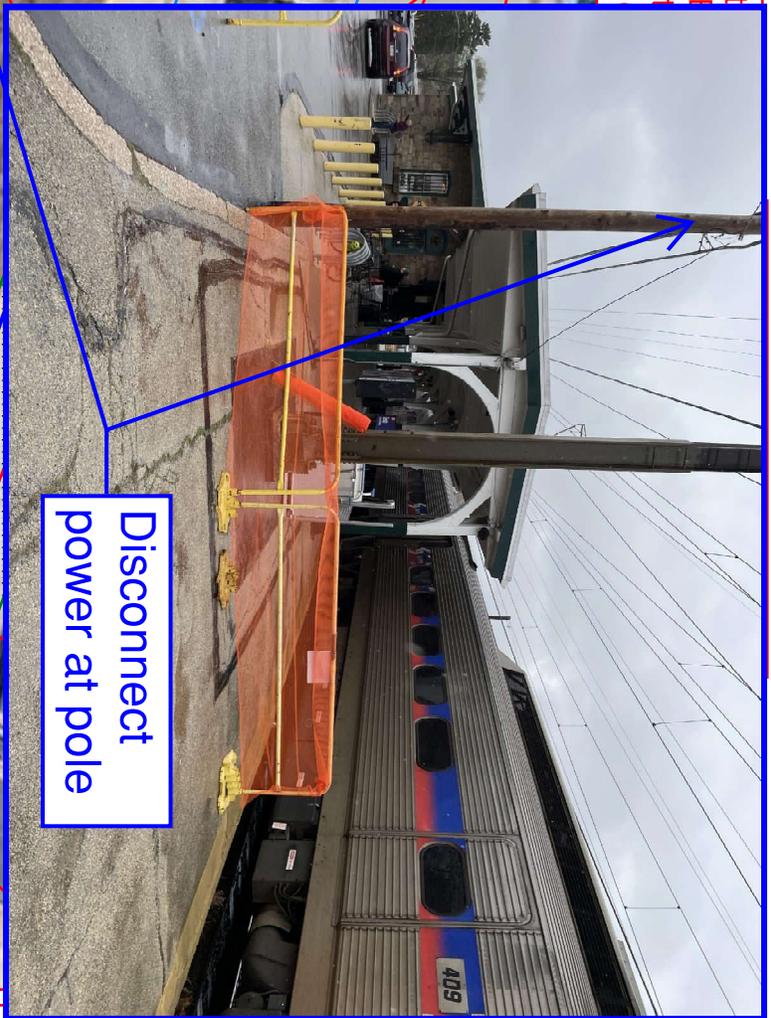
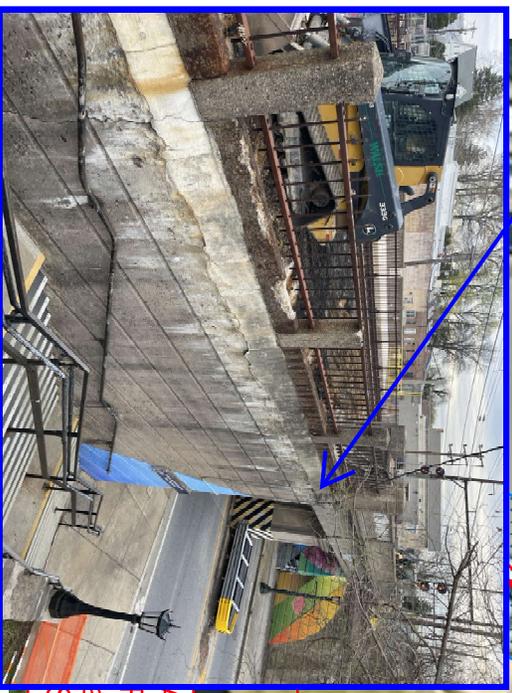
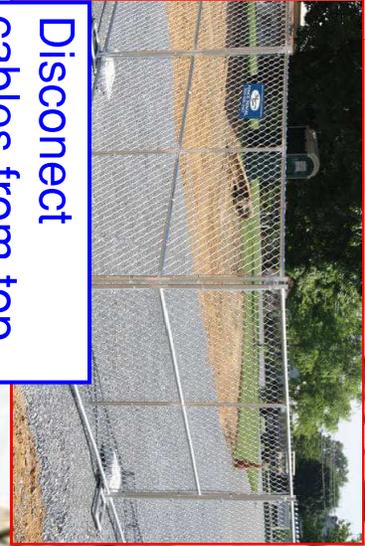
APPENDIX B

Additional area requested for storing of equipment and materials. Area to be fenced off.

Equipment Entering Existing Site. Must be Escorted to Protect Public



Disconnect cables from top of wall so it can be removed and replaced



Disconnect power at pole

Equipment Entering Existing Site. Equipment must be Escorted to Protect Public (Typical)

Area Cordoned off with Orange Safety Fencing for Construction and Material/Equipment Storage (Typical)



De-energize overhead power, light, and lighting below bridge

Protective Fencing in Orange Safety Area (Typical)



Remove and replace Platform (Typical)

APPENDIX C



Southwestern Pennsylvania
Transportation Authority

ENGINEERING, MAINTENANCE, AND CONSTRUCTION
BRIDGES & BUILDINGS

BRIDGE WATERPROOFING & SPALL REPAIRS RRD/MAINLINE MP# 11.83 OVER EASTON ROAD BRIDGE IMPROVEMENTS

SEPTA PROJECT NO.: N/A

FTA NO.: N/A

PENNDOT PROJECT NO.: N/A

LIST OF DRAWINGS

SHEET NO.	DRAWING NO.	GENERAL DRAWINGS
1	G101	COVER SHEET
2	G102	ABBREVIATIONS AND LEGEND

LIST OF DRAWINGS/ACT NO. NOTIFICATION LIST

SHEET NO.	DRAWING NO.	TRACK DRAWINGS
34	T101	TRACK 1 PLAN AND PROFILE
35	T102	TRACK 2 PLAN AND PROFILE
36	T103	TRACK NOTES, DETAILS, AND TYPICAL SECTIONS
37	T104	TRACK SHUNTING PLAN

ACT NO. NOTIFICATION LIST

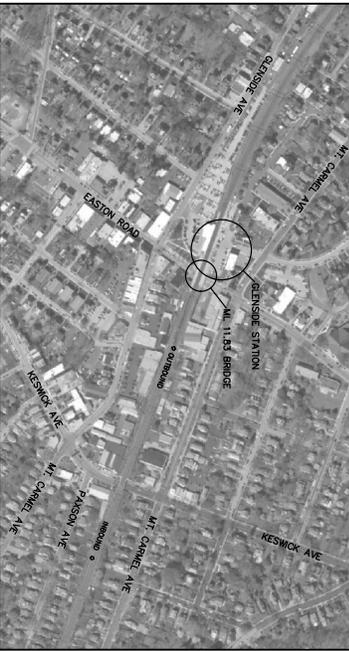
CONTACT PHONE LIST

CALL BEFORE YOU DIG !!
 (717) 838-1178
 (610) 486-1178
 PENNSYLVANIA ONE CALL SYSTEM, INC.
 1-800-242-1178

PURSENER SMITH
 215-500-8977



LOCATION



SHEET NO. DRAWING NO. ROADWAY DRAWINGS

10	R101	GENERAL NOTES - TRAFFIC
11	R102	SIEN FABRICATION DETAILS
12	R103	CONCRETE DETAIL PLAN
13	R104	PEDESTRIAN DETOUR PLAN

SHEET NO. DRAWING NO. STRUCTURAL DRAWINGS

14	S101	GENERAL PLAN AND ELEVATION
15	S102	GENERAL NOTES - STRUCTURAL (1 OF 2)
16	S103	GENERAL NOTES - STRUCTURAL (2 OF 2)
17	S104	CONCRETE DETAIL PLAN
18	S105	OUTBOUND PLATFORM
19	S106	PLATFORM CURB LAYOUT
20	S107	CONSTRUCTION STRAINING SECTIONS
21	S108	CONCRETE DETAIL PLAN
22	S109	WEST ABUTMENT PLAN AND ELEVATION
23	S110	WEST ABUTMENT PLAN AND WINGWALLS
24	S111	PIER PLAN AND ELEVATION
25	S112	EAST ABUTMENT PLAN AND ELEVATION
26	S113	BRIDGE PARASCIA AND BARRIER
27	S114	BRIDGE PARASCIA DETAIL PLAN
28	S115	REPAIR DETAILS - 2 OF 3
29	S116	REPAIR DETAILS - 3 OF 3
30	S117	REPAIR DETAILS - 4 OF 4
31	S118	PICTURE REFERENCE - 2 OF 4
32	S119	PICTURE REFERENCE - 3 OF 4
33	S120	PICTURE REFERENCE - 4 OF 4

ACT NO. NOTIFICATION LIST

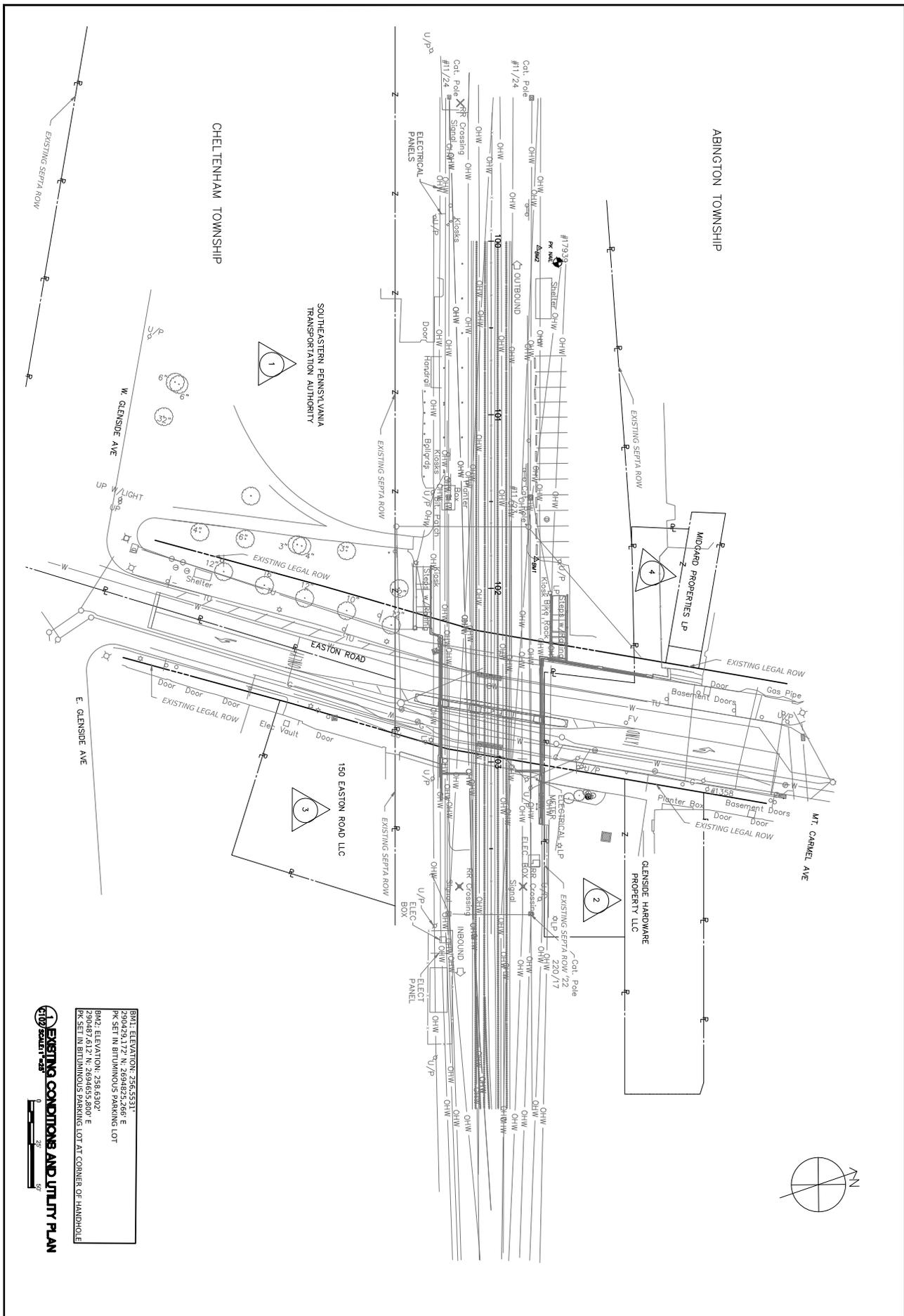
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37	NOTIFICATION

100% RESUBMISSION

MAINLINE BRIDGE MP 11.83 OVER EASTON ROAD
RRD MAINLINE, GLENSIDE, PA
BRIDGE 11.83 REHABILITATION
GENERAL COVER SHEET

NO SCALE
 OCT 2024
 G101

REV	DATE	DESCRIPTION
03	10/25/24	100% RESUBMISSION
02	10/22/24	100% SUBMISSION
01	10/17/24	60% SUBMISSION



EXISTING CONDITIONS AND UTILITY PLAN

BM1: ELEVATION: 256.5531'
 2904428.172' N; 2694825.266' E
 PK SET IN BITUMINOUS PARKING LOT

BM2: ELEVATION: 258.6302'
 2904487.612' N; 2694855.800' E
 PK SET IN BITUMINOUS PARKING LOT AT CORNER OF HANDHOLE

JACOBS

100% RESUBMISSION

DATE PRINTED: 10/24/2024 1:50:24 PM

BRIDGE 11.83 REHABILITATION

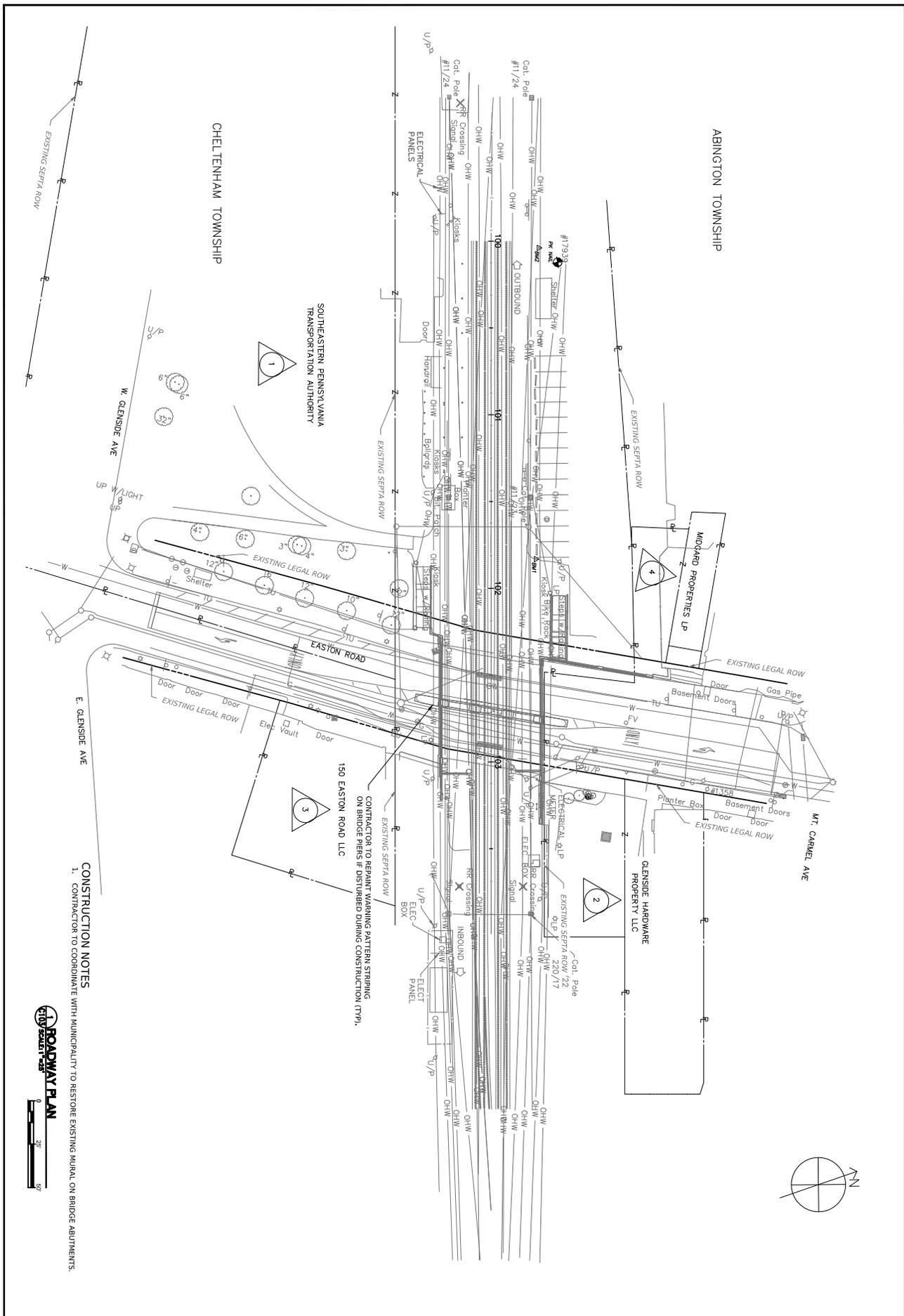
CIVIL

EXISTING CONDITIONS AND UTILITY PLAN

DATE: 10/24/2024

TIME: 1:50:24 PM

REV	DATE	DESCRIPTION
01	7/24/24	60% SUBMISSION
02	9/27/24	100% SUBMISSION
03	10/24/24	100% RESUBMISSION



CONSTRUCTION NOTES
 1. CONTRACTOR TO COORDINATE WITH MUNICIPALITY TO RESTORE EXISTING MURAL ON BRIDGE ABUTMENTS.



 PENNSYLVANIA DEPARTMENT OF TRANSPORTATION CHIEF ENGINEER JOHN J. MARRAS, P.E. 100 NORTH 30TH AVENUE PHILADELPHIA, PA 19104	Jacobs 1000 PENNSYLVANIA AVENUE PHILADELPHIA, PA 19106 TEL: 215-381-1000 WWW.JACOBS.COM	MAINLINE BRIDGE MP 11.83 OVER EASTON ROAD RRD MAINLINE, GLENSIDE, PA BRIDGE 11.83 REHABILITATION CIVIL ROADWAY PLAN	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <th>REV</th> <th>DATE</th> <th>DESCRIPTION</th> </tr> <tr> <td>01</td> <td>10/25/24</td> <td>100% RESUBMISSION</td> </tr> <tr> <td>02</td> <td>07/27/24</td> <td>100% SUBMISSION</td> </tr> <tr> <td>03</td> <td>07/26/24</td> <td>60% SUBMISSION</td> </tr> </table>	REV	DATE	DESCRIPTION	01	10/25/24	100% RESUBMISSION	02	07/27/24	100% SUBMISSION	03	07/26/24	60% SUBMISSION	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>DATE</td> <td>10/25/24</td> </tr> <tr> <td>SCALE</td> <td>1" = 25'</td> </tr> <tr> <td>PROJECT</td> <td>BRIDGE 11.83</td> </tr> <tr> <td>CONTRACT NO.</td> <td>C103</td> </tr> <tr> <td>CONTRACT DATE</td> <td>07/20/24</td> </tr> <tr> <td>CONTRACTOR</td> <td>111</td> </tr> <tr> <td>DESIGNER</td> <td>111</td> </tr> <tr> <td>CHECKER</td> <td>111</td> </tr> <tr> <td>DATE</td> <td>10/25/24</td> </tr> </table>	DATE	10/25/24	SCALE	1" = 25'	PROJECT	BRIDGE 11.83	CONTRACT NO.	C103	CONTRACT DATE	07/20/24	CONTRACTOR	111	DESIGNER	111	CHECKER	111	DATE	10/25/24
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MAINTENANCE AND PROTECTION OF TRAFFIC (MPT) GENERAL NOTES

1. THIS WORK CONSISTS OF THE MAINTENANCE OF TRAFFIC AND THE PROTECTION OF THE TRAVELING PUBLIC APPROACHING THE CONSTRUCTION AREA AND WITHIN THE LIMITS OF CONSTRUCTION.
2. FINISH, ERECT, PLACE, AND MAINTAIN TRAFFIC CONTROL SIGNS AND DEVICES. MAINTAIN TRAFFIC DURING HOURS OF CONSTRUCTION AND AT ALL OTHER TIMES CONSISTENT WITH THE METHODS INDICATED ON THESE DRAWINGS AND IN THE MOST CURRENT EDITIONS OF:
 - PENNDOT PUBLICATION 35, QUALIFIED PRODUCTS LIST FOR CONSTRUCTION (BULLETIN 151);
 - PA CODE, TITLE 67, CHAPTER 212, OFFICIAL TRAFFIC CONTROL DEVICES;
 - PENNDOT PUBLICATION 111, TRAFFIC CONTROL - PAVEMENT MARKINGS AND SIGNING STANDARDS;
 - PENNDOT PUBLICATION 213, TEMPORARY TRAFFIC CONTROL GUIDELINES;
 - PENNDOT PUBLICATION 236, HANDBOOK OF APPROVED SIGNS;
 - PENNDOT PUBLICATION 408, HIGHWAY CONSTRUCTION SPECIFICATIONS;
 - MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD);
3. IMMEDIATELY UPON COMPLETION OF THE WORK, REMOVE THE DEVICES.
4. ARRANGE FOR INSPECTION OF ALL TRAFFIC CONTROL DEVICES PRIOR TO START OF WORK.
5. THE TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF HIS OR HER RESPONSIBILITY AS SPECIFIED IN SECTION 901.2(a) OF PUBLICATION NO. 408/2020; LATEST REVISION.
6. ALL DISTANCES MAY BE ADJUSTED SLIGHTLY TO FIT FIELD CONDITIONS.
7. COVER OR REMOVE ALL CONFLICTING SIGNS OR SIGNS NOT IN USE.
8. MAINTAIN A MINIMUM LANE WIDTH OF 10 FEET.
9. INSTALL AND MAINTAIN TRAFFIC CONTROL SIGNS AND DEVICES THAT ARE IN NEW CONDITION THROUGHOUT THE DURATION OF THE PROJECT. ANY DAMAGE INCURRED SHALL BE IMMEDIATELY REPAIRED OR REPLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE REPRESENTATIVE.
10. LOCATE ALL SIGNS SO THAT SIGHT DISTANCES WILL NOT BE OBSTRUCTED AT DRIVERS AND LOCAL ROADS.
11. NOTIFY LOCAL EMERGENCY AUTHORITIES (E.G., POLICE, FIRE, MEDICAL, AFFECTED BUSINESSES, SCHOOL DISTRICTS), THE GENERAL PUBLIC, THE DISTRICT PERMIT MANAGER AND THE DISTRICT AUTOMATED PERMIT ROUTING AND ANALYSIS SYSTEM (APRAS) COORDINATOR AT LEAST FOURTEEN (14) DAYS PRIOR TO ANY SIGNIFICANT TRAFFIC IMPACTS (E.G., LATERAL WIDTH RESTRICTIONS LESS THAN 10 FEET) DETOURS.
12. RESTORE ALL TRAVEL LANES TO NORMAL TRAFFIC FLOW AT THE CLOSE OF EACH WORKING PERIOD UNDER SHORT-TERM CONDITIONS AND REMOVE ALL SHORT-TERM WORK ZONE TRAFFIC CONTROL SIGNING.
13. PROTECT ANY EXCAVATING OBSTRUCTIONS OR CONSTRUCTION WORK, SO AS NOT TO EXPOSE PEDESTRIANS TO HAZARDS.
14. TEMPORARILY COVER OR REMOVE ANY SIGNS THAT ARE IN CONFLICT WITH THE TEMPORARY TRAFFIC CONTROL MEASURES. DO NOT ATTACH TAPE OR ADHESIVE TO THE FACE OF ANY SIGNS THAT ARE TEMPORARILY COVERED DURING CONSTRUCTION OPERATIONS. ENSURE DURING THE REMOVAL AND STORAGE OF ALL SIGNS, THAT THE SHEETING FACE IS NOT DAMAGED OR SCRATCHED. REPLACE IN KIND ALL SIGNS AND/OR POSTS DAMAGED DURING REMOVAL, STORAGE, OR REINSTALLATION AT THE CONTRACTOR'S EXPENSE.
15. COMPLY WITH ACT 229 OF DECEMBER 2002 DURING CONSTRUCTION ACTIVITIES.
16. MAINTAIN ACCESS TO LOCAL BUSINESSES AT ALL TIMES.
17. COORDINATE WORK ZONE SIGNING AND OPERATIONS WITH OTHER PROJECTS IN THE AREA.
18. SEQUENCE OF CONSTRUCTION TO BE DETERMINED BY CONTRACTOR AND APPROVED BY THE REPRESENTATIVE PRIOR TO COMMENCING WORK. MAINTENANCE AND PROTECTION OF TRAFFIC DURING CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE APPLICABLE PATA DRAWINGS IN PENNDOT PUBLICATION 213.
19. POST THE SPECIAL SIGN THIS STREET TO BE CLOSED FOR MAINTENANCE (START DATE), TWO WEEKS PRIOR TO THE BEGINNING OF WORK. REMOVE THE SIGNS ONCE CONSTRUCTION BEGINS.
20. NOTIFY THE MUNICIPALITIES AT LEAST 14 DAYS PRIOR TO STARTING EACH PHASE OF WORK AND WITH ANY OTHER ROAD OR SIDEWALK RESTRICTIONS TO ALLOW PUBLIC NOTICE THROUGH THEIR WEBSITE AND NEWSLETTER.
21. NOTIFY SEPTA TWO WEEKS IN ADVANCE OF THE DISCONTINUANCE OF THE EXISTING BUS STOP LOCATIONS, BASED ON ANY PENDING CONSTRUCTION ACTIVITIES THAT WOULD BLOCK ACCESS. ALSO, NOTIFY SEPTA TWO WEEKS IN ADVANCE WHEN NEW STOPS ARE AVAILABLE TO USE.
CONTACT: MICHAEL DAGOSTINO, MDAGOSTINO@SEPTA.ORG
22. THESE PLANS ARE NOT INTENDED TO RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY FOR THE PROTECTION OF THE PUBLIC AND THE CONSTRUCTION PERSONNEL AS SPECIFIED IN SECTION 901.3(A) OF PENNDOT PUBLICATION 408. THE STANDARDS DESCRIBED ARE A MINIMUM AND ADDITIONAL PROTECTION MAY BE NEEDED IF PROBLEMS ARE ENCOUNTERED DURING THE TERM OF THE CONTRACT. CONSISTENTLY REVIEW THIS PLAN FOR ITS ADEQUACY AND RECOMMEND CHANGES FOR DEPARTMENT AND MUNICIPAL APPROVAL WHEN INADEQUACIES ARE DISCOVERED.
23. CONTRACTOR CANNOT IMPLEMENT DETOUR FOR BRIDGES ML 11.62 AND 11.83 CONCURRENTLY.



STATE OF PENNSYLVANIA
DEPARTMENT OF TRANSPORTATION
CONTRACT NO. 11-11-00000
PROJECT NO. 11-11-00000



JACOBS
1000 PENNSYLVANIA AVENUE
SUITE 1000
PHILADELPHIA, PA 19106
TEL: 215-381-1000
WWW.JACOBS.COM

DATE: 10/25/2024
TIME: 1:19:44 PM

MAINLINE BRIDGE MP 11.83 OVER EASTON ROAD
RRD MAINLINE, GLENSIDE, PA

BRIDGE 11.83 REHABILITATION
ROADWAY

GENERAL NOTES - TRAFFIC

NO.	DATE	DESCRIPTION
01	10/25/24	60% SUBMISSION
02	10/27/24	100% SUBMISSION
03	10/28/24	100% RESUBMISSION

SCALE: 1" = 4'

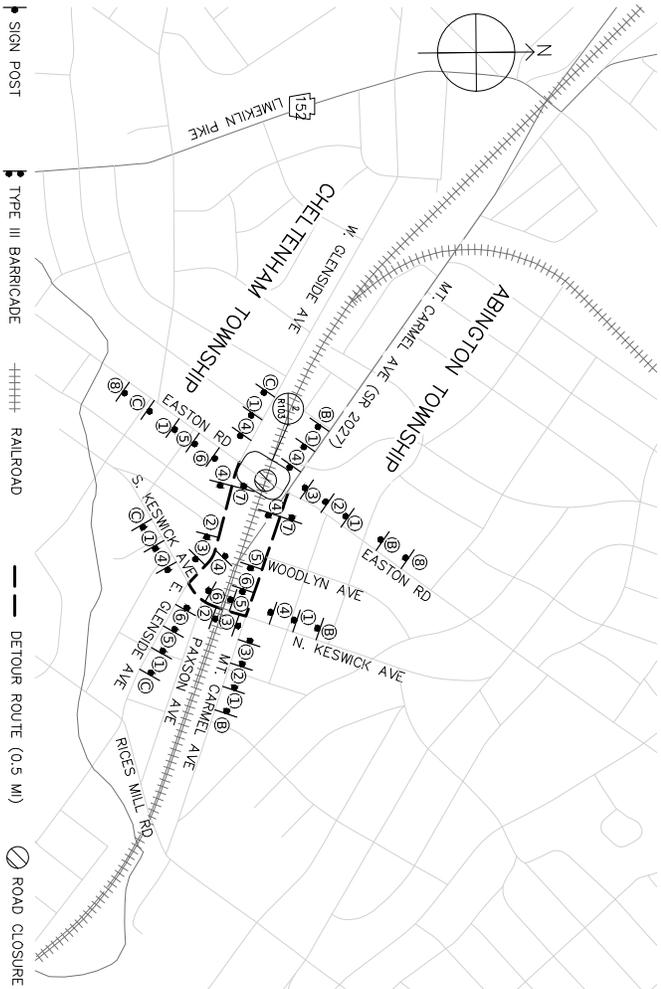
DATE: OCT 2024

PROJECT NO: 11-11-00000

CONTRACT NO: 11-11-00000

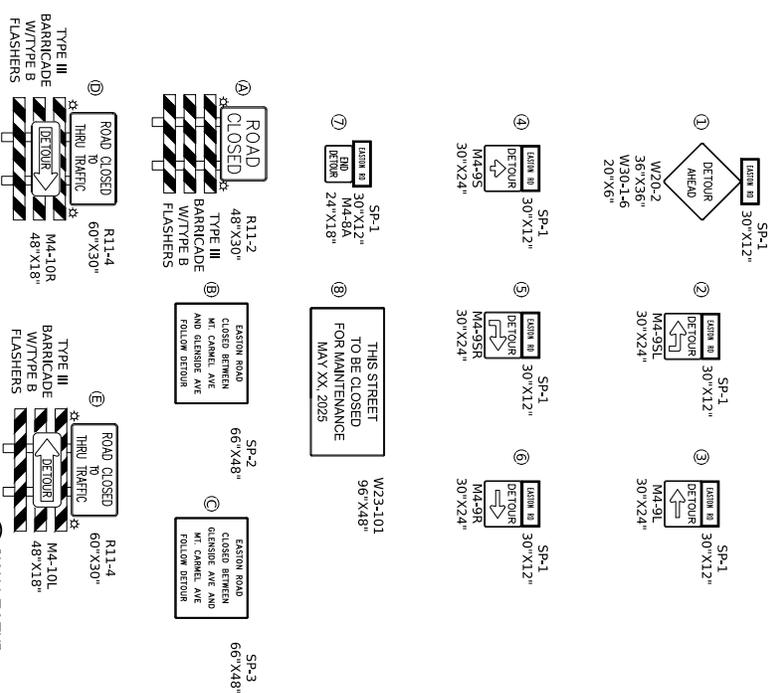
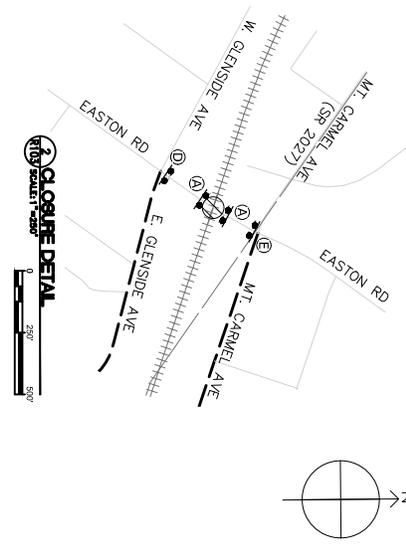
R101

03



NOTES:

1. NOTIFY ABRINGTON AND CHEL TENNHAM TOWNSHIPS, PENNDOT DISTRICT 6A, EMERGENCY SERVICES PROVIDERS AND LOCAL SCHOOL DISTRICTS A MINIMUM OF TWO (2) WEEKS PRIOR TO THE IMPLEMENTATION OF THIS DETOUR.
2. MAINTAIN ACCESS TO ALL BUSINESSES AND RESIDENCES FOR THE ENTIRE DURATION OF THE DETOUR.
3. THE DETOUR, RESTRAIN ACCESS TO PLATONR STAIRS AT ALL TIMES.
4. WHEN IT IS NOT FEASIBLE TO MAINTAIN PEDESTRIAN CONNECTION BETWEEN INBOUND AND OUTBOUND STAIRS, UTILIZE PEDESTRIAN DETOUR.
5. COORDINATE DETOUR PLAN WITH AREA EVENTS INCLUDING, BUT NOT LIMITED TO, THE GLENSIDE FOOD TRUCK FESTIVAL (SEPTEMBER) AND WINTERFEST.
6. COORDINATE DETOUR PLAN WITH OTHER LOCAL PROJECTS, CONTACT TOWNSHIPS, MONTGOMERY COUNTY, AND/OR PENNDOT FOR A LIST OF PLANNED PROJECTS.
7. USE SUFFICIENT NUMBER OF TYPE III BARRICADES TO CLOSE OFF ENTIRE ROAD.
8. SIGNAGE SHOULD BE AT LEAST 7 WEEKS IN ADVANCE OF CLOSURE. REMOVE SIGN AT START OF CLOSURE.
9. SIGN LOCATIONS ARE APPROXIMATE. PLACE SIGNS IN ACCORDANCE WITH PENNDOT PUBLICATION 213.



JACOBS

1000 MARKET STREET, SUITE 200
PHILADELPHIA, PA 19102

VEHICULAR DETOUR PLAN

DATE: 10/25/2024

PROJECT: BRIDGE 11.83 REHABILITATION ROADWAY

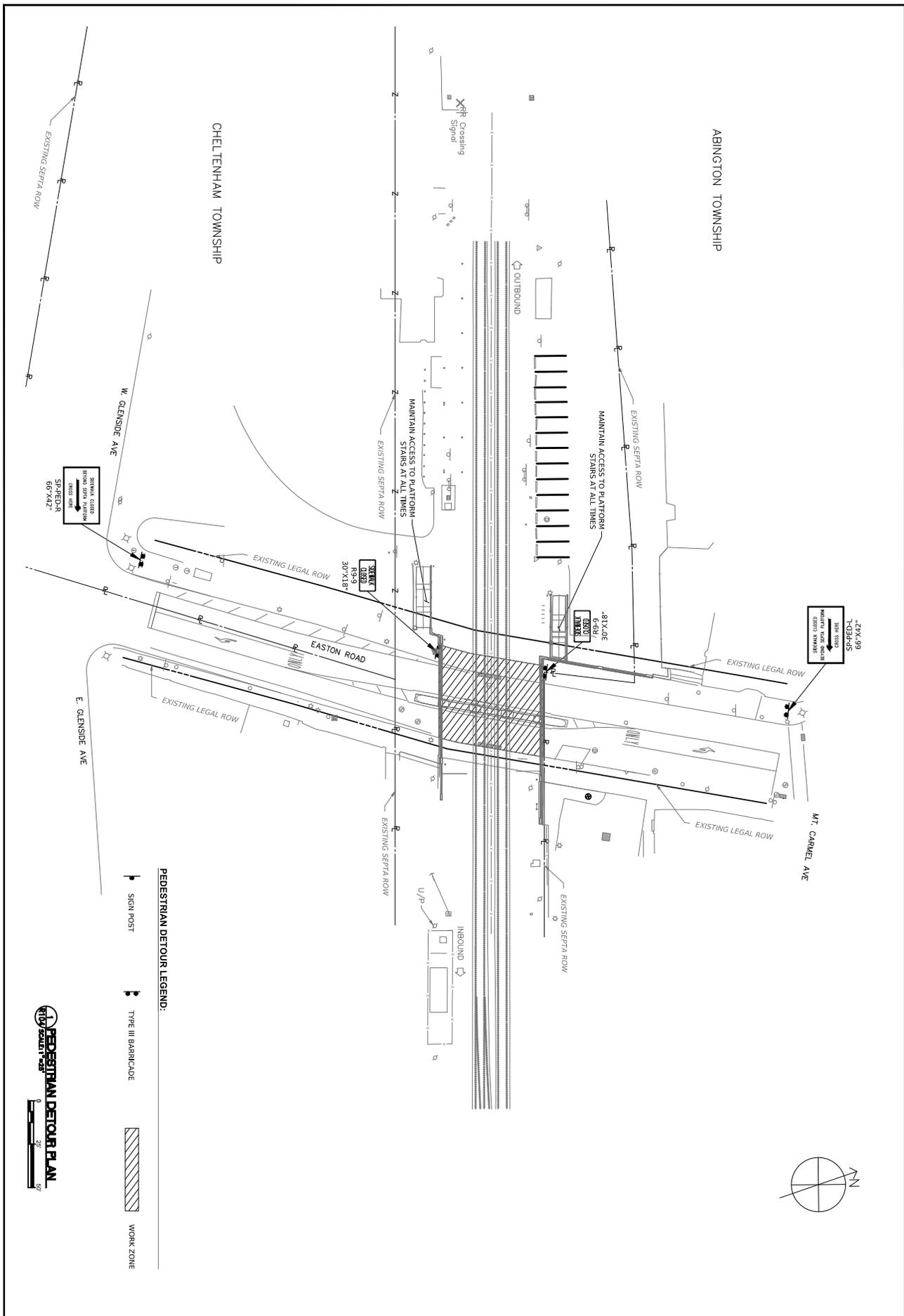
LOCATION: MAINLINE BRIDGE MP 11.83 OVER EASTON ROAD, RRD MAINLINE, GLENSIDE, PA

REV	DATE	DESCRIPTION
01	10/25/24	60% SUBMISSION
02	10/25/24	100% SUBMISSION
03	10/25/24	100% RESUBMISSION

PROJECT NO: R103

DATE: 10/25/24

SCALE: 1"=100'



PEDESTRIAN DETOUR LEGEND:

- SIGN POST
- TYPE III BARRICADE
- WORK ZONE

PEDESTRIAN DETOUR PLAN
 R104
 10/25/2024

<p>JACOBS 1000 MARKET STREET, SUITE 200 PHILADELPHIA, PA 19102 TEL: 215 381 1000 WWW.JACOBS.COM</p>	<p>JACOBS 1000 MARKET STREET, SUITE 200 PHILADELPHIA, PA 19102 TEL: 215 381 1000 WWW.JACOBS.COM</p>	<p>JACOBS 1000 MARKET STREET, SUITE 200 PHILADELPHIA, PA 19102 TEL: 215 381 1000 WWW.JACOBS.COM</p>	<p>JACOBS 1000 MARKET STREET, SUITE 200 PHILADELPHIA, PA 19102 TEL: 215 381 1000 WWW.JACOBS.COM</p>	<p>JACOBS 1000 MARKET STREET, SUITE 200 PHILADELPHIA, PA 19102 TEL: 215 381 1000 WWW.JACOBS.COM</p>	<p>JACOBS 1000 MARKET STREET, SUITE 200 PHILADELPHIA, PA 19102 TEL: 215 381 1000 WWW.JACOBS.COM</p>												
<p>MAINLINE BRIDGE MP 11.83 OVER EASTON ROAD RRD MAINLINE, GLENSIDE, PA BRIDGE 11.83 REHABILITATION ROADWAY PEDESTRIAN DETOUR PLAN</p>																	
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REV	DATE	DESCRIPTION															
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APP'D	CHKD	BY	DATE														
<p>100% RESUBMISSION DATE PRINTED: 10/25/2024 1:19:45 PM C:\PWORKING\JACOBS_B\MD\MARCHESANO\00560101E_SEPTA_R100.DWG</p>																	

GENERAL STRUCTURAL NOTES

6. ANCHORAGE TO CONCRETE:

- ANCHOR ROSS
 - ANCHOR ROSS SHALL BE ASTM F1554 GRADE 55 W/31 SUPPLEMENT, UNLESS NOTED OTHERWISE. FINISH ANCHOR ROSS PREPARED WITH MORTAR DOUBLE REBAR NUTS. ANCHOR ROSS SHALL BE SET IN CONCRETE. FINISH ANCHOR ROSS SHALL BE HOT DIPPED GALVANIZED IN ACCORDANCE WITH ASTM A153 OR STAINLESS STEEL ASTM F939 GROUP 3 (TYPE 316).
 - PLATE WELDERS SHALL BE WELDED ALL AROUND WHERE AISC OVERSIZED HOLES ARE USED IN BASE PLATES.
 - ANCHOR ROSS SHALL HAVE SUFFICIENT LENGTH TO PROVIDE THE MINIMUM EMBEDMENT SPACING ON THE DRAWINGS.
 - ANCHOR ROSS SHALL BE WELDED TO THE CONCRETE SURFACE WITH EPOXY RESIN. THE EPOXY RESIN SHALL BE PERFORMED PER THE ANCHORAGE REQUIREMENTS OF THE AISC BUILDING CODE REQUIREMENTS FOR STRUCTURAL.
 - ANCHOR ROSS INSTALLATION SHALL BE DOCUMENTED WITH REPORTING AND DRAWING AFTER BASE INSTALLATION.
 - ANCHOR ROSS SHALL BE INSTALLED BY A TOL-CHECKER CONDITION UNLESS NOTED OTHERWISE.
 - NO REPAIR OF BOND OF THE ANCHOR ROSS IS PERMITTED.
- NON-SINKING GROUT
 - NON-SINKING GROUT SHALL BE ORDERED FROM APPROVED SUPPLIER. GROUT SHALL BE NON-SETTABLE, NON-SINKING, NON-SHRINKABLE GROUT COMPANION TO ASTM C1109. GROUT SHALL HAVE A SPECIFIED MINIMUM COMPRESSIVE STRENGTH OF 8,000 PSI AT 5 DAYS. PRE-CURING OF BASE PLATES SHALL NOT BE PERMITTED.

POST-INSTALLED ANCHORAGE TO CONCRETE

- ANCHOR CAPACITY USED IN DESIGN SHALL BE BASED ON THE TECHNICAL DATA PROVIDED IN THE ICC APPROVAL REPORT. SUBSTITUTION REQUESTS MUST INCLUDE ICC ESR REPORT SHOWING COMPLIANCE WITH RELEVANT BUILDING CODE AND REASON PROSE TO USE. THE DESIGN OF POST-INSTALLED ANCHORS OR EMBEDDING BARS TO CONCRETE SHALL BE PERFORMED PER THE ANCHORAGE REQUIREMENTS OF THE AISC BUILDING CODE REQUIREMENTS FOR STRUCTURAL.
- INSTALL ANCHORS PER THE MANUFACTURER'S INSTRUCTIONS, AS INCLUDED IN THE ANCHOR PRODUCT.
- ANCHOR CAPACITY IS DEPENDENT UPON SPACING BETWEEN CONCRETE ANCHORS AND EDGE DISTANCE. INSTALL ANCHORS IN ACCORDANCE WITH THE MANUFACTURER'S INSTRUCTIONS.
- CONCRETE REPAIRING BARS IN THE STRUCTURE WILL CONFLICT WITH ANCHOR LOCATIONS. UNLESS NOTED OTHERWISE ON THE DRAWINGS, THE CONTRACTOR SHALL LOCATE THE POSITION OF THE EXISTING REINFORCING BARS AND AVOID COLLISION.

EXCEPT WHERE INDICATED ON THE DRAWINGS, POST-INSTALLED ANCHORS SHALL CONSIST OF THE FOLLOWING ANCHOR TYPES, OR STEEL ANCHOR TYPES (TYPE 316):

DRILLED-IN STEEL ANCHORS INTO CONCRETE

- HEAT TREAT M60-62 STEEL ANCHORS PER ICC ESR-3027
- SIMPSON TITAN HD STEEL ANCHOR PER ICC ESR-2713

DRILLED-IN EXPANSION ANCHORS INTO CONCRETE

- HEAT TREAT M60-62 STEEL ANCHORS PER ICC ESR-3197
- SIMPSON STRONG BOLT 2 WEDGE ANCHOR PER ICC ESR-3037

ADHESIVE ANCHORS AND REBAR DOWNDOWNS INTO CONCRETE

- HEAT TREAT 2007 ADHESIVE ANCHOR SYSTEM WITH HEAT TREAT ROD PER ICC ESR-3187
- SIMPSON SET-UP ADHESIVE ANCHORING SYSTEM PER ICC ESR-2698

POST-INSTALLED REBAR DOWNDOWNS INTO CONCRETE

- THE DESIGN OF STRONG POST-INSTALLED REINFORCING BARS SHALL BE PERFORMED PER THE DEVELOPMENT AND STRESS REQUIREMENTS OF THE AISC BUILDING CODE REQUIREMENTS FOR STRUCTURAL CONCRETE.
- ANCHORS IN CONCRETE ELEMENTS (AC 308), TABLE 3.8, ICC-ES EVALUATION SERVICE REPORT SHOWING COMPLIANCE WITH BE AS REQUIRED.
- POST-INSTALLED REINFORCING BAR INSTALLATION SHALL BE PERFORMED PER THE MANUFACTURER'S PRINTED INSTALLATION INSTRUCTIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING THE NECESSARY PERMITS AND OBTAINING DOCUMENTATION OF INSTALLER CERTIFICATION PRIOR TO COMMENCEMENT OF ANCHOR INSTALLATION.
- THE POSITION OF EXISTING REINFORCING BARS IN THE CONCRETE STRUCTURE SHALL BE LOCATED PRIOR TO POST-INSTALLING REBAR. EXISTING REBAR SHALL BE LOCATED USING GRAVITY, X-RAY, CHIPPING OR OTHER APPROVED MEANS.
- MECHANICALLY REINFORCED EXISTING CONCRETE SURFACE TO A MINIMUM AMPLITUDE OF 1/4 INCH.
- EXCEPT WHERE INDICATED ON THE DRAWINGS, POST-INSTALLED REBAR SHALL CONSIST OF THE FOLLOWING EPOXY SYSTEMS:
 - HEAT TREAT 2007 PER ICC ESR-3187
 - SIMPSON SET-UP ADHESIVE ANCHORING SYSTEM PER ICC ESR-2698

14. SPECIAL INSPECTIONS

- THESE INSPECTIONS DO NOT RELIEVE THE CONTRACTOR OF HIS OR HER RESPONSIBILITIES TO CARRY OUT HIS OWN QUALITY CONTROL INSPECTIONS AND TESTING.
- STATEMENT OF STRUCTURAL SPECIAL INSPECTION
 - SPECIAL INSPECTION MUST CONFORM TO CHAPTER 17 OF THE BC.
 - DEFEND THE REQUIREMENTS OF THE BC.
 - STRUCTURAL OBSERVATION FOR SEISMIC RESISTANCE IS NOT REQUIRED PER BC SECTION 1704A.1.
 - STRUCTURAL OBSERVATION FOR WIND RESISTANCE IS NOT REQUIRED PER BC SECTION 1704A.2.
 - SPECIAL INSPECTION FOR WIND RESISTANCE IS NOT REQUIRED PER BC SECTION 1706.11.
 - SPECIAL INSPECTIONS ARE REQUIRED FOR THE FOLLOWING MATERIALS AND INSTALLATIONS:
 - CONCRETE 2018 BC 1706.3

1.5. WORKS & MATERIALS

- THE CONTRACTOR SHALL MINIMIZE DISTURBANCE TO EXISTING WORKS. UPON COMPLETION OF CONCRETE REPAIR WORK, THE CONTRACTOR SHALL REPAIR AND PROTECT EXISTING WORKS TO ORIGINAL CONDITION. THE CONTRACTOR SHALL PROVIDE ACCESS TO THE CONCRETE SURFACES FOR PAINTING.

REPAIR QUANTITY TABLE

ITEM NO.	DESCRIPTION	REPAIR TYPE	QTY
S-1	CONCRETE SPALL	CENTER STEP 1 - STAIR REPAIR	6 SF
S-2	CONCRETE SPALL	UPPER STEP 1 - STAIR REPAIR	6 SF
S-3	CONCRETE SPALL	UPPER STEP 10 - STAIR REPAIR	6 SF
S-4	CONCRETE SPALL	TOP LANDING - TYPE 1 SMALL REPAIR	6 SF

ITEM NO.	DESCRIPTION	REPAIR TYPE	QTY
S-1	CONCRETE SPALL	LOWER STEP 4 LEFT HALF - STAIR REPAIR	4 SF
S-2	CONCRETE SPALL	LOWER STEP 5 RIGHT HALF - STAIR REPAIR	4 SF
S-3	CONCRETE SPALL	LOWER STEP 6 - STAIR REPAIR	6 SF
S-4	CONCRETE SPALL	LOWER STEP 7 RIGHT HALF - STAIR REPAIR	4 SF
S-5	CONCRETE SPALL	CENTER STEP 1 - STAIR REPAIR	6 SF
S-6	CONCRETE SPALL	CENTER STEPS 5, 6 & 7 - STAIR REPAIR	24 SF
S-7	CONCRETE SPALL	CENTER STEP 10 - STAIR REPAIR	6 SF
S-8	CONCRETE SPALL	UPPER LANDING - TYPE 1 SMALL REPAIR	6 SF
S-9	CONCRETE SPALL	UPPER STEP 1 - STAIR REPAIR	6 SF
S-10	CONCRETE SPALL	UPPER STEP 5 - STAIR REPAIR	6 SF
S-11	CONCRETE SPALL	UPPER STEP 8 - STAIR REPAIR	6 SF
S-12	CONCRETE SPALL	UPPER STEP 11 - STAIR REPAIR	6 SF
S-13	CONCRETE SPALL	TOP LANDING - TYPE 1 SMALL REPAIR	6 SF

ITEM NO.	DESCRIPTION	REPAIR TYPE	QTY
S-1	CONCRETE SPALL	NORTHWEST CORNER - TYPE 2 SMALL REPAIR	3.4 SF
S-2	CONCRETE SPALL	SOUTHWEST CORNER - TYPE 2 SMALL REPAIR	1.2 SF
R-1	BARRETT REPLACEMENT	SOUTHWEST W/ & BARRETT - REPLACEMENT	37 LF
C-1	VERTICAL CRACK	NORTHWEST CORNER BARRETT - CRACK REPAIR	3.0 LF
C-2	VERTICAL CRACK	SOUTHWEST W/ - CRACK REPAIR	8 LF

ITEM NO.	DESCRIPTION	REPAIR TYPE	QTY
S-1	CONCRETE SPALL	COLUMN 1 - TYPE 2 SMALL REPAIR	96 SF
S-2	CONCRETE SPALL	COLUMN 2 - TYPE 2 SMALL REPAIR	112 SF
S-3	CONCRETE SPALL	COLUMN 3 - TYPE 2 SMALL REPAIR	112 SF
S-4	CONCRETE SPALL	COLUMN 4 - TYPE 2 SMALL REPAIR	96 SF
S-5	CONCRETE SPALL	EAST FACE - TYPE 2 SMALL REPAIR	4 SF
S-6	CONCRETE SPALL	EAST FACE - TYPE 2 SMALL REPAIR	4 SF
S-7	CONCRETE SPALL	NORTH NOSE - TYPE 2 SMALL REPAIR	7.3 SF
S-8	CONCRETE SPALL	WEST FACE - TYPE 2 SMALL REPAIR	4.3 SF
S-9	CONCRETE SPALL	CORNER/TOPPING - MASON REPAIR	196 SF
C-1	VERTICAL CRACK	EAST FACE COLUMN 1 - CRACK REPAIR	4.0 LF
C-2	VERTICAL CRACK	EAST FACE COLUMN 2 - CRACK REPAIR	2.1 LF
C-3	VERTICAL CRACK	EAST FACE COLUMN 3 - CRACK REPAIR	5.1 LF
C-4	VERTICAL CRACK	EAST FACE COLUMN 4 - CRACK REPAIR	2.1 LF
C-5	VERTICAL CRACK	EAST FACE RIGHT REPT - CRACK REPAIR	4.1 LF
C-6	VERTICAL CRACK	WEST FACE COLUMN 1 - CRACK REPAIR	4.1 LF
C-7	VERTICAL CRACK	WEST FACE COLUMN 2 - CRACK REPAIR	4.1 LF
C-8	VERTICAL CRACK	WEST FACE CENTER ARCH - CRACK REPAIR	2 LF
C-9	VERTICAL CRACK	WEST FACE COLUMN 3 - CRACK REPAIR	4 LF
C-10	VERTICAL CRACK	WEST FACE COLUMN 4 - CRACK REPAIR	4 LF

ITEM NO.	DESCRIPTION	REPAIR TYPE	QTY
S-1	CONCRETE SPALL	TYPE 2 SMALL REPAIR	562 SF

REPAIR NOTES:

- CONTRACTOR TO REMOVE DEFECTIVE CONCRETE UNTIL SOUND CONCRETE IS ESTABLISHED. QUANTITIES AND REPAIR TYPE NEED TO BE REASSESSED BY THE CONTRACTOR AFTER REMOVAL OF DEFECTS.
- REPAIR SHALL BE PERFORMED PER THE REPAIR TYPE. SEE DWGS 5109 TO 5113.
- FOR REPAIR DETAILS SEE DWGS 5114 TO 5116.

100% SUBMISSION

DATE PRINTED: 10/23/2024 11:16:14 AM

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100% SUBMISSION

DATE: 10/23/2024

MAINLINE BRIDGE MP 11.83 OVER EASTON ROAD

RRD MAINLINE, GLENSIDE, PA

BRIDGE 11.83 REHABILITATION

STRUCTURAL

GENERAL NOTES - STRUCTURAL (2 OF 2)

100% SUBMISSION

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100% SUBMISSION

DATE: 10/23/2024

MAINLINE BRIDGE MP 11.83 OVER EASTON ROAD

RRD MAINLINE, GLENSIDE, PA

BRIDGE 11.83 REHABILITATION

STRUCTURAL

GENERAL NOTES - STRUCTURAL (2 OF 2)

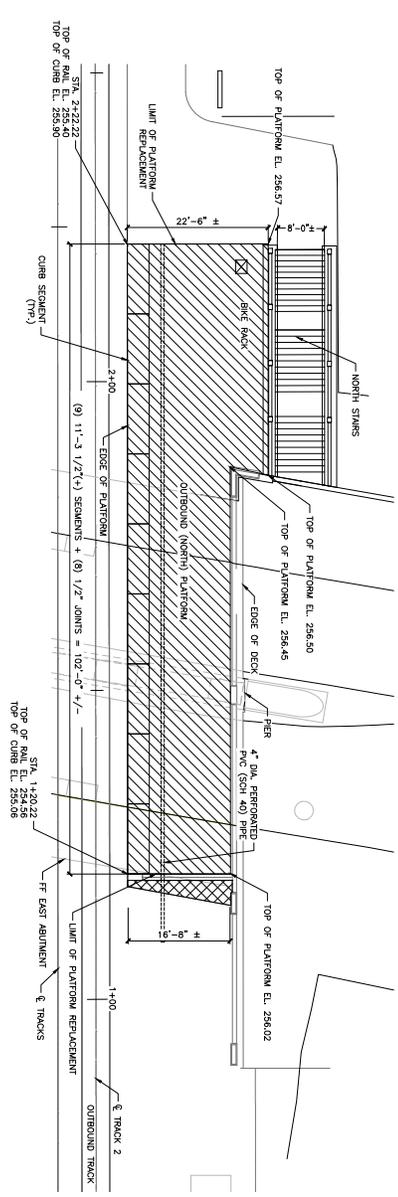
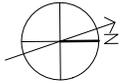
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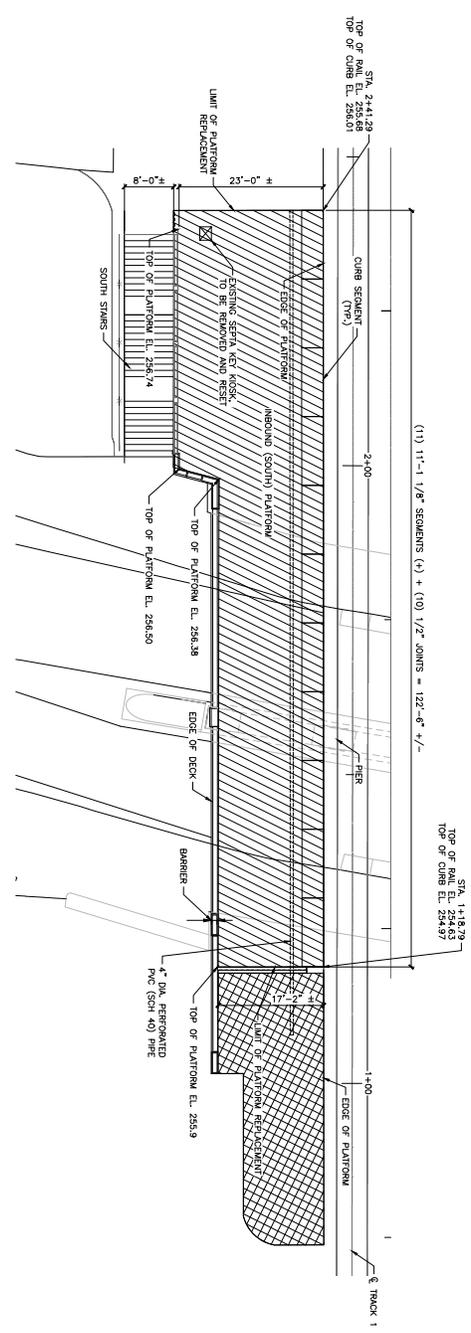
JACOBS

100% SUBMISSION

DATE: 10/23/2024



1
OUTBOUND PLATFORM - CURB SEGMENT LAYOUT
SCALE: 1"=10'



2
INBOUND PLATFORM - CURB SEGMENT LAYOUT
SCALE: 1"=10'

100% SUBMISSION

DATE PRINTED: 10/23/2024 11:16:37 AM

REV	DATE	DESCRIPTION
01	7/26/24	60% SUBMISSION
02	9/27/24	100% SUBMISSION
03	10/25/24	100% RESUBMISSION

MAINLINE BRIDGE MP 11.83 OVER EASTON ROAD
RRD MAINLINE, GLENSIDE, PA

BRIDGE 11.83 REHABILITATION
STRUCTURAL

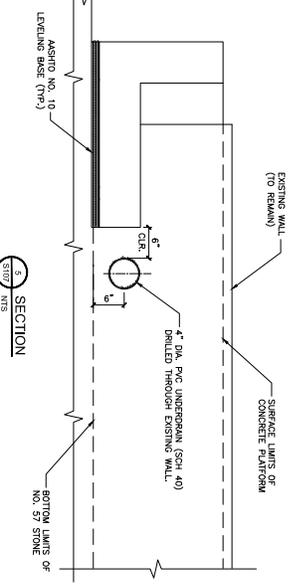
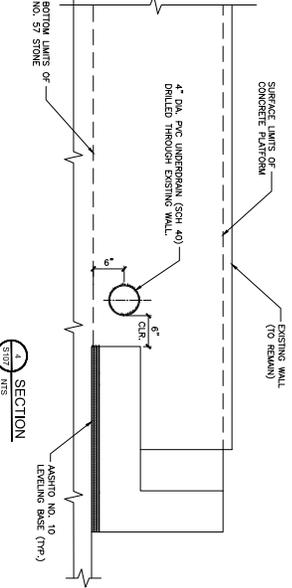
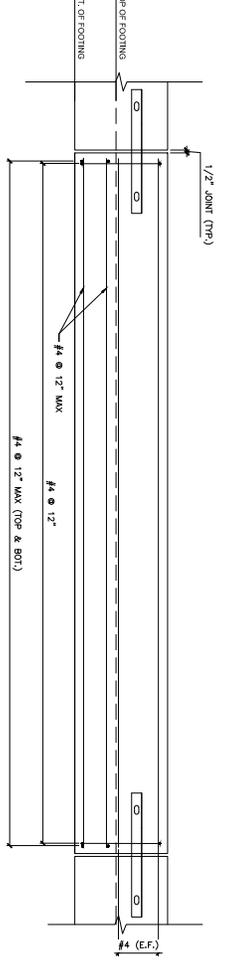
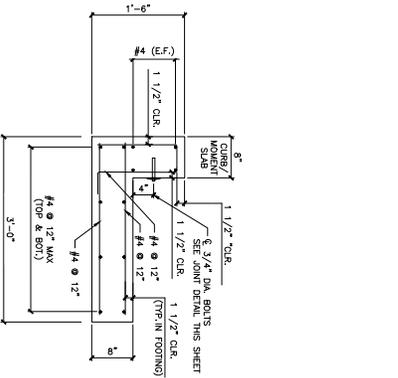
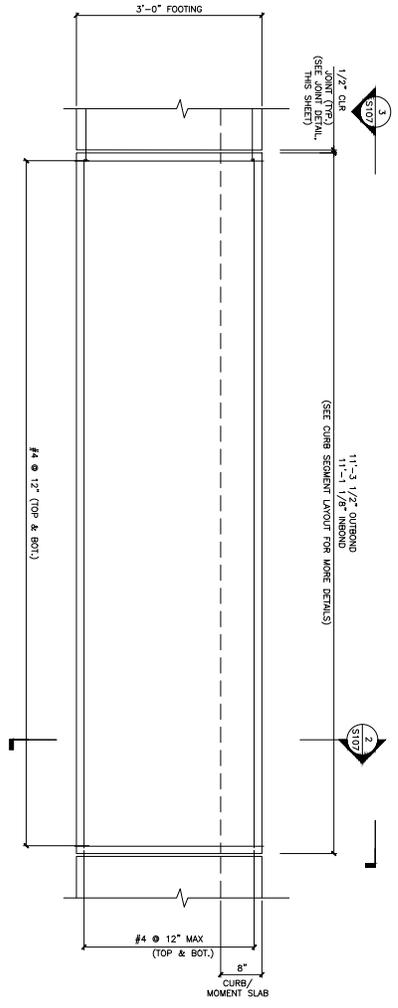
PLATFORM CURB LAYOUT

PROJECT NO: S106

DATE: OCT 2024

SCALE: 1"=10'

03



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ENGINEERING
ARCHITECTURE
PLANNING
100 MARKET STREET, SUITE 200
PHILADELPHIA, PA 19106

MAINLINE BRIDGE MP 11.83 OVER EASTON ROAD
RRD MAINLINE, GLENSIDE, PA

BRIDGE 11.83 REHABILITATION
STRUCTURAL

PLATFORM CURB DETAILS

DATE: 10/25/24
BY: [REDACTED]
CHECKED: [REDACTED]
APPROVED: [REDACTED]

PROJECT NO: S107
SHEET NO: 03

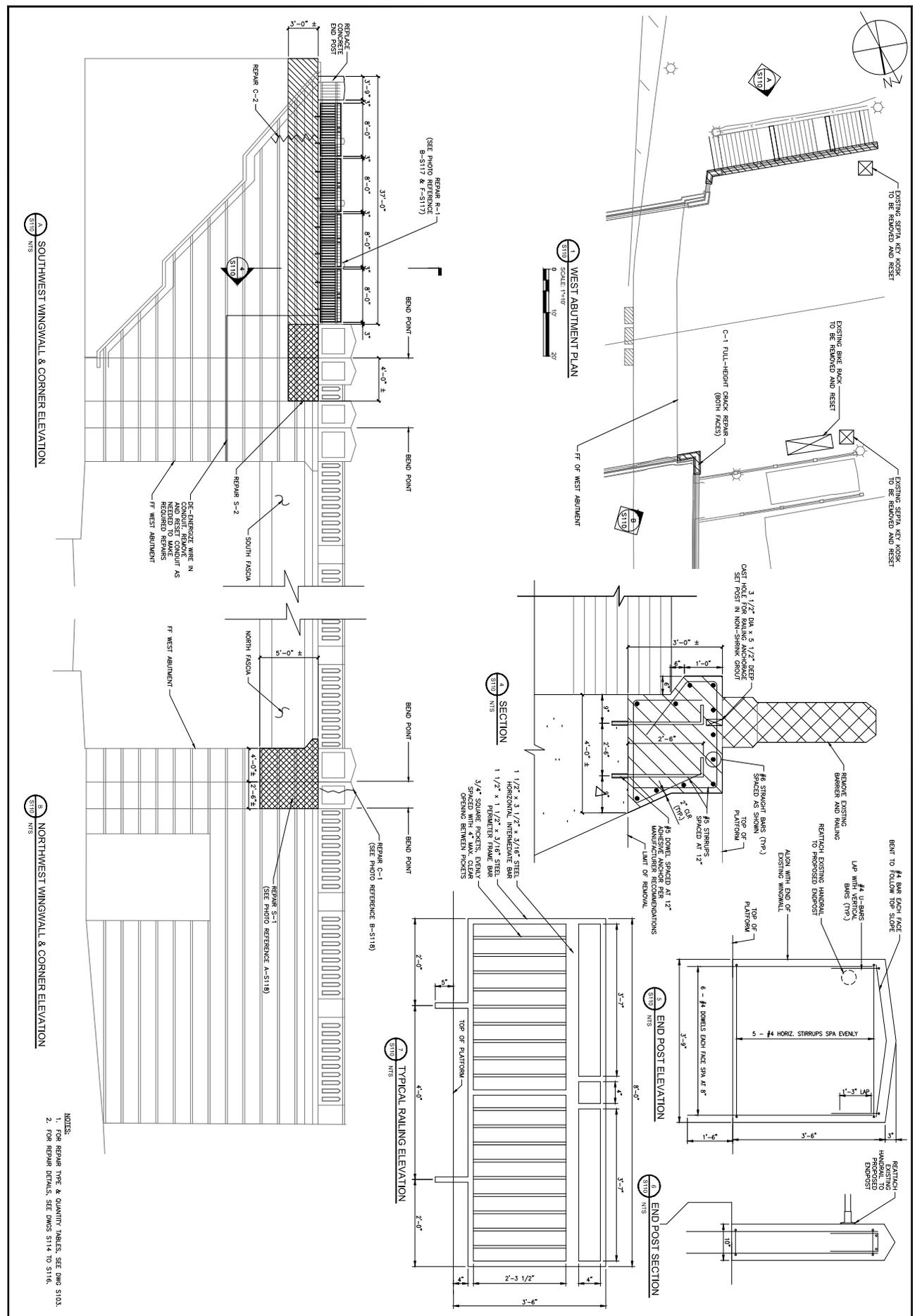
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03	10/25/24	100% RESUBMISSION

DATE: 10/25/24

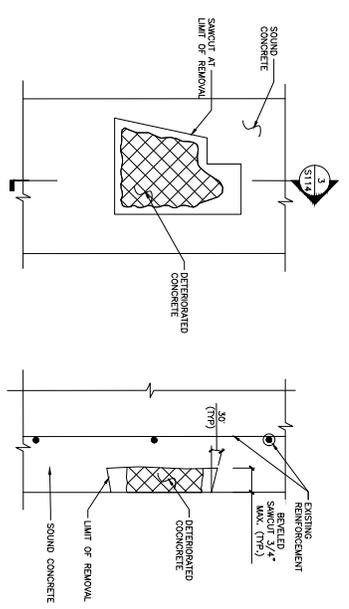
SCALE: 1/8" = 1'-0"

100% SUBMISSION

DATE PRINTED: 10/23/2024 11:16:39 AM



		Jacobus ENGINEERING 1000 W. MAIN ST. SUITE 100 GLENDELE, PA 15116 TEL: 717-261-1111 FAX: 717-261-1112													
PROJECT INFORMATION															
PROJECT NO.	11.83	DATE	10/20/2024												
PROJECT NAME	MAINLINE BRIDGE MP 11.83 OVER EASTON ROAD														
CLIENT	RRD MAINLINE, GLENDELE, PA														
DESIGNER	JACOBUS ENGINEERING														
DATE	10/20/2024														
SCALE	AS NOTED														
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02	9/27/24	100% SUBMISSION													
03	10/20/24	60% SUBMISSION													
BRIDGE 11.83 REHABILITATION STRUCTURAL WEST ABUTMENT PLAN & WINGWALLS															
NOTES: 1. FOR REPAIR TYPE & QUANTITY TABLES, SEE DWG S103. 2. FOR REPAIR DETAILS, SEE DWGS S114 TO S116.															
S110 DATE: 10/20/24 DRAWN BY: [Redacted] CHECKED BY: [Redacted] SCALE: AS NOTED															

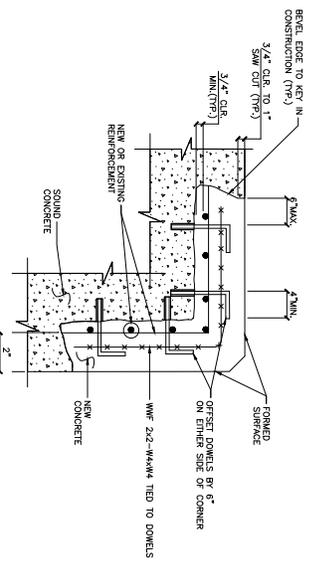


2 ELEVATION VIEW
S114 NTS

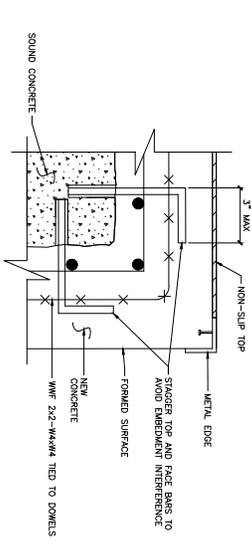
3 SECTION
S114 NTS

CONCRETE REPAIR TYPE 1
S114 NTS

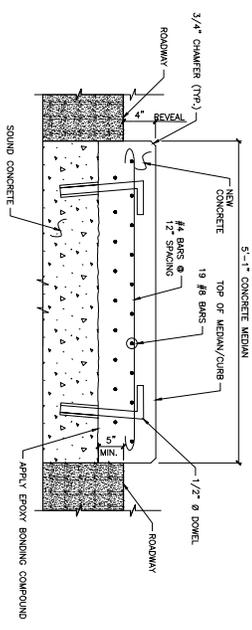
REPAIR TYPE 1 IS USED WHEN REPAIR OF DETERIORATED CONCRETE IS LESS THAN OR EQUAL TO 3/4" FOR REPAIR NOTES SEE SHEET S116



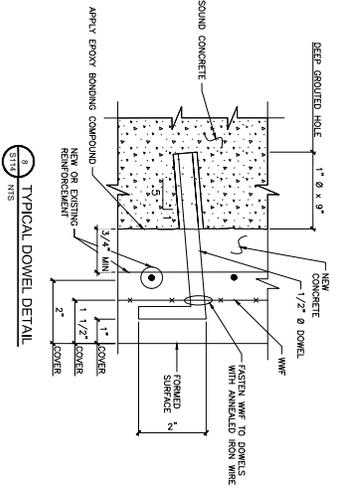
5 TYPICAL CORNER REPAIR DETAIL
S114 NTS



7 STAIR REPAIR DETAIL
S114 NTS



6 MEDIAN REPAIR DETAIL
S114 NTS



8 TYPICAL DOWEL DETAIL
S114 NTS

LEGEND:
 REMOVE DETERIORATED CONCRETE.

Jacobs

1000 MARKET STREET, SUITE 200
 FORT MYERS, FL 33901
 TEL: 888.488.4646
 WWW.JACOBS.COM

REV	DATE	DESCRIPTION
01	7/26/24	60% SUBMISSION
02	8/27/24	100% SUBMISSION
03	10/25/24	100% RESUBMISSION

PROJECT NO. 11.83

DATE: 10/23/2024 11:17:53 AM

MAINLINE BRIDGE MP 11.83 OVER EASTON ROAD
 RRD MAINLINE, GLENSIDE, PA

BRIDGE 11.83 REHABILITATION
 STRUCTURAL

REPAIR DETAILS - 1 OF 3

DATE: 10/23/2024

PROJECT NO. 11.83

NO.	DATE	DESCRIPTION
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02	08/22/24	100% SUBMISSION
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33	10/05/24	100% RESUBMISSION
34	10/05/24	100% RESUBMISSION
35	10/05/24	100% RESUBMISSION
36	10/05/24	100% RESUBMISSION
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38	10/05/24	100% RESUBMISSION
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40	10/05/24	100% RESUBMISSION
41	10/05/24	100% RESUBMISSION
42	10/05/24	100% RESUBMISSION
43	10/05/24	100% RESUBMISSION
44	10/05/24	100% RESUBMISSION
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46	10/05/24	100% RESUBMISSION
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50	10/05/24	100% RESUBMISSION

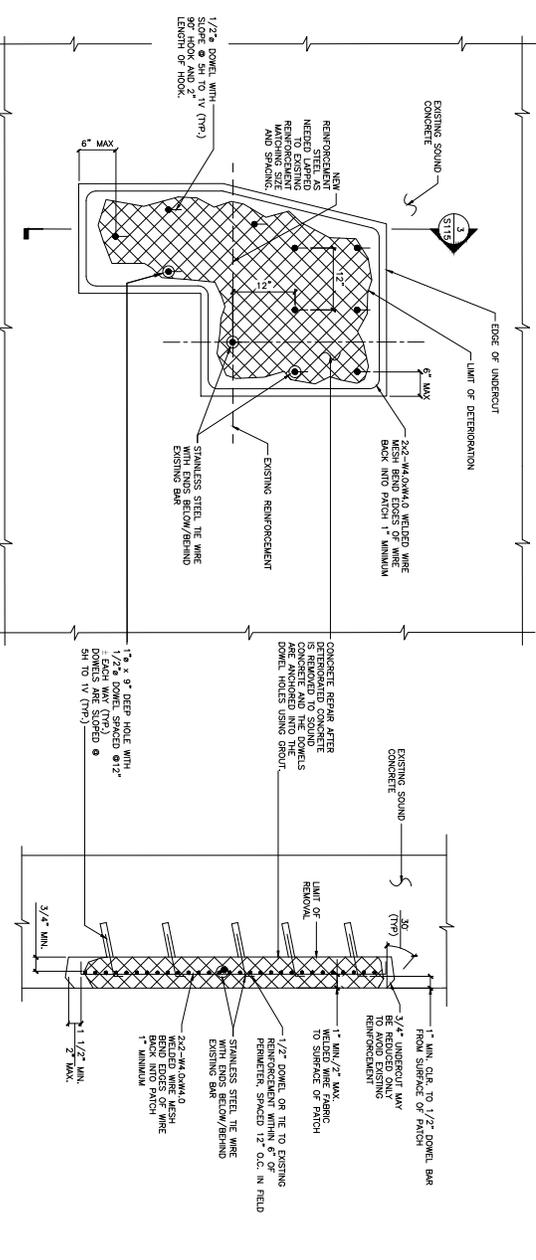
BRIDGE 11.83 REHABILITATION
STRUCTURAL
 REPAIR DETAILS - 2 OF 3
 MAINLINE BRIDGE MP 11.83 OVER EASTON ROAD
 RRD MAINLINE, GLENSIDE, PA

DATE: 07/27/2024
 SCALE: AS NOTED
 SHEET: S115
 PROJECT: BRIDGE 11.83 REHABILITATION

NOTE:
 PROVIDE GALVANIZED WIRE TO CONTACT EXISTING REINFORCEMENT. PROVIDE GALVANIZED WIRE MESH ALONG THE PERIMETER OF THE REMOVAL AREA AT A MINIMUM SPACING OF 3/4" ON CENTER. PROVIDE GALVANIZED WIRE MESH ALONG THE PERIMETER AND WITHIN VERTICAL JOINTS ALONG THE PERIMETER AND WITHIN SPACES AT GREATER THAN 12" SPACING OR NOT LOCATED TO PROVIDE THE CONDITIONS AS LISTED ABOVE. REMOVE EXISTING REINFORCEMENT AND PROVIDE GALVANIZED WIRE MESH TO PROVIDE THE CONDITIONS AT THE SAME SPACINGS. SUBMIT FLOWERS, JOINTED AND SPECIAL CEMENTS, MORTARS AND CONCRETES FOR APPROVAL.

LEGEND:
 - REMOVE DETRIORATED CONCRETE AND REPLACE

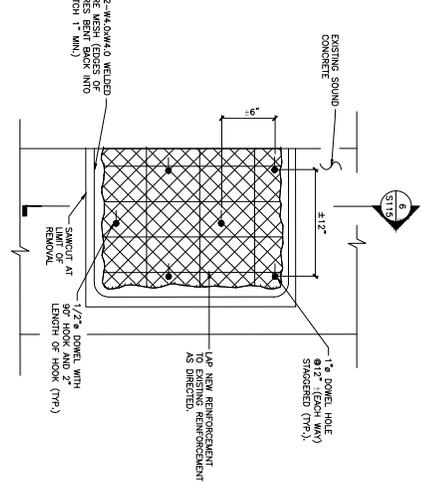
- REINFORCED CONCRETE REPAIR TYPE 2A NOTES:**
- SQUARE OFF DETRIORATED CONCRETE TO SOUND CONCRETE WITH A SAWCUT OF 3/4" MINIMUM BUT NOT TO THE DEPTH OF THE REINFORCEMENT STEEL.
 - REMOVE ALL LOOSE AND WEATHERED CONCRETE TO PROVIDE A SOUND BOND BETWEEN REINFORCEMENT AND NEW CONCRETE.
 - IF INTERSPERSED CONCRETE EXTENDS BEYOND THE REINFORCEMENT REMOVE THE CONCRETE TO AT LEAST 3/4" BEYOND THE REINFORCEMENT.
 - USE AN EPOXY BONDING COMPOUND MEETING THE REQUIREMENTS OF ASTM C881. COAT CONTACT SURFACES OF WIRE MESH WITH CONCRETE BONDING COMPOUND. PLACE CONCRETE BOND BETWEEN CONTACT SURFACES AND FRESH CONCRETE. WIRE MESH OR SANDWICH HANDED EPOXY BEFORE RECOATING WITH FRESH EPOXY.
 - WIRE MESH MAY BE SUBSTITUTED FOR NEW REINFORCEMENT IF INDICATED ON DESIGN DRAWINGS.
 - CLEAN EXISTING REINFORCEMENT BY MECHANICAL MEANS.
 - NEW REINFORCEMENT TO BE GALVANIZED.
 - CONCRETE REPAIR TYPE 2A ARE PAVABLE AS CONCRETE REPAIRS TYPE 2.



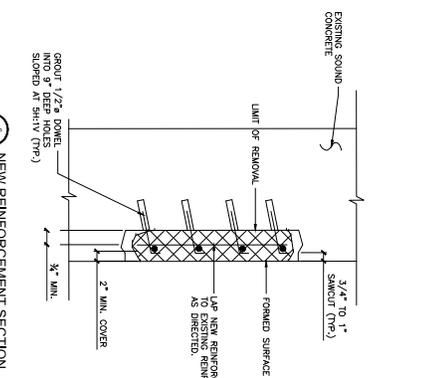
2. ELEVATION VIEW
 NTS

3. SECTION
 NTS

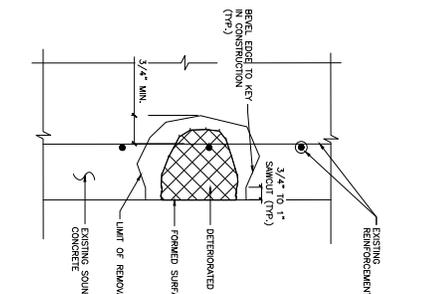
CONCRETE REPAIR TYPE 2A
NOTE: REPAIR TYPE 2A IS USED WHEN DEPTH OF DETRIORATION IS GREATER THAN 1 1/2" ON CENTERS.



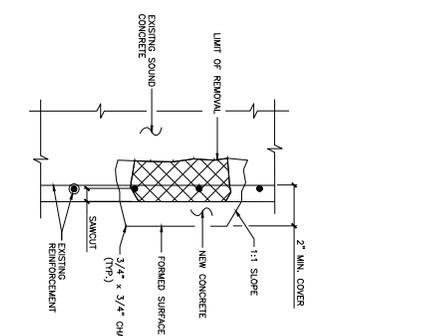
4. ELEVATION VIEW
 NTS



5. NEW REINFORCEMENT SECTION
 NTS

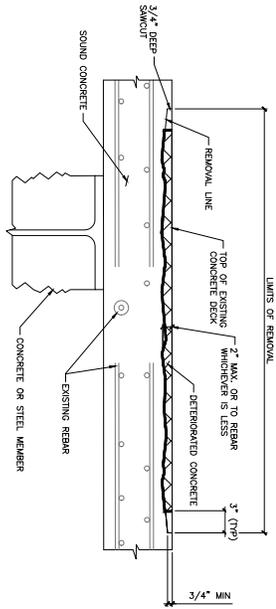


6. EXISTING REINFORCEMENT SECTION
 NTS



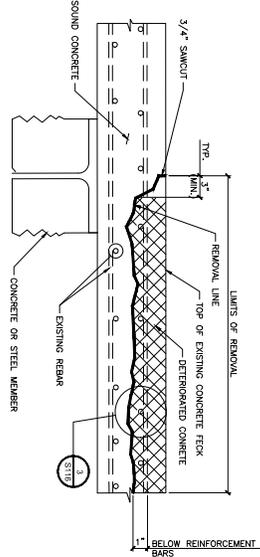
7. BLISTER SECTION
 NTS

NOTE: CONCRETE REPAIR TYPE 2 DETAIL FOR AREAS WITH EXISTING REINFORCEMENT HAVING LESS THAN 2" OF COVER

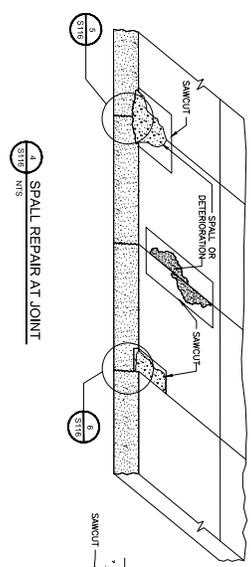


1 DECK REPAIR TYPE 1**

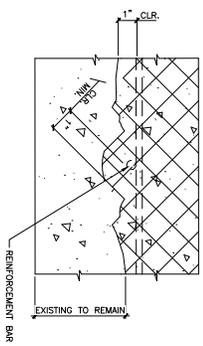
- ** TYPE 1 REPAIR IS TO BE RARELY USED.
 - USE TYPE 2 REPAIRS IN MOST SITUATIONS.
- DECK REPAIR TYPE 1 NOTES:
- DECK REPAIR TYPE 2 MAY BE REQUIRED WITHIN THE AREA OF A DECK REPAIR TYPE 1.



2 DECK REPAIR TYPE 2

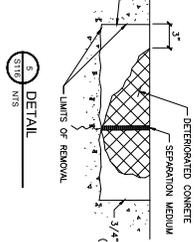


3 SPALL REPAIR AT JOINT

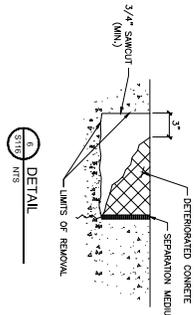


4 DETAIL

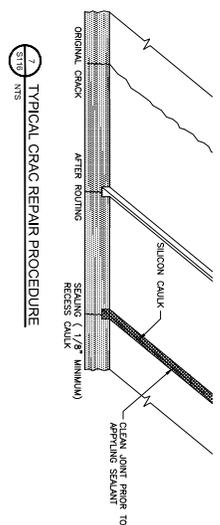
NOTE: USE WHEN THE EXPOSURE OF THE PRIMARY LONGITUDINAL REINFORCEMENT IS MORE THAN 1\"/>



5 DETAIL



6 DETAIL



7 TYPICAL CRACK REPAIR PROCEDURE

NOTE: VERTICAL 3/4\"/>

NOTE: USE CRACK SEALANT THAT MEETS THE REQUIREMENTS OF ASTM C881. USE SILICON CAULK SEALANT THAT MEETS THE REQUIREMENTS OF ASTM C881. USE SEALANT THAT MEETS THE REQUIREMENTS OF ASTM C881.

- REINFORCED CONCRETE REPAIR TYPE 1 NOTES:
- SQUARE OFF DETERIORATED CONCRETE TO SOUND CONCRETE WITH A SAWCUT OF 3/4\"/>
 - REMOVE ALL LOOSE AND DEBAMAGED CONCRETE TO PROVIDE A SOUND BOND AREA. REMOVE ALL REINFORCEMENT CONCRETE FROM THE AREA OF THE REPAIR. PROVIDE A SOUND BOND BETWEEN EXISTING CONCRETE AND NEW CONCRETE. USE A RIGID HARDENING CONCRETE PATCHING COMPOUND TO PATCH THE REPAIR. USE A RIGID HARDENING CONCRETE PATCHING COMPOUND TO PATCH THE REPAIR. USE A RIGID HARDENING CONCRETE PATCHING COMPOUND TO PATCH THE REPAIR.
 - IF DETERIORATED CONCRETE EXTENDS BEYOND THE PRIMARY REINFORCEMENT, REMOVE THE CONCRETE TO AT LEAST 3/4\"/>
 - USE AN EPOXY BONDING COMPOUND MEETING THE REQUIREMENTS OF ASTM C881. COAT CONTACT SURFACES OF REPAIR AREAS WITH CONCRETE BONDING COMPOUND. APPLY THE BONDING COMPOUND TO BOTH THE EXISTING CONCRETE AND THE NEW CONCRETE. USE THE BONDING COMPOUND TO ENSURE BOND BETWEEN CONTACT SURFACES AND FRESH CONCRETE. WIRE BRUSH OR SANDBLAST HARDENED EPOXY BEFORE RECORDING WITH REINFORCEMENT.
 - USE FORMS ONLY WHEN REPAIR OF DETERIORATED CONCRETE IS GREATER THAN 2\"/>
 - USE A MAGNETOMETER TO LOCATE EXISTING REINFORCEMENT WHEN DRILLING DOWN Holes TO AVOID DRILLING THRU EXISTING BARS.
 - AN APPROVED EPOXY ANCHORING SYSTEM IN 90\"/>
 - A #4 DEFORMED REINFORCEMENT BOLT 1\"/>
 - ALTERNATE WIRE MESH MAY BE SUBSTITUTED FOR #2-W4-D4-M4. PROVIDED WIRE SPACING DOES NOT EXCEED 4\"/>
 - IF EXISTING REINFORCEMENT IS TO BE LEFT IN PLACE, THE WIRE MESH SHALL EXCEEDS EXISTING REINFORCEMENT.
 - CLEAN EXISTING REINFORCEMENT BY MECHANICAL MEANS.
 - LAP EQUIVALENT NEW REINFORCEMENT TO THE EXISTING REINFORCEMENT AS DIRECTED.
 - REINFORCEMENT BARS TO BE GALVANIZED.

REINFORCED CONCRETE REPAIR TYPE 2 NOTES:

- SQUARE OFF DETERIORATED CONCRETE TO SOUND CONCRETE WITH A SAWCUT OF 3/4\"/>
- USE HAND TOOLS TO REMOVE ALL LOOSE AND DEBAMAGED CONCRETE THAT PROVIDES A SOUND BOND BETWEEN EXISTING CONCRETE AND NEW CONCRETE. USE A RIGID HARDENING CONCRETE PATCHING COMPOUND TO PATCH THE REPAIR. USE A RIGID HARDENING CONCRETE PATCHING COMPOUND TO PATCH THE REPAIR.
- IF DETERIORATED CONCRETE EXTENDS BEYOND THE PRIMARY REINFORCEMENT, REMOVE THE CONCRETE TO AT LEAST 3/4\"/>
- USE AN EPOXY BONDING COMPOUND MEETING THE REQUIREMENTS OF ASTM C881. COAT CONTACT SURFACES OF REPAIR AREAS WITH CONCRETE BONDING COMPOUND. APPLY THE BONDING COMPOUND TO BOTH THE EXISTING CONCRETE AND THE NEW CONCRETE. USE THE BONDING COMPOUND TO ENSURE BOND BETWEEN CONTACT SURFACES AND FRESH CONCRETE. WIRE BRUSH OR SANDBLAST HARDENED EPOXY BEFORE RECORDING WITH REINFORCEMENT.
- USE FORMS ONLY WHEN REPAIR OF DETERIORATED CONCRETE IS GREATER THAN 2\"/>
- USE A MAGNETOMETER TO LOCATE EXISTING REINFORCEMENT WHEN DRILLING DOWN Holes TO AVOID DRILLING THRU EXISTING BARS.
- AN APPROVED EPOXY ANCHORING SYSTEM IN 90\"/>
- A #4 DEFORMED REINFORCEMENT BOLT 1\"/>
- ALTERNATE WIRE MESH MAY BE SUBSTITUTED FOR #2-W4-D4-M4. PROVIDED WIRE SPACING DOES NOT EXCEED 4\"/>
- IF EXISTING REINFORCEMENT IS TO BE LEFT IN PLACE, THE WIRE MESH SHALL EXCEEDS EXISTING REINFORCEMENT.
- CLEAN EXISTING REINFORCEMENT BY MECHANICAL MEANS.
- LAP EQUIVALENT NEW REINFORCEMENT TO THE EXISTING REINFORCEMENT AS DIRECTED.
- REINFORCEMENT BARS TO BE GALVANIZED.

Jacobs

CONSTRUCTION CONSULTANTS INC.

1000 MARKET STREET, SUITE 1000

PHILADELPHIA, PA 19102

PROJECT: BRIDGE 11.83 REHABILITATION

DATE: 10/20/2024

SCALE: AS NOTED

PROJECT NO: S116

DATE: 10/20/2024

PROJECT NO: 11

DATE: 10/20/2024

PROJECT NO: 03

MAINLINE BRIDGE MP 11.83 OVER EASTON ROAD

RRD MAINLINE, GLENSIDE, PA

BRIDGE 11.83 REHABILITATION

STRUCTURAL

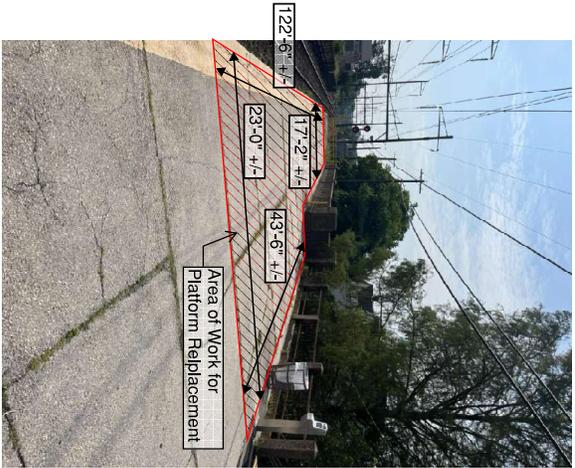
REPAIR DETAILS - 3 OF 3

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02	10/20/24	100% SUBMISSION
03	10/20/24	60% SUBMISSION

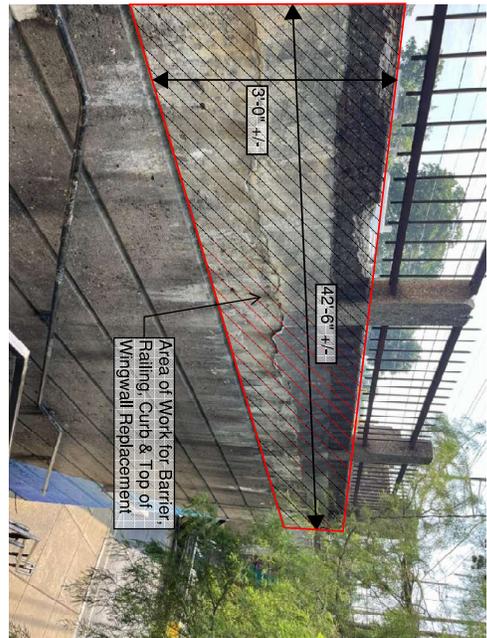
100% SUBMISSION

DATE PRINTED: 10/23/2024 11:17:55 AM

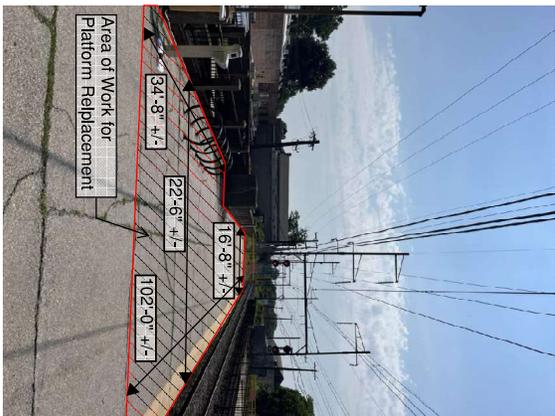
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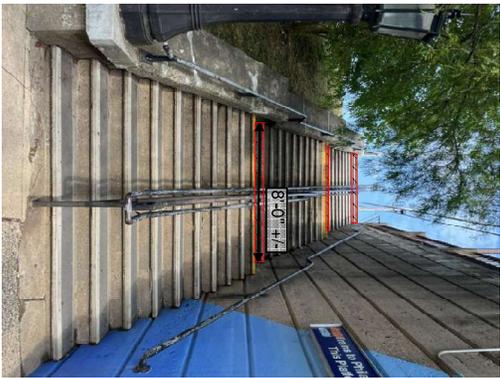
1.1 INBOUND PLATFORM REPAIRS



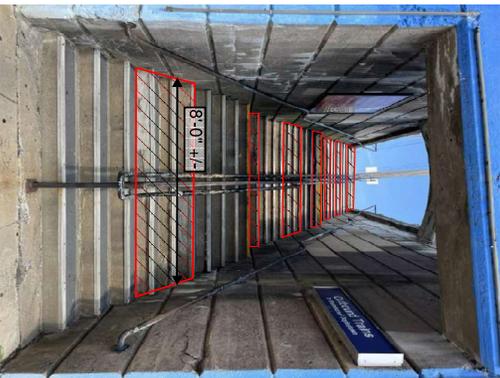
1.2 INBOUND PLATFORM BARRIER AND WINGWALL REPAIRS



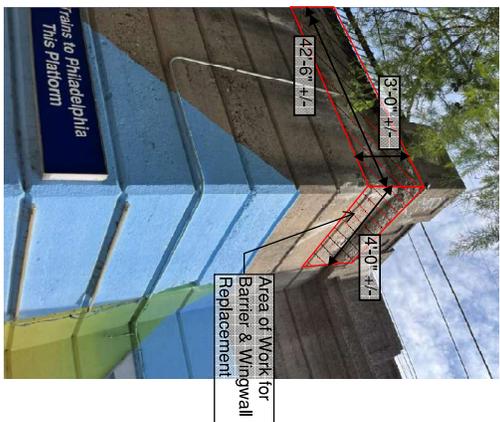
1.3 OUTBOUND PLATFORM REPAIRS



1.4 INBOUND PLATFORM STAIRS (SOUTH) REPAIRS



1.5 OUTBOUND PLATFORM STAIRS (NORTH) REPAIRS



1.6 SOUTHWEST ABUTMENT CORNER, INBOUND PLATFORM AND WINGWALL REPAIRS



PA DEPARTMENT OF TRANSPORTATION
 CHIEF ENGINEER
 TRAVIS W. HARRIS, P.E.
 100 NORTH SECOND STREET, SUITE 100
 HARRISBURG, PA 17103

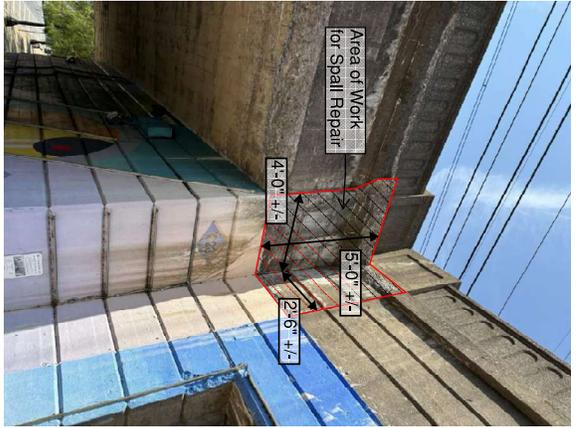
PROJECT NUMBER: 15-0000089E
 SHEET NUMBER: 11 OF 11
 DATE: 07/26/24
 DRAWN BY: JACOBSON
 CHECKED BY: JACOBSON
 APPROVED BY: JACOBSON

Jacobs
 A BLACK & VEATCH COMPANY
 1000 PENNSYLVANIA AVENUE
 SUITE 1000
 HARRISBURG, PA 17103

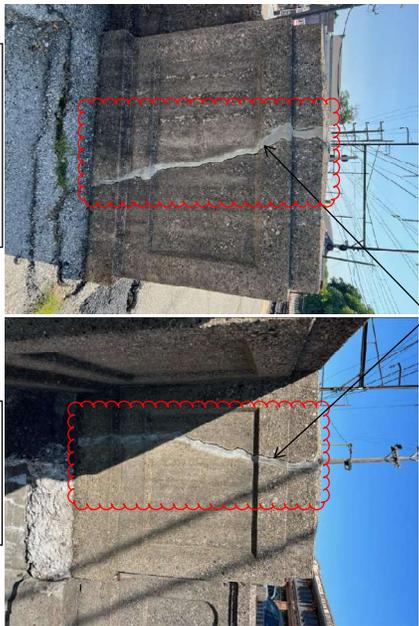
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02	7/27/24	100% SUBMISSION
03	10/25/24	100% RESUBMISSION

MAINLINE BRIDGE MP 11.83 OVER EASTON ROAD
 RRD MAINLINE, GLENSIDE, PA
BRIDGE 11.83 REHABILITATION
 STRUCTURAL
 PICTURE REFERENCE - 1 OF 4

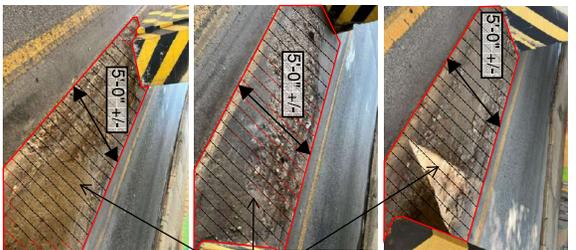
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PROJECT: BRIDGE 11.83 REHABILITATION	DATE: 10/20/24
NO. 11	11
100% SUBMISSION	03



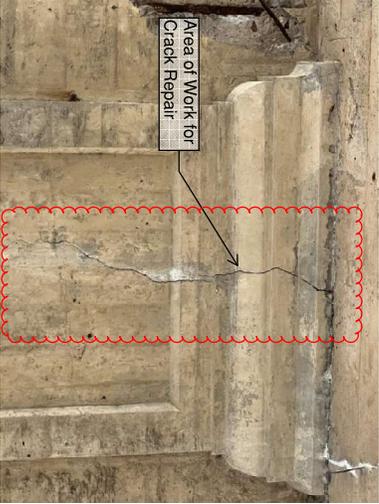
A NORTHWEST ABUTMENT CORNER SPALL REPAIRS



B NORTHWEST ABUTMENT CORNER BARRIER CRACK REPAIRS



C PIER CURB/MEDIAN REPAIRS



D PIER CRACK REPAIRS - EAST SIDE



E PIER COLUMN SPALL REPAIRS

Area of Work to rebuild concrete curb/pier footing Spall Repair (Typ.)

Area of Work for Spall Repair (Typ.)

Area of Work for Crack Repair (Typ.)



PA DEPARTMENT OF TRANSPORTATION
CONSTRUCTION DIVISION
100 NORTH SECOND STREET, SUITE 100
PHILADELPHIA, PA 19102

Jacobs

100 MARKET STREET, SUITE 200
PHILADELPHIA, PA 19102

REV	DATE	DESCRIPTION
03	10/25/24	100% RESUBMISSION
02	9/27/24	100% SUBMISSION
01	7/26/24	60% SUBMISSION

MAINLINE BRIDGE MP 11.83 OVER EASTON ROAD
RRD MAINLINE, GLENSIDE, PA

BRIDGE 11.83 REHABILITATION
STRUCTURAL

PICTURE REFERENCE - 2 OF 4

S118

AS NOTED

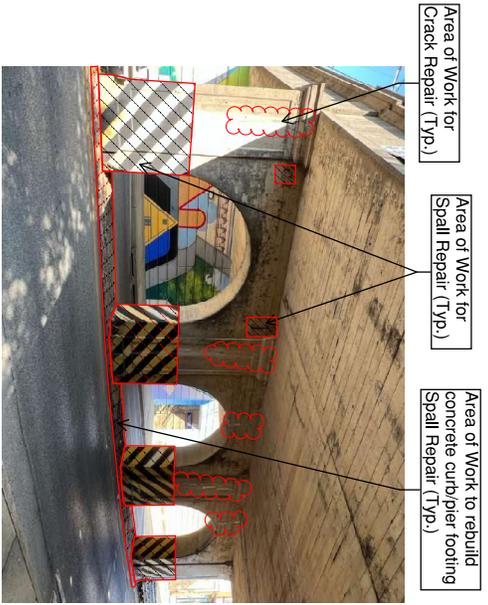
OCT 2024

11

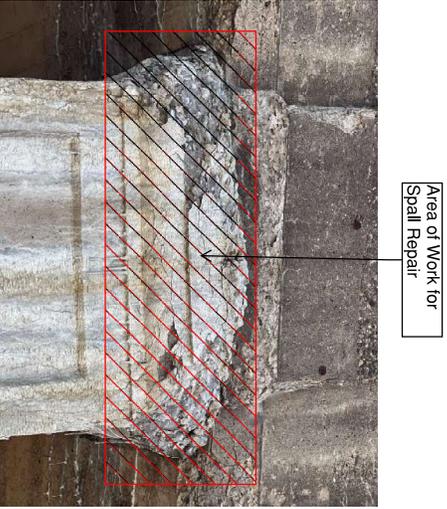
03

DATE PRINTED: 9/27/2024 10:03:52 AM

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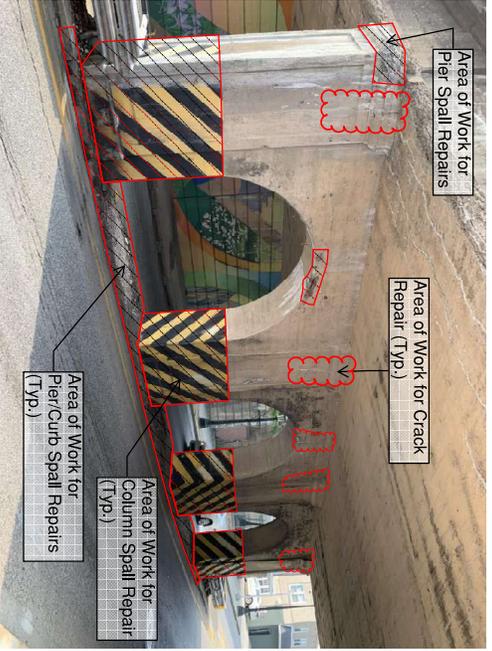
A
S119
N1S
PIER REPAIRS SUMMARY - EAST SIDE



B
S119
N1S
PIER COLUMN SPALL REPAIRS



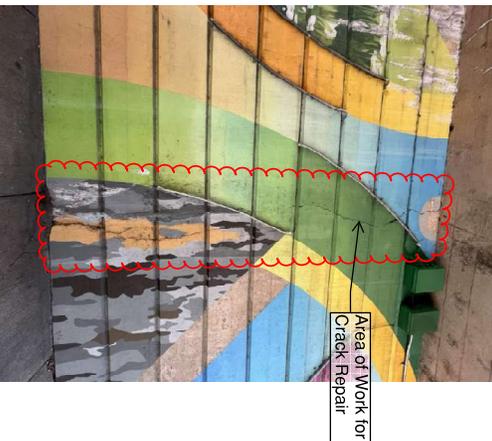
C
S119
N1S
PIER ARCH SPALL REPAIRS



D
S119
N1S
PIER REPAIRS SUMMARY - WEST SIDE



E
S119
N1S
EAST ABUTMENT - NORTHEAST CORNER SPALL REPAIR



F
S119
N1S
EAST ABUTMENT WALL CRACK REPAIR



Jacobs
ENGINEERING GROUP
1000 PENNSYLVANIA AVENUE, SUITE 1000
PHILADELPHIA, PA 19106

REV	DATE	DESCRIPTION
03	10/25/24	100% RESUBMISSION
02	9/22/24	100% SUBMISSION
01	7/26/24	60% SUBMISSION

MAINLINE BRIDGE MP 11.83 OVER EASTON ROAD
RRD MAINLINE, GLENSIDE, PA

BRIDGE 11.83 REHABILITATION
STRUCTURAL

PICTURE REFERENCE - 3 OF 4

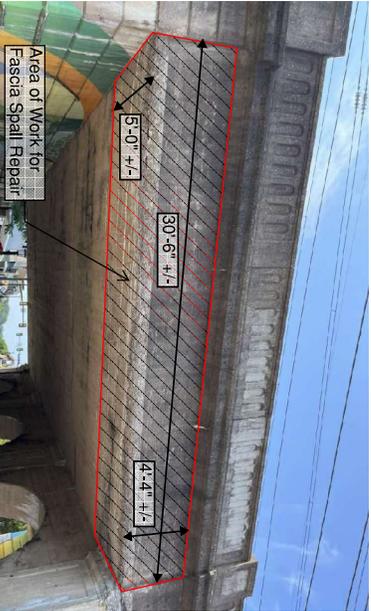
S119

DATE: 10/25/24
DRAWN BY: [Name]
CHECKED BY: [Name]
DATE: 10/25/24

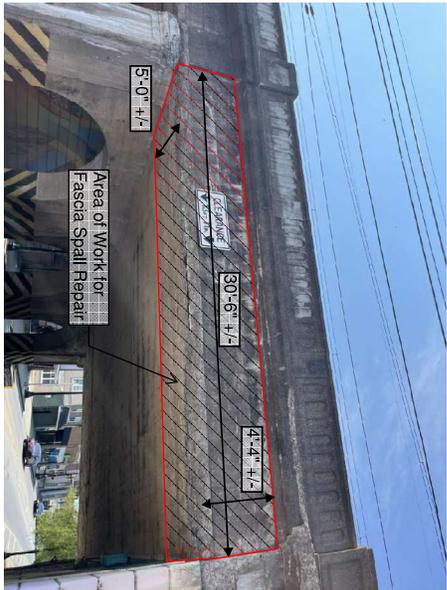
Area of Work for Spall Repair



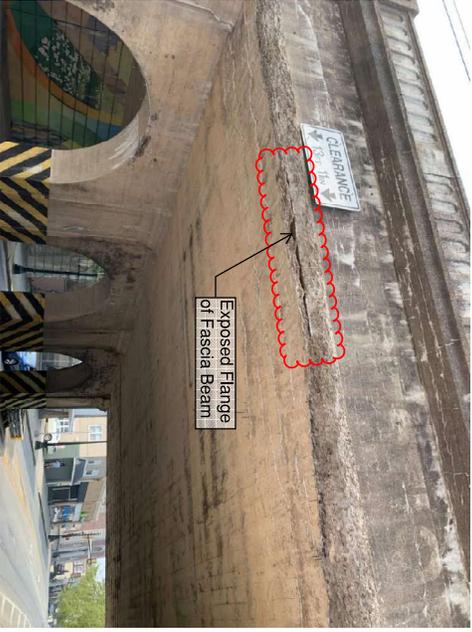
Ⓐ EAST ABUTMENT - SOUTHEAST RETAINING WALL SPALL REPAIR



Ⓑ NORTH FASCIA - SPALL REPAIRS OVER NORTHBOUND LANES



Ⓒ NORTH FASCIA - SPALL REPAIRS OVER SOUTHBOUND LANES



Ⓓ NORTH FASCIA - SPALL REPAIRS WITH EXPOSED FLANGE OVER SOUTHBOUND LANES



Ⓔ NORTH FASCIA - SPALL REPAIRS



PA DEPARTMENT OF TRANSPORTATION
 EASTON BRIDGE
 PROJECT NO. 11-83
 DRAWING NO. 11-83-70

DATE: 10/22/24
 TIME: 10:03 AM
 PROJECT: EASTON BRIDGE
 DRAWING: 11-83-70

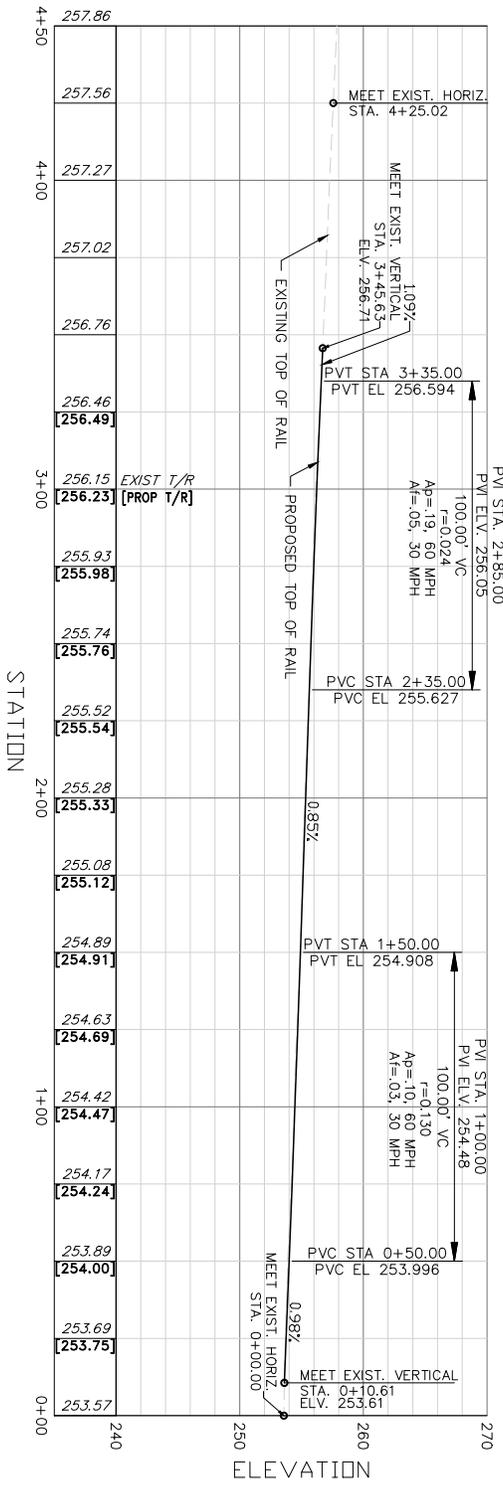
Jacobs

11-83-70-03
 STRUCTURAL
 BRIDGE 11.83 REHABILITATION

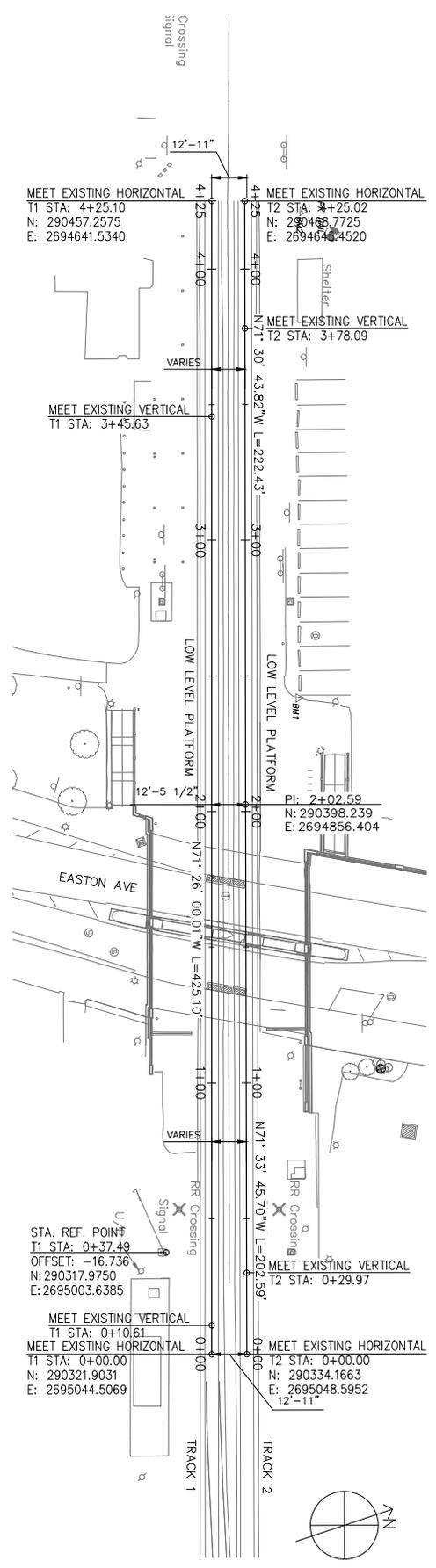
REV	DATE	DESCRIPTION
01	7/26/24	60% SUBMISSION
02	9/27/24	100% SUBMISSION
03	10/25/24	100% RESUBMISSION

MAINLINE BRIDGE MP 11.83 OVER EASTON ROAD
 RRD MAINLINE, GLENSIDE, PA
BRIDGE 11.83 REHABILITATION
STRUCTURAL
 PICTURE REFERENCE - 4 OF 4

NO.	DATE	BY	CHKD	APPD
01	10/22/24	AS NOTED		
02	10/22/24	11		
03	10/22/24	11		



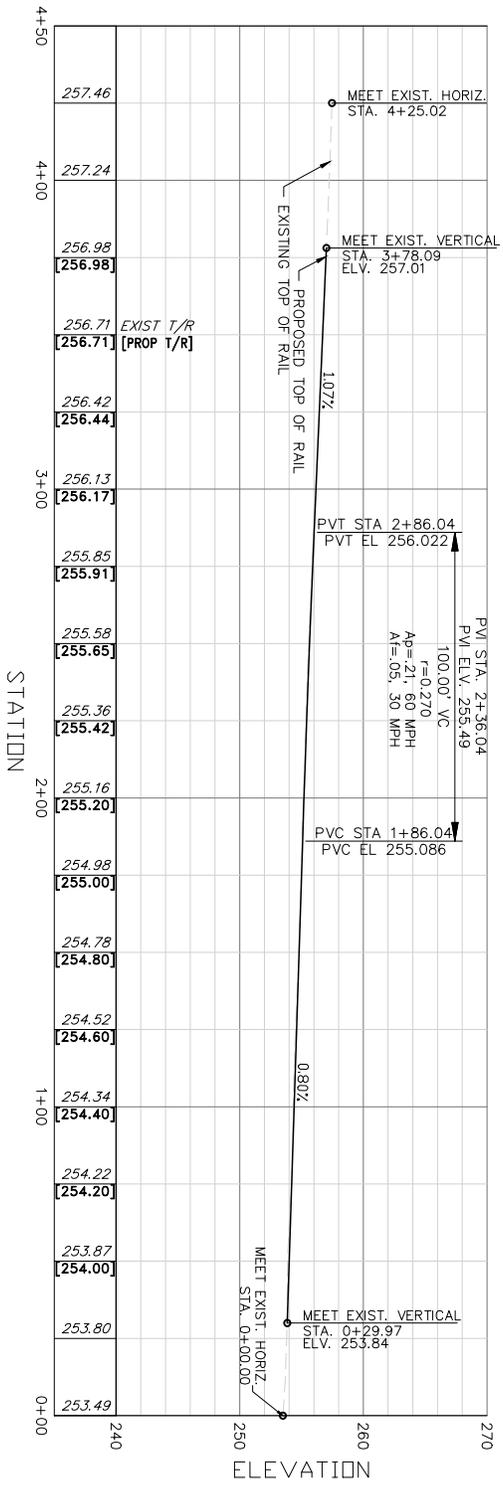
2 TRACK 1 PROPOSED PROFILE
 (1/8" SCALES NOTED)
 HORIZ: 1"=20'
 VERT: 1"=5'



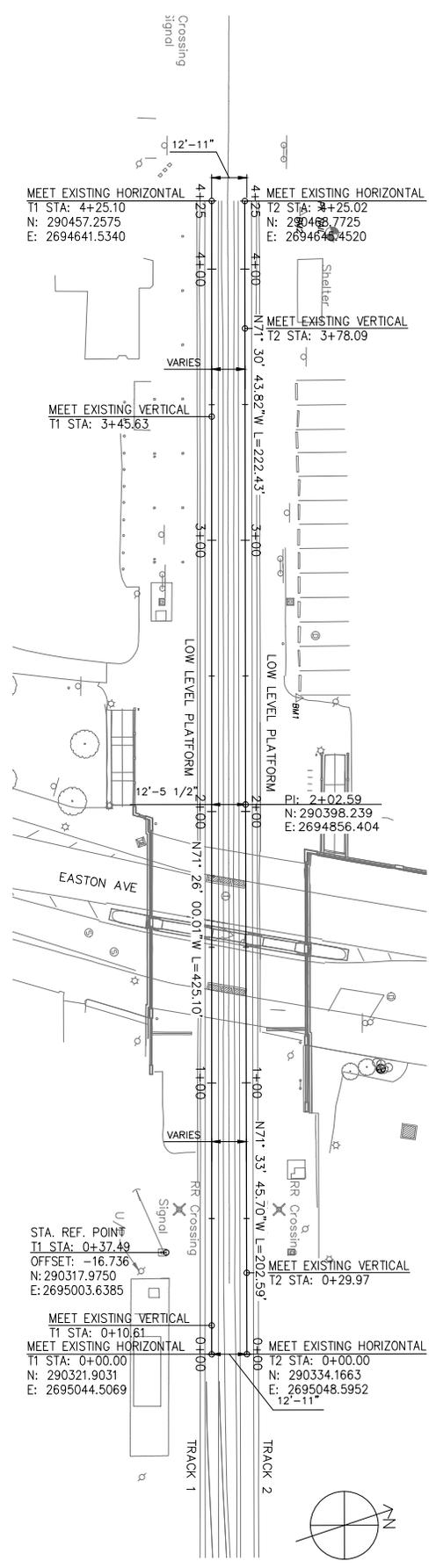
1 TRACK PLAN
 (1/8" SCALES NOTED)

MAINLINE BRIDGE MP 11.83 OVER EASTON ROAD RRD MAINLINE, GLENSIDE, PA BRIDGE 11.83 REHABILITATION TRACK TRACK 1 PLAN AND PROFILE			
TITLE: AS NOTED DATE: OCT 2024 DRAWN BY: N/A CHECKED BY: N/A PROJECT NO: T101 SHEET NO: 03	DATE: 03 DESCRIPTION: 100% RESUBMISSION DATE: 02 DESCRIPTION: 100% SUBMISSION DATE: 01 DESCRIPTION: 60% SUBMISSION	BY: MJK CKD: DAB APD: DP	DATE: 03 DESCRIPTION: 100% RESUBMISSION DATE: 02 DESCRIPTION: 100% SUBMISSION DATE: 01 DESCRIPTION: 60% SUBMISSION

100% SUBMISSION DATE PRINTED: 10/25/2024 3:22:59 PM C:\P\WORKING\JACOBS_BAU\JEG_KUEHNEM\0561305\TRACK_EASTONAVE_SHEETS.DWG



2 TRACK 2 PROPOSED PROFILE
 (100) SCALES NOTED
 HORIZ: 1"=20'
 VERT: 1"=5'



1 TRACK PLAN
 (100) SCALES NOTED

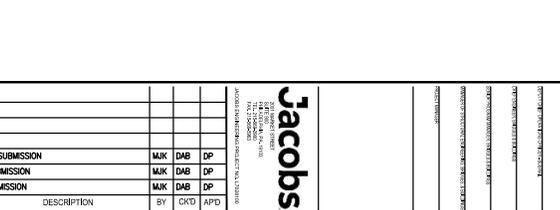
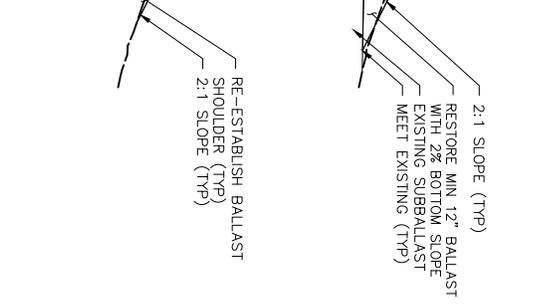
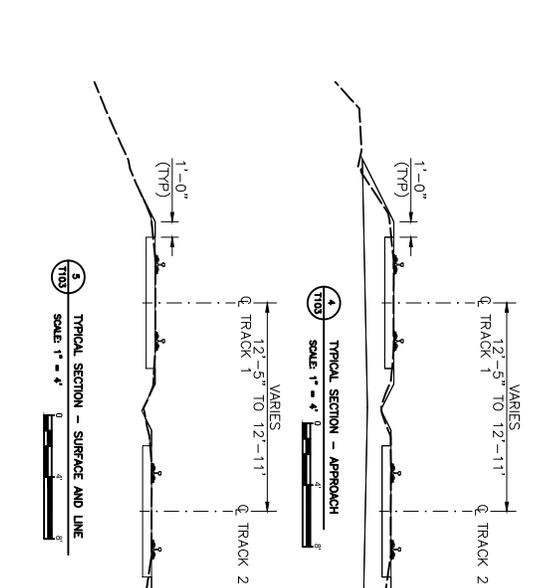
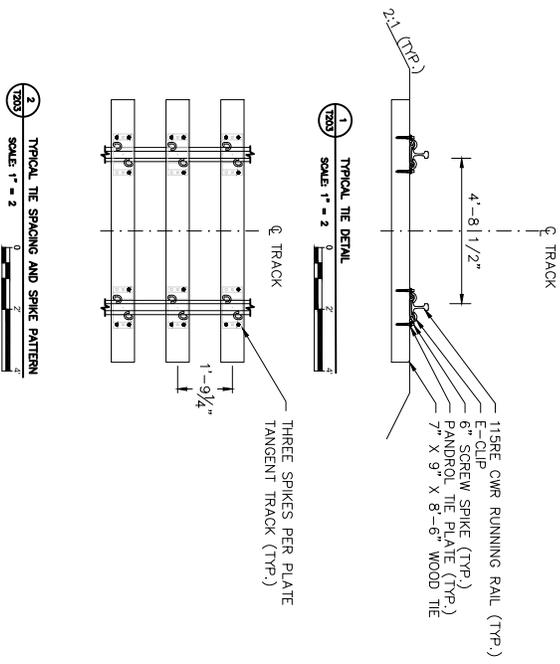
DATE	BY	CHKD	APPD
03/10/24	M.K.	DAB	DP
02/28/24	M.K.	DAB	DP
01/27/24	M.K.	DAB	DP

MAINLINE BRIDGE MP 11.83 OVER EASTON ROAD
 RRD MAINLINE, GLENSIDE, PA
BRIDGE 11.83 REHABILITATION
TRACK
 TRACK 2 PLAN AND PROFILE

REV	DATE	DESCRIPTION
03	10/25/24	100% RESUBMISSION
02	08/27/24	100% SUBMISSION
01	07/26/24	60% SUBMISSION

Jacobs
 1000 MARKET STREET, SUITE 200
 PHILADELPHIA, PA 19102
 TEL: 215-381-6000
 WWW.JACOBS.COM

CONSTRUCTION
 1000 MARKET STREET, SUITE 200
 PHILADELPHIA, PA 19102
 TEL: 215-381-6000
 WWW.CONSTRUCTION.COM



- NOTES:**
1. TRACK STATIONING IS MEASURED ALONG THE CENTERLINE OF PROPOSED ALIGNMENT.
 2. UTILIZE EXISTING TRACK WHEREVER PRACTICAL TO ACHIEVE THE FINAL ALIGNMENT BY LINING/SURFACING. INSTALL NEW TRACK ON BRIDGE AND WHERE NECESSARY ON THE APPROACHES TO ALLOW FOR CONSTRUCTION OF THE NEW BRIDGE.
 3. EXCAVATE EXISTING BALLAST TO DEPTH NECESSARY TO CONSTRUCT THE NEW BRIDGE EXISTING HARDPAN SHALL REMAIN UNDISTURBED.
- BENCHMARKS:**
1. BM1/ELEVATION: 266.6531', 290429.172 N, 26944826.266 E, PK NAIL
 2. BM2/ELEVATION: 298.6302', 290487.612 N, 26946535.800 E, PK NAIL

BRIDGE 11.83 REHABILITATION TRACK

TRACK NOTES, DETAILS, AND TYPICAL SECTIONS

NO.	DATE	DESCRIPTION	BY	CHKD	APPD
01	07/24/24	60% SUBMISSION	MJK	DAB	92
02	08/27/24	100% SUBMISSION	MJK	DAB	92
03	10/25/24	100% RESUBMISSION	MJK	DAB	92

DESIGNER: JACOBS ENGINEERING GROUP, INC.

CHECKED: DAB

DATE: 10/25/24

SCALE: 1" = 4'

APPENDIX D

ABBREVIATIONS

AV	AV CONDUIT	AV	AV CONDUIT
AVC	AV CONTROL	AV	AV CONTROL
AVL	AV LIGHT	AV	AV LIGHT
AVF	AV FLOOR	AV	AV FLOOR
AVR	AV RISE	AV	AV RISE
AVS	AV SERVICE	AV	AV SERVICE
AVT	AV TRUNK	AV	AV TRUNK
AVW	AV WIRE	AV	AV WIRE
AVX	AV CROSS	AV	AV CROSS
AVY	AV YARD	AV	AV YARD
AVZ	AV ZONE	AV	AV ZONE
AW	AW	AW	AW
AWB	AW BENCH	AW	AW BENCH
AWC	AW C	AW	AW C
AWD	AW D	AW	AW D
AWF	AW FLOOR	AW	AW FLOOR
AWG	AW G	AW	AW G
AWH	AW H	AW	AW H
AWI	AW I	AW	AW I
AWJ	AW J	AW	AW J
AWK	AW K	AW	AW K
AWL	AW L	AW	AW L
AWM	AW M	AW	AW M
AWN	AW N	AW	AW N
AWO	AW O	AW	AW O
AWP	AW P	AW	AW P
AWQ	AW Q	AW	AW Q
AWR	AW R	AW	AW R
AWS	AW S	AW	AW S
AWT	AW T	AW	AW T
AWU	AW U	AW	AW U
AWV	AW V	AW	AW V
AWW	AW W	AW	AW W
AWX	AW X	AW	AW X
AWY	AW Y	AW	AW Y
AWZ	AW Z	AW	AW Z
AX	AX	AX	AX
AXB	AX B	AX	AX B
AXC	AX C	AX	AX C
AXD	AX D	AX	AX D
AXE	AX E	AX	AX E
AXF	AX F	AX	AX F
AXG	AX G	AX	AX G
AXH	AX H	AX	AX H
AXI	AX I	AX	AX I
AXJ	AX J	AX	AX J
AXK	AX K	AX	AX K
AXL	AX L	AX	AX L
AXM	AX M	AX	AX M
AXN	AX N	AX	AX N
AXO	AX O	AX	AX O
AXP	AX P	AX	AX P
AXQ	AX Q	AX	AX Q
AXR	AX R	AX	AX R
AXS	AX S	AX	AX S
AXT	AX T	AX	AX T
AXU	AX U	AX	AX U
AXV	AX V	AX	AX V
AXW	AX W	AX	AX W
AXX	AX X	AX	AX X
AXY	AX Y	AX	AX Y
AXZ	AX Z	AX	AX Z
AY	AY	AY	AY
AYB	AY B	AY	AY B
AYC	AY C	AY	AY C
AYD	AY D	AY	AY D
AYE	AY E	AY	AY E
AYF	AY F	AY	AY F
AYG	AY G	AY	AY G
AYH	AY H	AY	AY H
AYI	AY I	AY	AY I
AYJ	AY J	AY	AY J
AYK	AY K	AY	AY K
AYL	AY L	AY	AY L
AYM	AY M	AY	AY M
AYN	AY N	AY	AY N
AYO	AY O	AY	AY O
AYP	AY P	AY	AY P
AYQ	AY Q	AY	AY Q
AYR	AY R	AY	AY R
AYS	AY S	AY	AY S
AYT	AY T	AY	AY T
AYU	AY U	AY	AY U
AYV	AY V	AY	AY V
AYW	AY W	AY	AY W
AYX	AY X	AY	AY X
AYY	AY Y	AY	AY Y
AYZ	AY Z	AY	AY Z
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AZE	AZ E	AZ	AZ E
AZF	AZ F	AZ	AZ F
AZG	AZ G	AZ	AZ G
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AZR	AZ R	AZ	AZ R
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AZT	AZ T	AZ	AZ T
AZU	AZ U	AZ	AZ U
AZV	AZ V	AZ	AZ V
AZW	AZ W	AZ	AZ W
AZX	AZ X	AZ	AZ X
AZY	AZ Y	AZ	AZ Y
AZZ	AZ Z	AZ	AZ Z

LEGEND - SYMBOLS

	DRAWING TITLE
	NORTH ARROW
	ELEVATION REFERENCE
	SECTION REFERENCE
	DETAIL REFERENCE
	REFERENCE ELEVATION LINE
	EXISTING DOOR
	NEW DOOR
	ACCESSIBLE SYMBOL
	STAR/RAMP DIRECTION
	ROOF PITCH
	EXISTING SPOT ELEVATION
	NEW SPOT ELEVATION
	REVISION CLOUD
	SIGN NUMBER
	SIGN TYPE
	SHRUB TREE
	MULTI-STEM SHRUB TREE
	FLOWERING TREE
	EVERGREEN TREE
	SHRUB
	ORNAMENTAL GRASS
	FERN
	TURF GRASS
	BROOM/EROSION CONTROL SEED MARK
	EXISTING UNDERGROUND ELECTRIC SERVICE
	EXISTING SANITARY SEWER
	EXISTING GAS SERVICE
	EXISTING UNDERGROUND TELEPHONE SERVICE
	EXISTING TELEPHONE SERVICE
	EXISTING CABLE TV SERVICE
	EXISTING PROPERTY LINE
	OVERHEAD WIRE
	EXISTING FENCE
	EXISTING MAJOR CONTOUR
	EXISTING MINOR CONTOUR
	PROPOSED MAJOR CONTOUR
	PROPOSED MINOR CONTOUR
	LIGHT POLE
	EXISTING SANITARY MANHOLE
	EXISTING UTILITY MANHOLE
	EXISTING MANHOLE
	EXISTING WATER VALVE
	EXISTING UTILITY VALVE

100% RESUBMISSION

MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE
RRD MAINLINE, GLENSIDE, PA

BRIDGE 11.62 REHABILITATION
GENERAL

ABBREVIATIONS AND LEGEND

NO.	DATE	DESCRIPTION
01	07/24/2024	100% RESUBMISSION
02	07/24/2024	100% SUBMISSION
03	07/24/2024	60% SUBMISSION

DATE: 07/24/2024

SCALE: 1/1

PROJECT NO: G202

SURVEY NOTES:

- EXISTING BASE SURVEY BY KVA CONSULTING ENGINEERS, INC (KVA) ON MAY 11, 2022 AND PROVIDED BY SEPA.
- THIS SURVEY HAS BEEN PREPARED AND COMPLETED WITHOUT THE BENEFIT OF A TITLE SEARCH AND CONVEYANCE RECORD. THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL UNDERGROUND UTILITIES AND RESTRICTIONS OF RECORD THAT A TITLE SEARCH MAY DISCLOSE.
- THE HORIZONTAL DATUM FOR THIS PLAN IS BASED ON MDCR20(11) THE PENNSYLVANIA STATE PLANE COORDINATE SYSTEM, SOUTH ZONE. THE VERTICAL DATUM IS BASED ON NAVD83.
- UNDERGROUND UTILITIES ARE SHOWN RELATIVE TO THE LOCATION OF THEIR SURFACE FEATURE LOCATIONS. JACOBS DOES NOT WARRANT THAT THEIR UNDERGROUND UTILITIES ARE COMPLETE OR EXACT. JACOBS HAS NOT SURVEYED THE UNDERGROUND UTILITIES. A PENNSYLVANIA ONE CALL WAS COMPLETED ON JULY 11, 2024. SERIAL NO. 20241932307 - ABBINGTON TOWNSHIP AND 20241932306 - CHELTERHAM TOWNSHIP.
- THE PROJECT UNITS ARE IN U.S. SURVEY FEET.
- RIGHT OF WAY FOR KESWICK AVE AS INDICATED ON THE SEPA VALUATION MAPS. ALL WORK TO OCCUR WITHIN THE LEGAL RIGHT-OF-WAY.

GENERAL NOTES:

- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE FOLLOWING CURRENT STANDARDS AS APPLICABLE: MUNICIPAL DETAILS AND STANDARDS SEPA 408, PENNSYLVANIA CONSTRUCTION STANDARDS (OR DRAWINGS), FEDERAL HIGHWAY STANDARDS (OR DRAWINGS), PAPER EROSION AND SEDIMENTATION CONTROL STANDARDS DETAILS.
- THE PROJECT IS NOT LOCATED WITHIN FEMA 100-YEAR FLOODPLAIN.
- BASED ON REVIEW OF THE EXISTING SITE CONDITIONS, NO PORTION OF THE PROJECT WOULD BE CONSIDERED AS A JURISDICTION, WETLAND OR WATERWAY. THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL UNDERGROUND UTILITIES AND RESTRICTIONS OF RECORD BY OPENING 150' USACE SECTION 604 WILL NOT BE REQUIRED.
- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE APPROVED EROSION AND SEDIMENTATION CONTROL PLAN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ANY ADDITIONAL EROSION CONTROL MEASURES THAT MAY BE REQUIRED AS CONSTRUCTION PROGRESSES. THE CONTRACTOR IS REQUIRED TO CONTACT THE PENNSYLVANIA DEPARTMENT OF ENVIRONMENTAL PROTECTION TO OBTAIN ANY PERMITS OR APPROVALS TO IMPLEMENT ANY REVISIONS TO THE APPROVED PLAN.
- NO OBJECTS SHALL BE PLACED, PLANTED, OR SET WITHIN THE AREA OF ANY THE EASEMENT OR RIGHT-OF-WAY WOULD ADVERSELY IMPACT THE FUNCTION OF THE EASEMENT OR RIGHT-OF-WAY.
- CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL REQUIRED APPROVALS, PERMITS, AND ACCEPTANCE INSPECTIONS FROM ALL REQUIRED AGENCIES.

EXISTING CONDITIONS

- ALL DIMENSIONS, ELEVATIONS, AND PHYSICAL CONDITIONS SHOWN ON THE DRAWINGS ARE TO BE MAINTAINED THROUGHOUT CONSTRUCTION. THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL UNDERGROUND UTILITIES AND RESTRICTIONS OF RECORD BY OPENING 150' USACE SECTION 604 WILL NOT BE REQUIRED. OTHER AVAILABLE SOURCES, SUCH AS DEPOTIONS OF EXISTING CONSTRUCTION ARE INTENDED TO BE GENERAL, APPROXIMATE, AND LIMITED TO THOSE AREAS FOR WHICH THE CONTRACTOR HAS BEEN ADVISED. THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL UNDERGROUND UTILITIES AND RESTRICTIONS OF RECORD BY OPENING 150' USACE SECTION 604 WILL NOT BE REQUIRED.
- THE EXACT EXTENT OF CONSTRUCTION OR RESTORATION WORK CANNOT BE DETERMINED FROM THE DRAWINGS. THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL UNDERGROUND UTILITIES AND RESTRICTIONS OF RECORD BY OPENING 150' USACE SECTION 604 WILL NOT BE REQUIRED. ACTUAL FIELD CONDITIONS MAY REQUIRE ADJUSTMENTS TO THE CONSTRUCTION DETAILS, MATERIAL QUANTITIES, AND EXTENT OF THE MODIFICATION WORK SHOWN ON DRAWINGS. PERFORM THE WORK TO MEET FIELD CONDITIONS ENCOUNTERED.
- EXAMINE AND FIELD VERIFY ALL EXISTING AND OPEN DIMENSIONS AND CONDITIONS PRIOR TO COMMENCEMENT OF THE WORK, AND FABRICATION OF CONSTRUCTION MATERIALS. REPORT VARIANCES FROM THE DRAWINGS AND SPECIFICATIONS TO THE ARCHITECT IMMEDIATELY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FOR THE SHIP AND ERECTION PLANS, INCHUTE CHANGES AND ADJUSTMENTS ON DRAWINGS SUBMITTED.

CONSTRUCTION NOTES

- THE CONTRACTOR SHALL VERIFY ALL REQUIREMENTS AND POINTS OF CONSTRUCTION, THE LOCATION OF ALL NEW CONSTRUCTION, AND VERIFY ALL STAKES, OFFSETS, AND CLEARANCES.
- THE CONTRACTOR SHALL MAINTAIN ALL UTILITY SERVICES TO REMAIN AND TEMPORARY CONSTRUCTION SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION. A WRITTEN CONSTRUCTION SEQUENCE PLAN AND COORDINATE ANY REQUIRED BREAKS IN UTILITY SERVICE WITH SEPA AND THE APPROPRIATE UTILITY PRIOR TO COMMENCING ANY WORK REQUIRING A BREAK IN UTILITY SERVICE.
- THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE AT ALL TIMES DURING CONSTRUCTION.
- THE CONTRACTOR IS RESPONSIBLE FOR THE REMOVAL OF ALL CONSTRUCTION WASTE FROM THE SITE. THE CONTRACTOR SHALL PROVIDE A WRITTEN WASTE MANAGEMENT PLAN TO SEPA PRIOR TO CONSTRUCTION. THE CONTRACTOR WILL PROVIDE EVIDENCE OF LEGAL DISPOSAL.
- NUMERICAL DIMENSIONS AND ELEVATIONS SHOWN SHALL SUPERSEDE ANY DISCREPANCY IN THE SCALES OF THE DRAWINGS. DO NOT SCALE THE DRAWINGS.
- NO WORK THAT WILL FOUL THE TRACK MAY BE PERFORMED WITHOUT PRIOR SEPA AUTHORIZATION. SEE SPECIFICATIONS SECTIONS 010600 AND 010650 FOR ADDITIONAL INFORMATION.
- COORDINATE ANY NEED FOR TRACK OUTAGE WITH THE SEPA PROJECT MANAGER. APPROVAL FOR TRACK OUTAGE IS SOLELY AT SEPA'S DISCRETION. SEE SECTION 011000 FOR ADDITIONAL INFORMATION.
- CONTRACTOR WILL CONTACT THE PA ONE CALL SYSTEM (800-242-1776) NO LESS THAN THREE (3) AND NO MORE THAN TEN (10) WORKING DAYS PRIOR TO BEGINNING CONSTRUCTION.
- THE CONTRACTOR MUST PROVIDE AND MAINTAIN SAFE PEDESTRIAN ACCESS FROM ALL OPERATIONAL AREAS TO ACTIVE PLATFORMS AND OPERATIONAL STATION BUILDING AT ALL TIMES DURING CONSTRUCTION.
- THE CONTRACTOR MUST MAINTAIN MINIMUM 10'-WIDE ENTRANCE AND EXIT LINES TO/FROM THE STATION LOT AT ALL TIMES.
- THE CONTRACTOR IS RESPONSIBLE TO PROVIDE ALL NECESSARY ON-SITE AND OFF-SITE TEMPORARY TRAFFIC CONTROL AND DIRECTIONAL SIGNAGE AND PARALLEL MARKINGS TO ALLOW SAFE MOVEMENT THROUGH CONSTRUCTION AREAS AND TO AID FROM ALL TEMPORARY AREAS.
- THE CONTRACTOR WILL ESTABLISH AND MAINTAIN TEMPORARY BENCHMARKS ON-SITE TO PERFORM OPERATIONS DURING CONSTRUCTION.
- CONTRACTOR SHALL COORDINATE WITH THE MUNICIPALITIES TO RESTORE ANY DISTURBED LANDSCAPING DURING CONSTRUCTION.

OWNERS

SOUTHSHORE PENNSYLVANIA TRANSPORTATION AUTHORITY
 1000 MARKET STREET
 PHILADELPHIA, PA 19107
 PHONE: (215) 566-7800

UTILITY CONTACTS

PECO ENERGY
 440 PARK AVE
 HANOVER, PA 17033
 CONTACT: JEFFREY W. HANCOCK
 EMAIL: jwhanco@peco.com
 (215) 956-3114

ABBINGTON TOWNSHIP SEWER
 1000 MARKET STREET
 ABBINGTON, PA 19001
 CONTACT: GEORGE WIRKLEY
 EMAIL: gwirkley@abbingtonpa.gov
 (215) 884-8329

CONCAST CABLE
 400 MAIN FERRYWAY BLVD
 PHILADELPHIA, PA 19107
 CONTACT: ANDREW WILLIAMS
 (215) 686-5580

CROWN CASTLE
 300 BARR HARBOR DR., SUITE 300
 CONROCK, PA 19428
 CONTACT: CLAUDIA WADSWORTH
 EMAIL: CLAUDIA.WADSWORTH@CROWNCASTLE.COM
 (610) 567-7016

SITE ADDRESS

KESWICK AVENUE
 GLENSIDE, PA 19038

VERZON PENNSYLVANIA LLC
 900 BAKE ST, 11TH FLOOR
 PHILADELPHIA, PA 19103
 CONTACT: ANTHONY S. PORTOLISE@VERZON.COM
 EMAIL: ANTHONY.S.PORTOLISE@VERZON.COM
 (215) 588-0792

AQUA PENNSYLVANIA
 782 LANCASTER AVE
 BRN WARR, PA 19010
 CONTACT: JILL MULLER
 EMAIL: jmulle@aquapenn.com
 (215) 887-1000

CHELTERHAM TOWNSHIP SEWER
 2530 CONROCK RD
 CONROCK, PA 19027
 CONTACT: TOM CINGOLA
 EMAIL: tcgingola@chelterham-township.org
 (267) 887-1000

LUMEN
 CONTACT: JEREMY WEIKS
 EMAIL: JEREMY.WEIKS@LUMEN.COM
 (910) 973-6497

CALL BEFORE YOU DIG!
 1-800-242-1776
 PENNSYLVANIA ONE CALL
 CONSTRUCTION PHASE - STOP CALL
 SERIAL NUMBERS:
 20241932306
 20241932307

ALL LOCATIONS OF EXISTING UTILITIES SHOWN ON THIS PLAN ARE FIELD VERIFIED. THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL UNDERGROUND UTILITIES AND RESTRICTIONS OF RECORD THAT A TITLE SEARCH MAY DISCLOSE. THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL UNDERGROUND UTILITIES AND RESTRICTIONS OF RECORD THAT A TITLE SEARCH MAY DISCLOSE. THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL UNDERGROUND UTILITIES AND RESTRICTIONS OF RECORD THAT A TITLE SEARCH MAY DISCLOSE.

JACOBS

100 MARKET STREET, SUITE 1000
 PHILADELPHIA, PA 19107
 TEL: 215.382.7000
 WWW.JACOBS.COM

PROJECT NO. 2024-001
 SHEET NO. 03
 DATE: OCT 2024

MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE
 RRD MAINLINE, GLENSIDE, PA

BRIDGE 11.62 REHABILITATION
 CIVIL

GENERAL NOTES - CIVIL

NO.	DATE	DESCRIPTION
01	07/24/24	60% SUBMISSION
02	07/24/24	100% SUBMISSION
03	07/24/24	100% RESUBMISSION
04	07/24/24	100% RESUBMISSION
05	07/24/24	100% RESUBMISSION
06	07/24/24	100% RESUBMISSION
07	07/24/24	100% RESUBMISSION
08	07/24/24	100% RESUBMISSION
09	07/24/24	100% RESUBMISSION
10	07/24/24	100% RESUBMISSION
11	07/24/24	100% RESUBMISSION
12	07/24/24	100% RESUBMISSION
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26	07/24/24	100% RESUBMISSION
27	07/24/24	100% RESUBMISSION
28	07/24/24	100% RESUBMISSION
29	07/24/24	100% RESUBMISSION
30	07/24/24	100% RESUBMISSION

SCALE: 1" = 10'-0"

DATE: OCT 2024

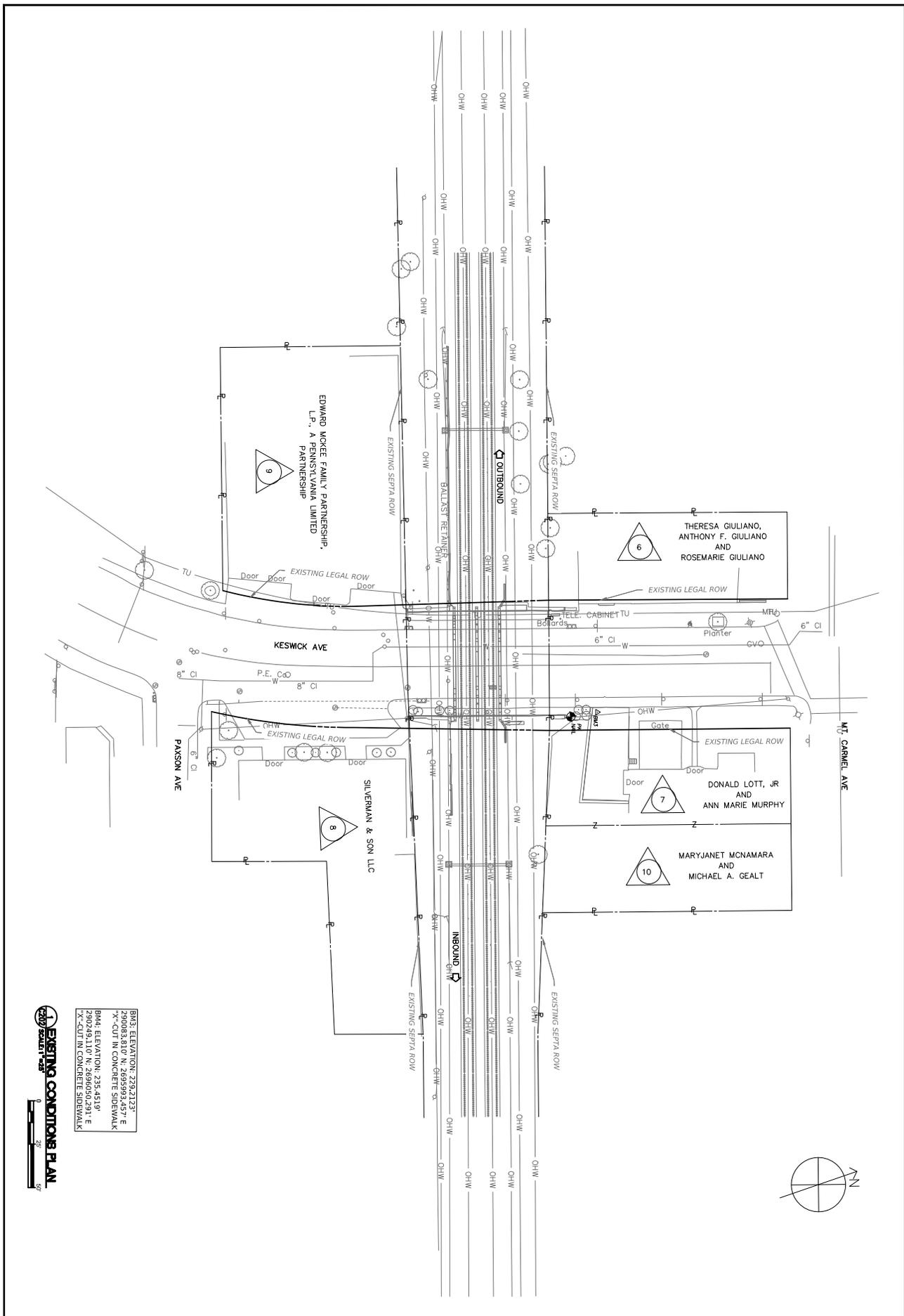
PROJECT NO. 2024-001

SHEET NO. 03

JACOBS

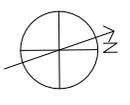
100 MARKET STREET, SUITE 1000
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 SHEET NO. 03
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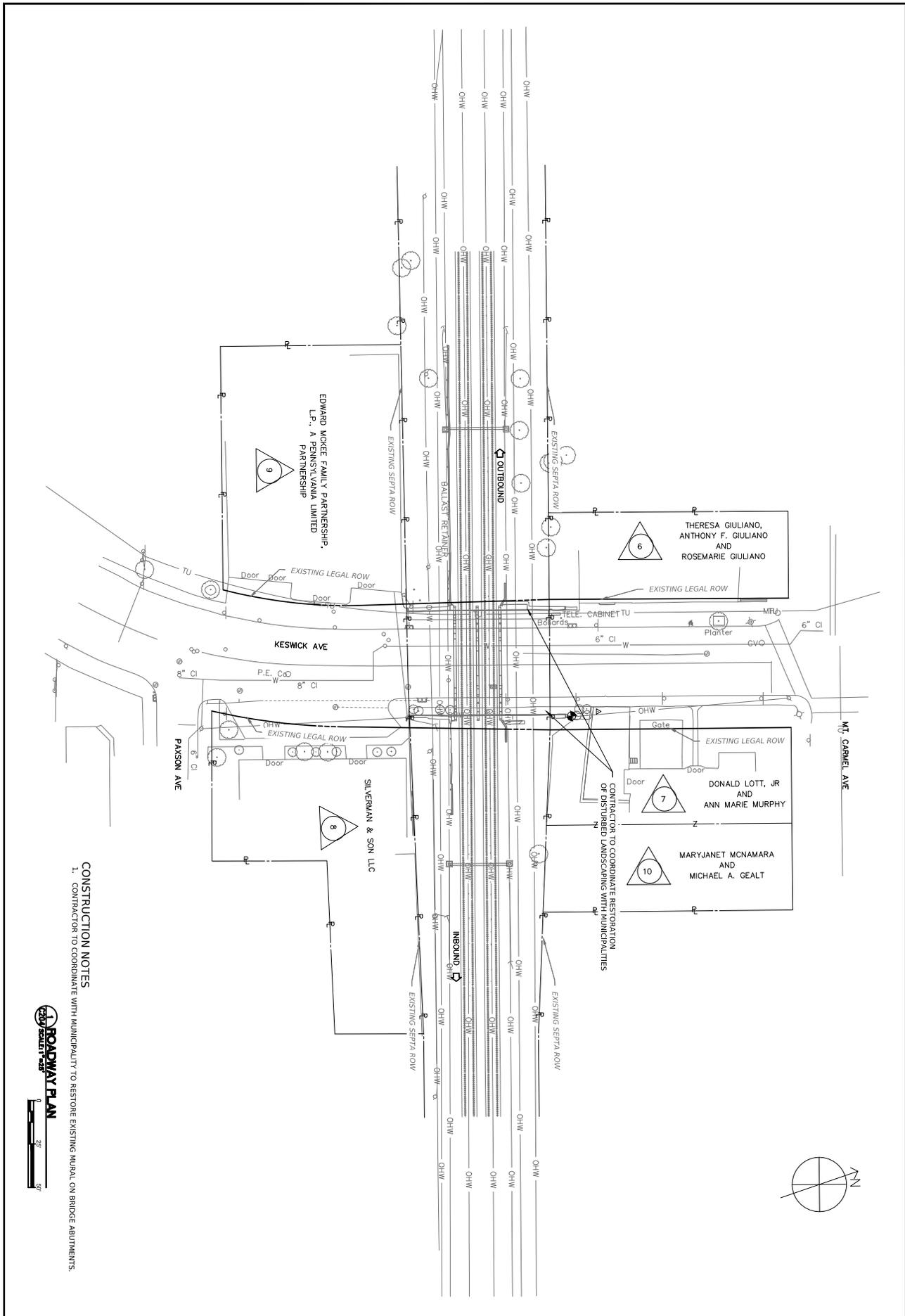
EXISTING CONDITIONS PLAN

BM3: ELEVATION: 239.2133'
 2900083.810' N; 2695993.457' E
 X-CUT IN CONCRETE SIDEWALK
 BM4: ELEVATION: 235.4519'
 2902248.110' N; 2696050.291' E
 X-CUT IN CONCRETE SIDEWALK



		Jacobus ENGINEERING 1000 MARKET STREET, SUITE 200 PHILADELPHIA, PA 19107													
PROJECT: MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE RRD MAINLINE, GLENSIDE, PA BRIDGE 11.62 REHABILITATION CIVIL EXISTING CONDITIONS PLAN	SHEET NO. 03 TOTAL SHEETS 08 DATE: OCT 2024 SCALE: 1" = 25' DRAWN BY: [blank] CHECKED BY: [blank]	REVISIONS: <table border="1"> <tr> <th>NO.</th> <th>DATE</th> <th>DESCRIPTION</th> </tr> <tr> <td>01</td> <td>7/26/24</td> <td>60% SUBMISSION</td> </tr> <tr> <td>02</td> <td>8/20/24</td> <td>100% SUBMISSION</td> </tr> <tr> <td>03</td> <td>10/25/24</td> <td>100% RESUBMISSION</td> </tr> </table>	NO.	DATE	DESCRIPTION	01	7/26/24	60% SUBMISSION	02	8/20/24	100% SUBMISSION	03	10/25/24	100% RESUBMISSION	APPROVED: [blank] DATE: [blank]
NO.	DATE	DESCRIPTION													
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02	8/20/24	100% SUBMISSION													
03	10/25/24	100% RESUBMISSION													

100% RESUBMISSION DATE PRINTED: 10/25/2024 1:15:42 PM C:\P\WORKING\JAC\035_B\10\MARCHESANO\0560082K_SEPTA_C100.DWG



CONSTRUCTION NOTES
 1. CONTRACTOR TO COORDINATE WITH MUNICIPALITY TO RESTORE EXISTING MURAL ON BRIDGE ABUTMENTS.



MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE RRD MAINLINE, GLENSIDE, PA BRIDGE 11.62 REHABILITATION CIVIL ROADWAY PLAN		<table border="1"> <thead> <tr> <th>REV</th> <th>DATE</th> <th>DESCRIPTION</th> <th>BY</th> <th>CHKD</th> <th>APPD</th> </tr> </thead> <tbody> <tr> <td>03</td> <td>10/25/24</td> <td>100% RESUBMISSION</td> <td>DM</td> <td>TAC</td> <td>TAC</td> </tr> <tr> <td>02</td> <td>9/27/24</td> <td>100% SUBMISSION</td> <td>DM</td> <td>TAC</td> <td>TAC</td> </tr> <tr> <td>01</td> <td>7/26/24</td> <td>60% SUBMISSION</td> <td>DM</td> <td>TAC</td> <td>TAC</td> </tr> </tbody> </table>		REV	DATE	DESCRIPTION	BY	CHKD	APPD	03	10/25/24	100% RESUBMISSION	DM	TAC	TAC	02	9/27/24	100% SUBMISSION	DM	TAC	TAC	01	7/26/24	60% SUBMISSION	DM	TAC	TAC
REV	DATE	DESCRIPTION	BY	CHKD	APPD																						
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02	9/27/24	100% SUBMISSION	DM	TAC	TAC																						
01	7/26/24	60% SUBMISSION	DM	TAC	TAC																						
<table border="1"> <tr> <th>NO.</th> <th>DATE</th> <th>DESCRIPTION</th> </tr> <tr> <td>1</td> <td>10/25/24</td> <td>100% SUBMISSION</td> </tr> </table>		NO.	DATE	DESCRIPTION	1	10/25/24	100% SUBMISSION	<table border="1"> <tr> <td>DATE PLOTTED:</td> <td>10/25/24 1:15:42 PM</td> </tr> <tr> <td>SCALE:</td> <td>1" = 25'</td> </tr> <tr> <td>PROJECT NO.:</td> <td>C204</td> </tr> <tr> <td>DRAWING NO.:</td> <td>03</td> </tr> </table>		DATE PLOTTED:	10/25/24 1:15:42 PM	SCALE:	1" = 25'	PROJECT NO.:	C204	DRAWING NO.:	03										
NO.	DATE	DESCRIPTION																									
1	10/25/24	100% SUBMISSION																									
DATE PLOTTED:	10/25/24 1:15:42 PM																										
SCALE:	1" = 25'																										
PROJECT NO.:	C204																										
DRAWING NO.:	03																										

MAINTENANCE AND PROTECTION OF TRAFFIC (MPT) GENERAL NOTES

1. THIS WORK CONSISTS OF THE MAINTENANCE OF TRAFFIC AND THE PROTECTION OF THE TRAVELING PUBLIC APPROACHING THE CONSTRUCTION AREA AND WITHIN THE LIMITS OF CONSTRUCTION.
2. FINISH, ERECT, PLACE, AND MAINTAIN TRAFFIC CONTROL SIGNS AND DEVICES. MAINTAIN TRAFFIC DURING HOURS OF CONSTRUCTION AND AT ALL OTHER TIMES CONSISTENT WITH THE METHODS INDICATED ON THESE DRAWINGS AND IN THE MOST CURRENT EDITIONS OF:
 - PENNDOT PUBLICATION 35, QUALIFIED PRODUCTS LIST FOR CONSTRUCTION (BULLETIN 151);
 - PA CODE, TITLE 67, CHAPTER 212, OFFICIAL TRAFFIC CONTROL DEVICES;
 - PENNDOT PUBLICATION 111, TRAFFIC CONTROL - PAVEMENT MARKINGS AND SIGNING STANDARDS;
 - PENNDOT PUBLICATION 213, TEMPORARY TRAFFIC CONTROL GUIDELINES;
 - PENNDOT PUBLICATION 236, HANDBOOK OF APPROVED SIGNS;
 - PENNDOT PUBLICATION 408, HIGHWAY CONSTRUCTION SPECIFICATIONS;
 - MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD);
3. IMMEDIATELY UPON COMPLETION OF THE WORK, REMOVE THE DEVICES.
4. ARRANGE FOR INSPECTION OF ALL TRAFFIC CONTROL DEVICES PRIOR TO START OF WORK.
5. THE TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF HIS OR HER RESPONSIBILITY AS SPECIFIED IN SECTION 901.21(a) OF PUBLICATION NO. 408/2020, LATEST REVISION.
6. ALL DISTANCES MAY BE ADJUSTED SLIGHTLY TO FIT FIELD CONDITIONS.
7. COVER OR REMOVE ALL CONFLICTING SIGNS OR SIGNS NOT IN USE.
8. MAINTAIN A MINIMUM LANE WIDTH OF 10 FEET.
9. INSTALL AND MAINTAIN TRAFFIC CONTROL SIGNS AND DEVICES THAT ARE IN NEW CONDITION THROUGHOUT THE DURATION OF THE PROJECT. ANY DAMAGE INCURRED SHALL BE IMMEDIATELY REPAIRED OR REPLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE REPRESENTATIVE.
10. LOCATE ALL SIGNS SO THAT SIGHT DISTANCES WILL NOT BE OBSTRUCTED AT DRIVERS' AND LOCAL ROADS.
11. NOTIFY LOCAL EMERGENCY AUTHORITIES (E.G., POLICE, FIRE, MEDICAL, AFFECTED BUSINESSES, SCHOOL DISTRICTS), THE GENERAL PUBLIC, THE DISTRICT PERMIT MANAGER AND THE DISTRICT AUTOMATED PERMIT ROUTING AND ANALYSIS SYSTEM (APRAS) COORDINATOR AT LEAST FOURTEEN (14) DAYS PRIOR TO ANY SIGNIFICANT TRAFFIC IMPACTS (E.G., LATERAL WIDTH RESTRICTIONS LESS THAN 16 FEET), DETOURS.
12. RESTORE ALL TRAVEL LANES TO NORMAL TRAFFIC FLOW AT THE CLOSE OF EACH WORKING PERIOD UNDER SHORT-TERM CONDITIONS AND REMOVE ALL SHORT-TERM WORK ZONE TRAFFIC CONTROL SIGNING.
13. PROTECT ANY EXCAVATING OBSTRUCTIONS OR CONSTRUCTION WORK, SO AS NOT TO EXPOSE PEDESTRIANS TO HAZARDS.
14. TEMPORARILY COVER OR REMOVE ANY SIGNS THAT ARE IN CONFLICT WITH THE TEMPORARY TRAFFIC CONTROL MEASURES. DO NOT ATTACH TAPE OR ADHESIVE TO THE FACE OF ANY SIGNS THAT ARE TEMPORARILY COVERED DURING CONSTRUCTION OPERATIONS. ENSURE DURING THE REMOVAL AND STORAGE OF ALL SIGNS, THAT THE SHEETING FACE IS NOT DAMAGED OR SCRATCHED. REPLACE IN KIND ALL SIGNS AND/OR POSTS DAMAGED DURING REMOVAL, STORAGE, OR REINSTALLATION AT THE CONTRACTOR'S EXPENSE.
15. COMPLY WITH ACT 229 OF DECEMBER 2002 DURING CONSTRUCTION ACTIVITIES.
16. MAINTAIN ACCESS TO LOCAL BUSINESSES AT ALL TIMES.
17. COORDINATE WORK ZONE SIGNING AND OPERATIONS WITH OTHER PROJECTS IN THE AREA.
18. SEQUENCE OF CONSTRUCTION TO BE DETERMINED BY CONTRACTOR AND APPROVED BY THE REPRESENTATIVE PRIOR TO COMMENCING WORK. MAINTENANCE AND PROTECTION OF TRAFFIC DURING CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE APPLICABLE PATA DRAWINGS IN PENNDOT PUBLICATION 213.
19. POST THE SPECIAL SIGN THIS STREET TO BE CLOSED FOR MAINTENANCE (START DATE), TWO WEEKS PRIOR TO THE BEGINNING OF WORK. REMOVE THE SIGNS ONCE CONSTRUCTION BEGINS.
20. NOTIFY THE MUNICIPALITIES AT LEAST 14 DAYS PRIOR TO STARTING EACH PHASE OF WORK AND WITH ANY OTHER ROAD OR SIDEWALK RESTRICTIONS TO ALLOW PUBLIC NOTICE THROUGH THEIR WEBSITE AND NEWSLETTER.
21. NOTIFY SEPTA TWO WEEKS IN ADVANCE OF THE DISCONTINUANCE OF THE EXISTING BUS STOP LOCATIONS, BASED ON ANY PENDING CONSTRUCTION ACTIVITIES THAT WOULD BLOCK ACCESS. ALSO, NOTIFY SEPTA TWO WEEKS IN ADVANCE WHEN NEW STOPS ARE AVAILABLE TO USE.
CONTACT: MICHAEL DAGOSTINO, MDAGOSTINO@SEPTA.ORG
22. COORDINATE ALL PARKING RESTRICTIONS WITH THE MUNICIPALITIES AT LEAST 2 WEEKS IN ADVANCE OF ANY CLOSURES.
23. THESE PLANS ARE NOT INTENDED TO RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY FOR THE PROTECTION OF THE PUBLIC AND THE CONSTRUCTION PERSONNEL AS SPECIFIED IN SECTION 901.31(A) OF PENNDOT PUBLICATION 408. THE STANDARDS PRESCRIBED ARE A MINIMUM AND ADDITIONAL PROTECTION MAY BE NEEDED IF PROBLEMS ARE ENCOUNTERED DURING THE TERM OF THE CONTRACT. CONSISTENTLY REVIEW THIS PLAN FOR ITS ADEQUACY AND RECOMMEND CHANGES FOR DEPARTMENT AND MUNICIPAL APPROVAL WHEN INADEQUACIES ARE DISCOVERED.
24. CONTRACTOR CANNOT IMPLEMENT DETOUR FOR BRIDGES ML 11.62 AND 11.83 CONCURRENTLY.



STATE OF PENNSYLVANIA
DEPARTMENT OF TRANSPORTATION
CONTRACT NO. 2024-01-001
PROJECT NO. 2024-01-001



CONSTRUCTION
BRIDGE 11.62 REHABILITATION
ROADWAY

DATE: 10/24/2024 9:18:47 PM

NO.	DATE	DESCRIPTION
01	07/26/24	60% SUBMISSION
02	07/27/24	100% SUBMISSION
03	10/24/24	100% RESUBMISSION

MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE
RRD MAINLINE, GLENSIDE, PA

BRIDGE 11.62 REHABILITATION ROADWAY

GENERAL NOTES - TRAFFIC

SCALE: 1" = 4'

DATE: OCT 2024

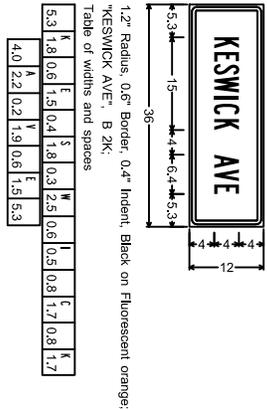
PROJECT NO: 2024-01-001

CONTRACT NO: 2024-01-001

R201

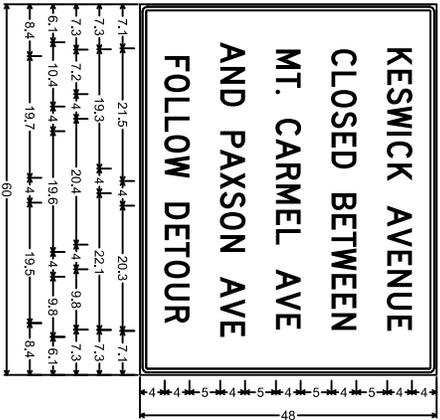
DATE: 10/24/2024 9:18:47 PM

SP-1



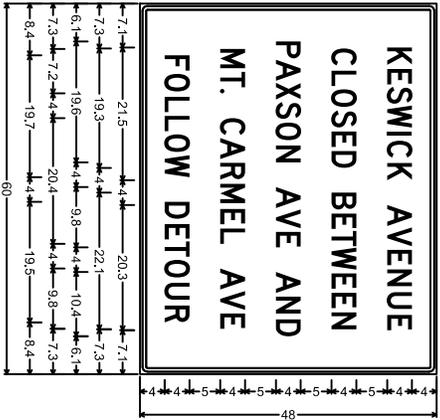
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 KESWICK AVE, B 2K;
 Table of widths and spaces

SP-2



1.2" Radius, 0.6" Border, 0.4" Indent, Black on Fluorescent orange;
 KESWICK AVENUE, D 2K;
 CLOSED BETWEEN, D 2K;
 MT. CARMEL AVE, D 2K;
 AND PAXSON AVE, D 2K;
 FOLLOW DETOUR, D 2K;
 Table of widths and spaces

SP-3



1.2" Radius, 0.6" Border, 0.4" Indent, Black on Fluorescent orange;
 KESWICK AVENUE, D 2K;
 CLOSED BETWEEN, D 2K;
 PAXSON AVE AND, D 2K;
 MT. CARMEL AVE, D 2K;
 FOLLOW DETOUR, D 2K;
 Table of widths and spaces

NOTE: ALL SIGN DETAIL DIMENSIONS SHOWN IN INCHES, UNLESS OTHERWISE NOTED.

Jacobs

100% RESUBMISSION
 100% SUBMISSION
 60% SUBMISSION

PROFESSIONAL ENGINEER
 STATE OF CALIFORNIA
 LICENSE NO. 10000

MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE
 RRD MAINLINE, GLENSIDE, PA

BRIDGE 11.62 REHABILITATION
 ROADWAY

SIGN FABRICATION DETAILS

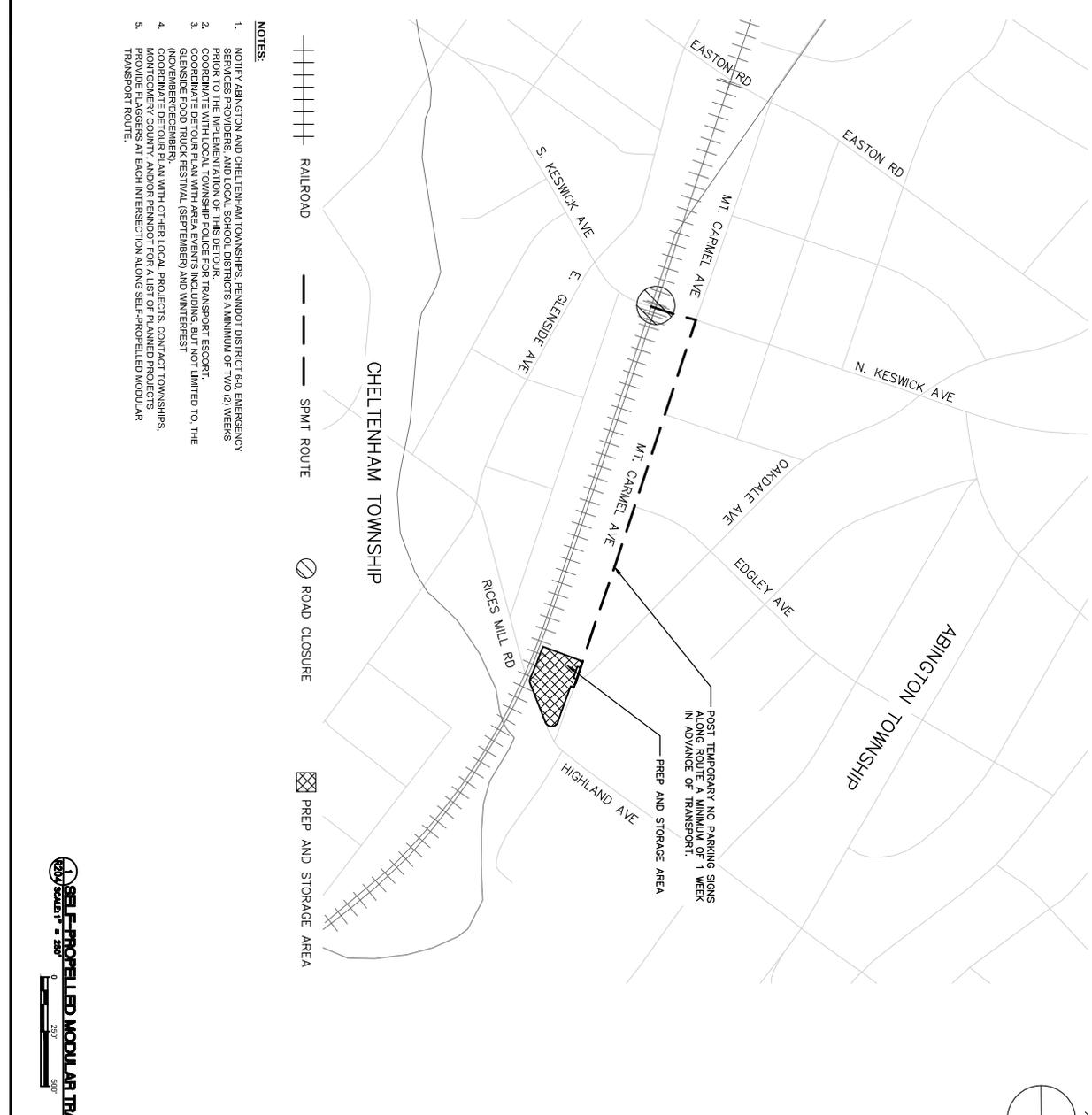
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2	10/27/2024	100% SUBMISSION
3	10/27/2024	60% SUBMISSION

R202

NO SCALE

OCT 2024

DATE PRINTED: 10/24/2024 9:18:47 PM



- NOTES:**
1. NOTIFY ARLINGTON AND CHEL TENHAM TOWNSHIPS, PENNDOT DISTRICT 60, EMERGENCY SERVICES PROVIDERS, AND LOCAL SCHOOL DISTRICTS A MINIMUM OF TWO (2) WEEKS PRIOR TO THE IMPLEMENTATION OF THIS DETOUR.
 2. COORDINATE WITH LOCAL TOWNSHIP POLICE FOR TRANSPORT ESCORT.
 3. COORDINATE WITH LOCAL TOWNSHIP POLICE FOR TRANSPORT ESCORT. (NOT LIMITED TO THE GLENSIDE FOOD TRUCK FESTIVAL (SEPTEMBER) AND WINTERFEST (NOVEMBER/DECEMBER).
 4. COORDINATE DETOURS PLAN WITH OTHER LOCAL PROJECTS, CONTRACT TOWNSHIPS, MONTGOMERY COUNTY, AND/OR PENNDOT FOR A LIST OF PLANNED PROJECTS.
 5. TRANSPORT ROUTE AT EACH INTERSECTION ALONG SELF-PROPELLED MODULAR TRANSPORT ROUTE.

1 SELF-PROPELLED MODULAR TRANSPORT (SPMT) ROUTE

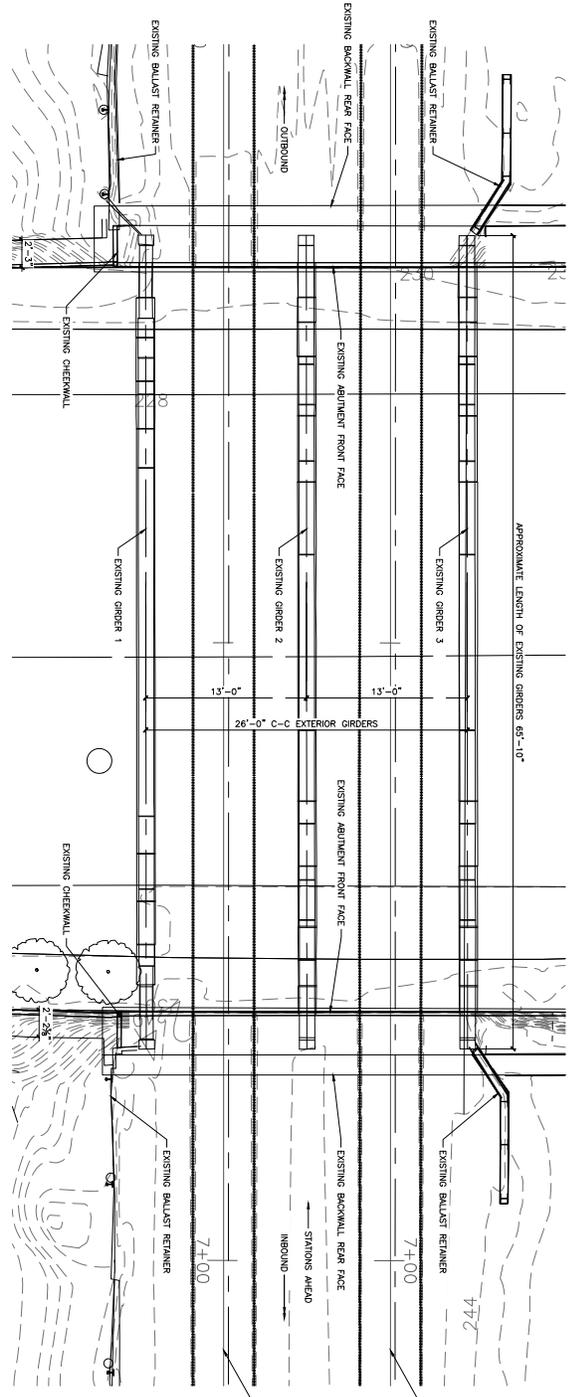
JACOBS
 1000 MARKET STREET, SUITE 200
 PHILADELPHIA, PA 19102
 TEL: 215 381 1000
 FAX: 215 381 1001
 WWW.JACOBS.COM

CONTRACT NO. 11.62
PROJECT NO. R204
DATE: OCT 2024
SCALE: 1" = 250'

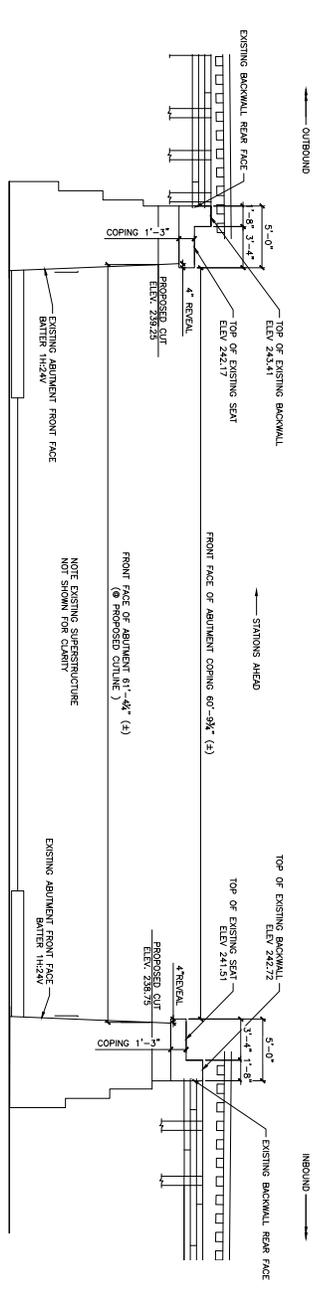
MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE
 RRD MAINLINE, GLENSIDE, PA
BRIDGE 11.62 REHABILITATION
 ROADWAY
 SELF-PROPELLED MODULAR TRANSPORT (SPMT) ROUTE

REV	DATE	DESCRIPTION	BY	CHKD	APPD
03	10/25/24	100% RESUBMISSION	DM	MM	MM
02	9/27/24	100% SUBMISSION	DM	MM	MM
01	7/26/24	60% SUBMISSION	DM	MM	MM

100% RESUBMISSION
 DATE PRINTED: 10/24/2024 9:18:47 PM
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1 PLAN
STRUCTURAL



2 ELEVATION
STRUCTURAL

- NOTES:
1. ALL DIMENSIONS, ELEVATIONS, AND PHYSICAL CONDITIONS SHOWN ON THE DRAWING FOR EXISTING STRUCTURES ARE TO BE VERIFIED BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE APPROPRIATE AGENCIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE APPROPRIATE AGENCIES.
 2. EVALUATE AND FIELD VERIFY ALL EXISTING AND GIVEN DIMENSIONS AND CONDITIONS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE APPROPRIATE AGENCIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE APPROPRIATE AGENCIES.
 3. CONTRACTOR TO VERIFY ALL DIMENSIONS PRIOR TO SHOP DRAWING PREPARATION.

Jacobs

100% RESUBMISSION

100% SUBMISSION

60% SUBMISSION

MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE

RRD MAINLINE, GLENSIDE, PA

BRIDGE 11.62 REHABILITATION

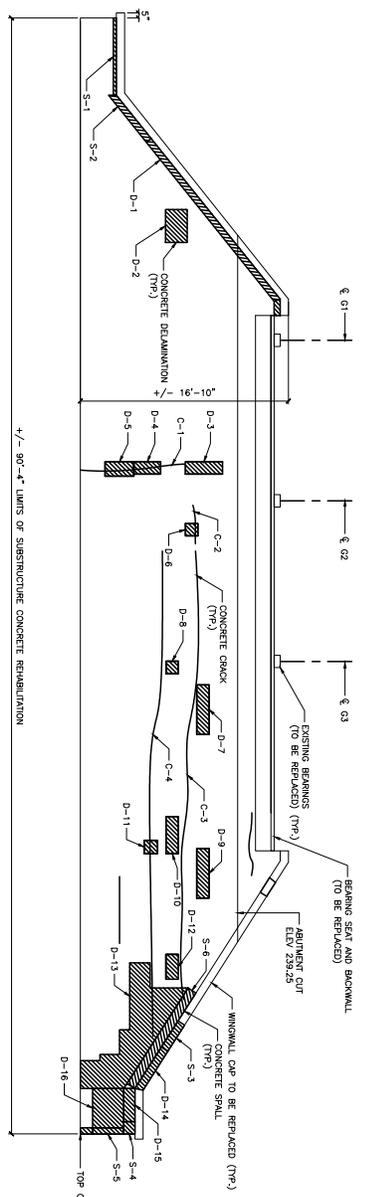
STRUCTURAL

EXISTING PLAN AND ELEVATION

REV	DATE	DESCRIPTION
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02	9/27/24	100% SUBMISSION
03	10/25/24	100% RESUBMISSION

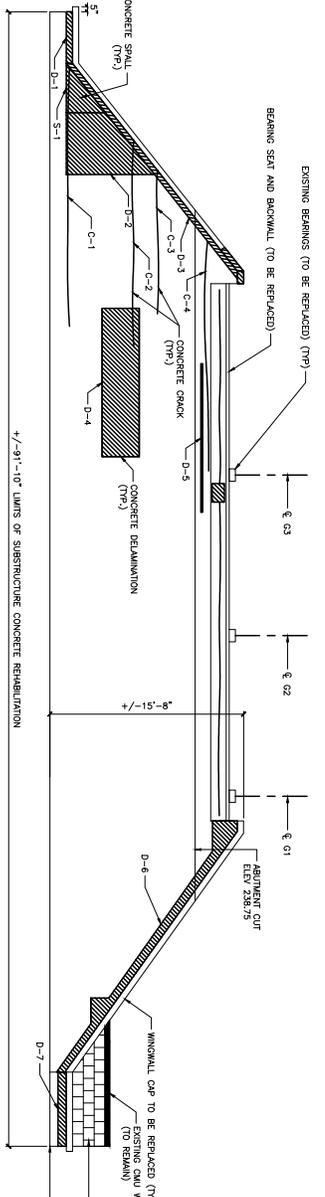
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NORTH ABUTMENT ELEVATION
(LOOKING STATIONS AHEAD)

ITEM NO.	DESCRIPTION	REPAIR TYPE	QTY	F & WHERE DIRECTED
D-1	CONCRETE DELAMINATION	SPALL REPAIR	9 SF	1 SF
D-2	CONCRETE DELAMINATION	SPALL REPAIR	5 SF	1 SF
D-3	CONCRETE DELAMINATION	SPALL REPAIR	3 SF	1 SF
D-4	CONCRETE DELAMINATION	SPALL REPAIR	3 SF	1 SF
D-5	CONCRETE DELAMINATION	SPALL REPAIR	3 SF	1 SF
D-6	CONCRETE DELAMINATION	SPALL REPAIR	4 SF	1 SF
D-7	CONCRETE DELAMINATION	SPALL REPAIR	1 SF	1 SF
D-8	CONCRETE DELAMINATION	SPALL REPAIR	4 SF	1 SF
D-9	CONCRETE DELAMINATION	SPALL REPAIR	3 SF	1 SF
D-10	CONCRETE DELAMINATION	SPALL REPAIR	2 SF	1 SF
D-11	CONCRETE DELAMINATION	SPALL REPAIR	2 SF	1 SF
D-12	CONCRETE DELAMINATION	SPALL REPAIR	13 SF	4 SF
D-13	CONCRETE DELAMINATION	SPALL REPAIR	3 SF	1 SF
D-14	CONCRETE DELAMINATION	SPALL REPAIR	3 SF	1 SF
D-15	CONCRETE DELAMINATION	SPALL REPAIR	6 SF	1 SF
D-16	CONCRETE DELAMINATION	SPALL REPAIR	2 SF	1 SF
S-1	CONCRETE SPALL	SPALL REPAIR	3 SF	1 SF
S-2	CONCRETE SPALL	SPALL REPAIR	2 SF	1 SF
S-3	CONCRETE SPALL	SPALL REPAIR	2 SF	1 SF
S-4	CONCRETE SPALL	SPALL REPAIR	1 SF	1 SF
S-5	CONCRETE SPALL	SPALL REPAIR	2 SF	1 SF
S-6	CONCRETE SPALL	SPALL REPAIR	7 SF	1 SF
C-1	CONCRETE CRACK	CRACK REPAIR	10 LF	1 LF
C-2	HORIZONTAL CRACK	CRACK REPAIR	4 LF	1 LF
C-3	HORIZONTAL CRACK	CRACK REPAIR	37 LF	4 LF
C-4	HORIZONTAL CRACK	CRACK REPAIR	40 LF	4 LF



FAR ABUTMENT ELEVATION
(LOOKING STATIONS AHEAD)

ITEM NO.	DESCRIPTION	REPAIR TYPE	QTY	F & WHERE DIRECTED
D-1	CONCRETE DELAMINATION	SPALL REPAIR	3 SF	1 SF
D-2	CONCRETE DELAMINATION	SPALL REPAIR	28 SF	3 SF
D-3	CONCRETE DELAMINATION	SPALL REPAIR	9 SF	1 SF
D-4	CONCRETE DELAMINATION	SPALL REPAIR	36 SF	4 SF
D-5	CONCRETE DELAMINATION	SPALL REPAIR	20 SF	2 SF
D-6	CONCRETE DELAMINATION	SPALL REPAIR	5 SF	1 SF
D-7	CONCRETE DELAMINATION	SPALL REPAIR	7 SF	1 SF
S-1	CONCRETE SPALL	SPALL REPAIR	20 LF	2 LF
C-1	HORIZONTAL CRACK	CRACK REPAIR	15 LF	2 LF
C-2	HORIZONTAL CRACK	CRACK REPAIR	10 LF	1 LF
C-3	HORIZONTAL CRACK	CRACK REPAIR	20 LF	2 LF
C-4	HORIZONTAL CRACK	CRACK REPAIR	20 LF	2 LF

- LEGEND**
- CONCRETE SPALLING AND DELAMINATION
 - CONCRETE CRACKING
- KEY:**
- D - DELAMINATION
 - S - SPALL
 - C - CRACK

NEAR ABUTMENT ELEVATION
(LOOKING STATIONS BACK)

- NOTES**
- ALL DIMENSIONS, ELEVATIONS, AND PHYSICAL CONDITIONS SHOWN ON THE DRAWING FOR THE EXISTING STRUCTURES ARE BASED ON LIMITED FIELD INSPECTIONS. CERTAIN DESIGN DRAWINGS FOR ORIGINAL STRUCTURES ARE NOT AVAILABLE. DIMENSIONS AND ELEVATIONS ARE APPROXIMATE AND LIMITED TO THOSE AREAS FOR WHICH WORK IS REQUIRED. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND ELEVATIONS AT THE SITE APPLICABLE TO THE WORK.
 - THE EXACT EXTENT OF CONSTRUCTION OR RESTORATION WORK CANNOT BE NECESSARILY OR ACCURATELY DETERMINED FROM THE COMPARISON OF AVAILABLE FIELD CONDITIONS WITH REPAIR RECOMMENDATIONS ON DRAWINGS. PERFORM THE WORK TO MEET FIELD CONDITIONS ENCOUNTERED.
 - ERASING AND FIELD VERIFY ALL SPALLS AND CRACK DIMENSIONS AND LOCATIONS PRIOR TO COMMENCEMENT OF THE WORK AND PROTECTIVE INTERFERENCES PROMPTLY TO THE SEPA PROJECT MANAGER. THE CONTRACTOR SHALL COORDINATE WITH SEPA FOR THE RESTORATION/REPAIRING BY BRIDGE 11.62. ARIZONA UNIVERSITY - ML BRIDGE 11.62). THE CONTRACTOR SHALL PROVIDE ACCESS AND ASSIST WITH PREPARING THE CONCRETE SURFACES FOR PAINTING.
 - REFER TO PRECAST ABUTMENT CAP AND WINDOW WALL CAP FOR ADDITIONAL INFORMATION.
 - CONTRACTOR TO VERIFY ALL DIMENSIONS PRIOR TO SHOP DRAWING PREPARATION.
 - THE CONTRACTOR SHALL MINIMIZE DISTURBANCE TO EXISTING TRAFFIC. UPON COMPLETION OF CONCRETE REPAIR WORK, THE CONTRACTOR SHALL COORDINATE WITH SEPA FOR THE RESTORATION/REPAIRING BY BRIDGE 11.62. ARIZONA UNIVERSITY - ML BRIDGE 11.62). THE CONTRACTOR SHALL PROVIDE ACCESS AND ASSIST WITH PREPARING THE CONCRETE SURFACES FOR PAINTING.

JACOBS

1000 RIVER STREET, SUITE 1000
DENVER, CO 80202
TEL: 303.733.8000
WWW.JACOBS.COM

PROFESSIONAL ENGINEER
STATE OF ARIZONA
LICENSE NO. 11874
DATE: 11/11/2019

MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE
RRD MAINLINE, GLENSIDE, PA

BRIDGE 11.62 REHABILITATION
STRUCTURAL

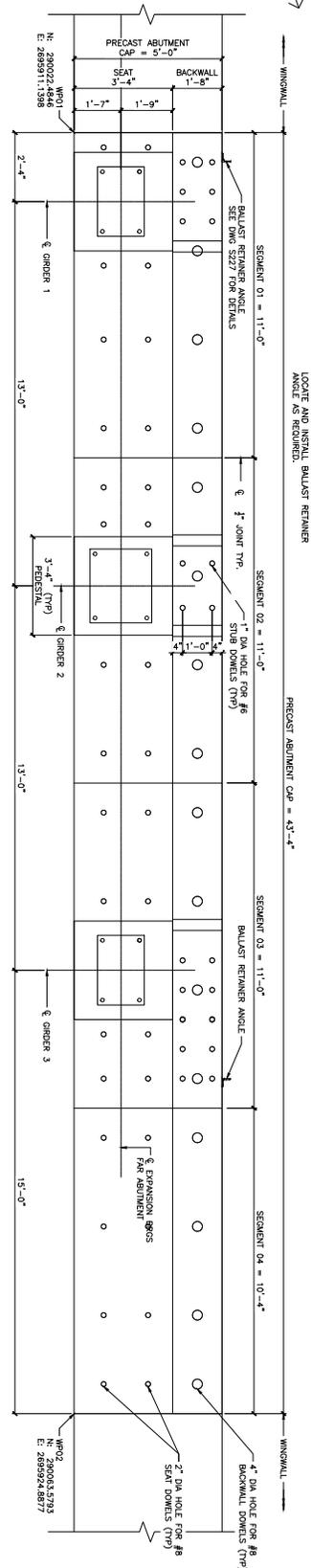
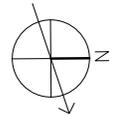
SUBSTRUCTURE REHABILITATION

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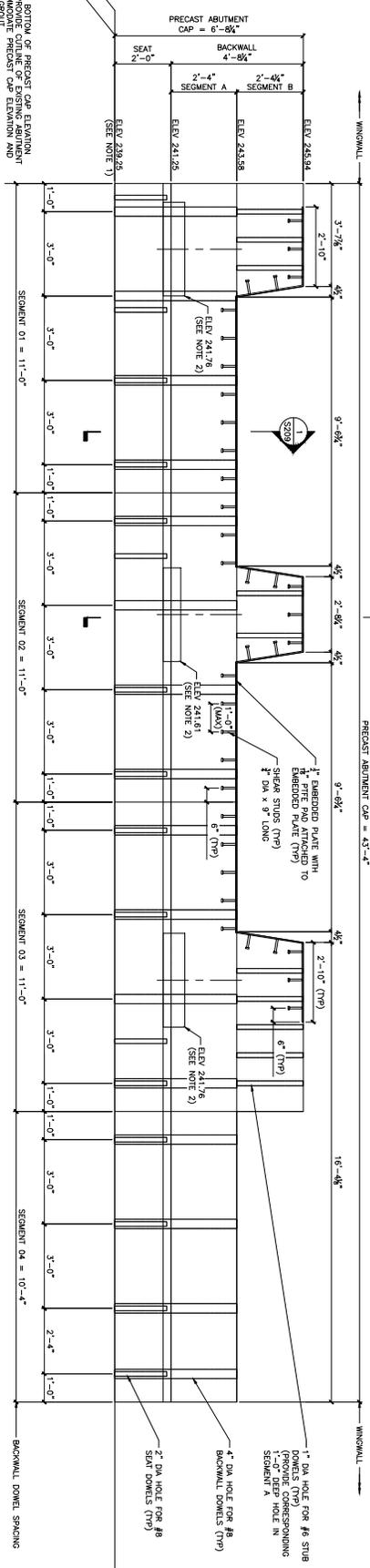
100% RESUBMISSION

DATE: 10/25/2024

DATE: 10/25/2024



2020 FAR ABUTMENT PRECAST CAP PLAN



2020 FAR ABUTMENT PRECAST CAP ELEVATION

- NOTES:
- REQUIRED BOTTOM OF PRECAST CAP ELEVATION SHOWN. REMOVE CURB OF EXISTING ABUTMENT LEAVING GROUT.
 - PRECAST ELEVATIONS NEED TO BE REQUESTED. FABRICATOR MUST PRECAST HEIGHTS AS REQUIRED TO MAINTAIN AS-DESIGNED BOTTOM OF CURB ELEVATIONS.

2020 FAR ABUTMENT PRECAST CAP PLAN AND ELEVATION

NO.	DATE	DESCRIPTION
1	10/25/2024	100% RESUBMISSION
2	10/25/2024	100% SUBMISSION
3	10/25/2024	60% SUBMISSION

MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE
RRD MAINLINE, GLENSIDE, PA

BRIDGE 11.62 REHABILITATION
STRUCTURAL

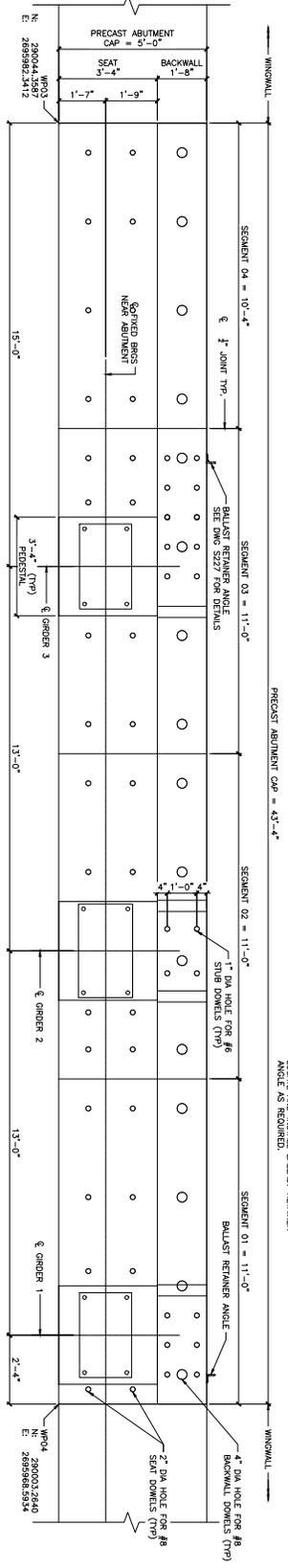
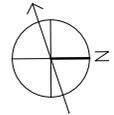
FAR ABUTMENT PRECAST CAP PLAN AND ELEVATION

NO.	DATE	DESCRIPTION
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3	10/25/2024	60% SUBMISSION

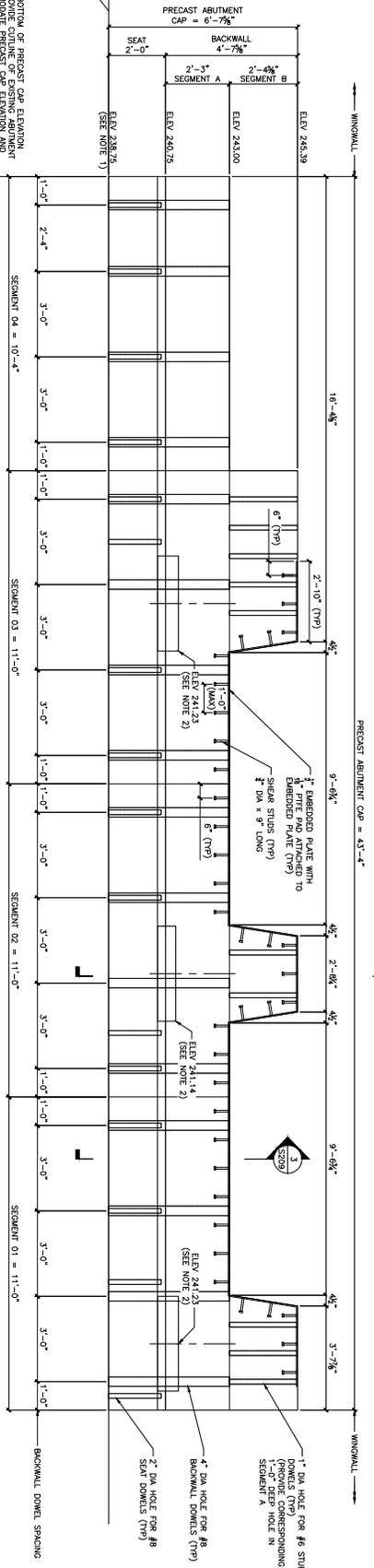
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DATE PRINTED: 10/25/2024 2:26:33 PM



NEAR ABUTMENT PRECAST CAP PLAN



NEAR ABUTMENT PRECAST CAP ELEVATION



- NOTES:
- REQUIRED BOTTOM OF PRECAST CAP ELEVATION SHOWN. REMOVE OUTLINE OF EXISTING ABUTMENT SETTING GRADE.
 - PRECAST CAP ELEVATION AND REINFORCEMENT ELEVATIONS NEED TO BE REQUESTED BY FABRICATOR. ADJUST PRECAST HEIGHTS AS REQUIRED TO MAINTAIN AS-DESIGNED BOTTOM OF PRECAST ELEVATION.

JACOBS

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N: 28999625934
E: 28999625934

JACOBS

2000032840
N: 28999625934
E: 28999625934

DATE: 10/25/2024 2:26:33 PM

SCALE: 1/2"=1'-0"

NO.	DATE	DESCRIPTION
1	10/25/2024	100% RESUBMISSION
2	10/25/2024	100% SUBMISSION
3	10/25/2024	60% SUBMISSION

MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE
RRD MAINLINE, GLENSIDE, PA

BRIDGE 11.62 REHABILITATION
STRUCTURAL

NEAR ABUTMENT PRECAST CAP PLAN AND ELEVATION

NO.	DATE	DESCRIPTION
1	10/25/2024	100% RESUBMISSION
2	10/25/2024	100% SUBMISSION
3	10/25/2024	60% SUBMISSION

NO.	DATE	DESCRIPTION
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2	10/25/2024	100% SUBMISSION
3	10/25/2024	60% SUBMISSION

NO.	DATE	DESCRIPTION
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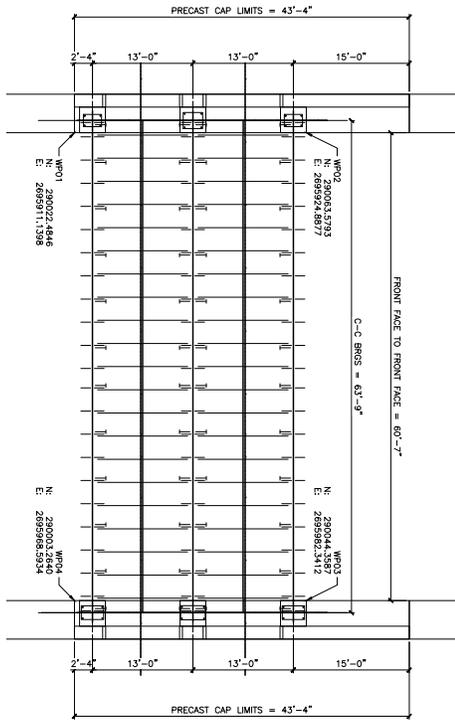
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2	10/25/2024	100% SUBMISSION
3	10/25/2024	60% SUBMISSION

NO.	DATE	DESCRIPTION
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2	10/25/2024	100% SUBMISSION
3	10/25/2024	60% SUBMISSION

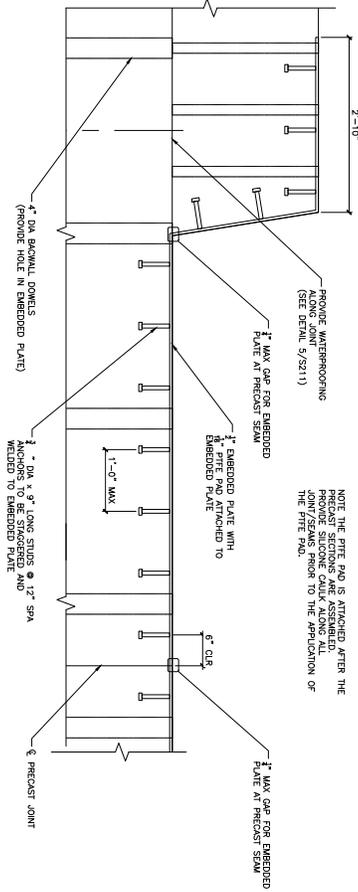
NO.	DATE	DESCRIPTION
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2	10/25/2024	100% SUBMISSION
3	10/25/2024	60% SUBMISSION

NO.	DATE	DESCRIPTION
1	10/25/2024	100% RESUBMISSION
2	10/25/2024	100% SUBMISSION
3	10/25/2024	60% SUBMISSION

1 ABUTMENT CAP STAKE-OUT PLAN

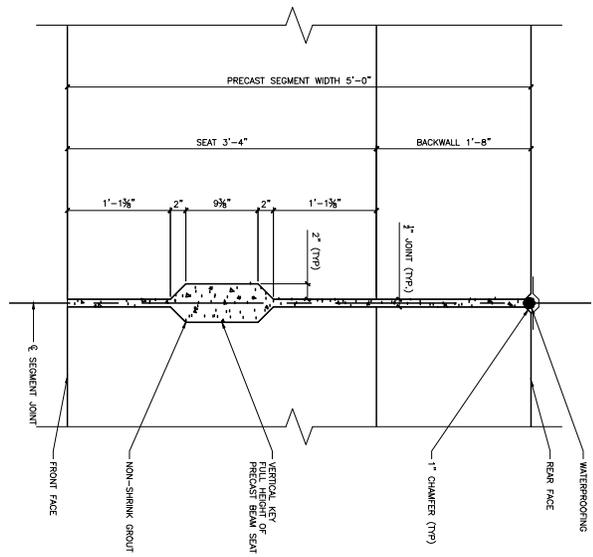


1 EMBEDDED PLATE PARTIAL ELEVATION

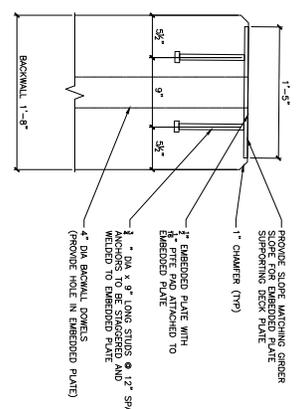


NOTE THE PTFE PAD IS ATTACHED AFTER THE CONCRETE CURES. PROVIDE SLOTTED CHANNELS ALONG ALL JOINT/SEAMS PRIOR TO THE APPLICATION OF THE PTFE PAD.

2 SEGMENT JOINT DETAIL



2 EMBEDDED PLATE SECTION



JACOBS
ENGINEERING CONSULTANTS ARCHITECTS INC.

100% RESUBMISSION

DATE: 10/25/2024 2:26:34 PM

BRIDGE 11.62 REHABILITATION
STRUCTURAL

PRECAST CAP DETAILS - 1

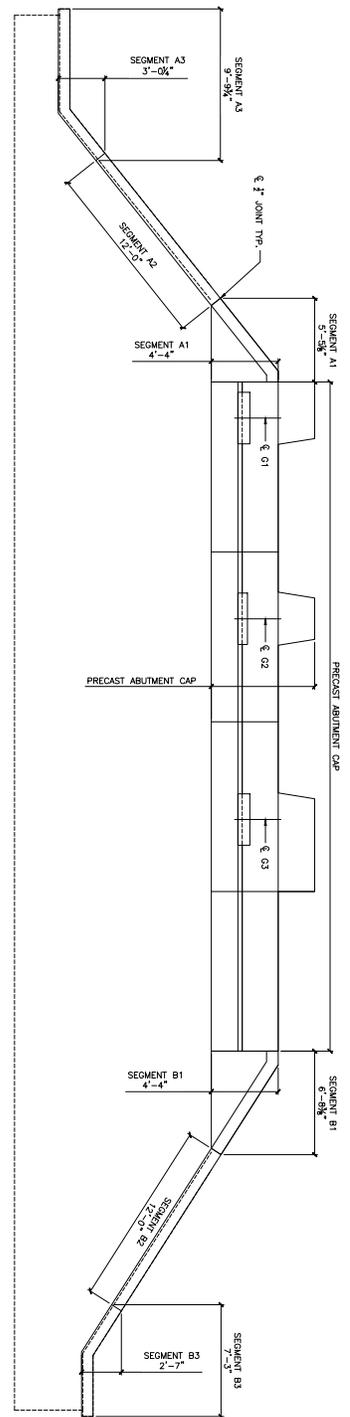
NO.	DATE	DESCRIPTION
01	7/24/24	60% SUBMISSION
02	7/27/24	100% SUBMISSION
03	10/25/24	100% RESUBMISSION

210

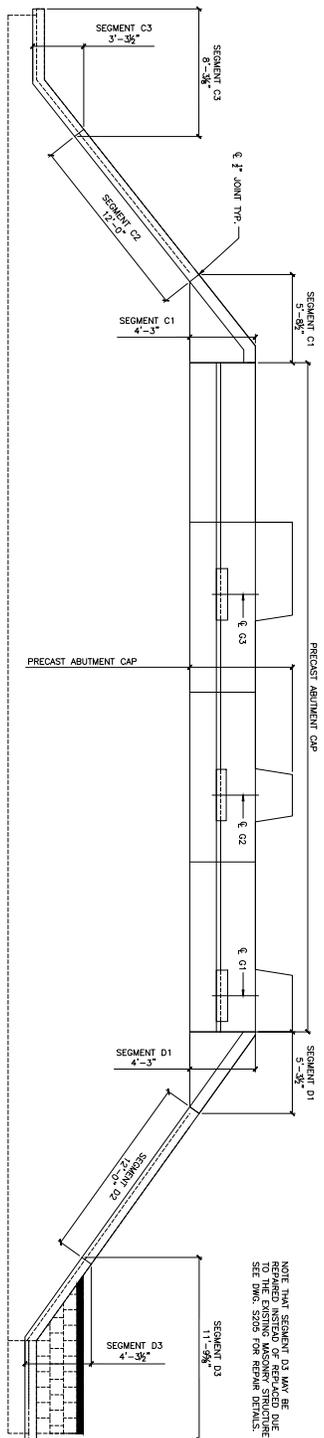
OCT 2024

AS NOTED

11



1 FAR ABUTMENT ELEVATION
 0 10' 20'
 LOOKING STATIONS AHEAD

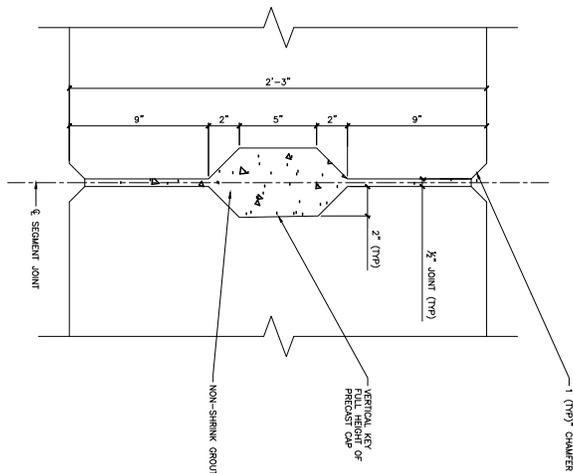
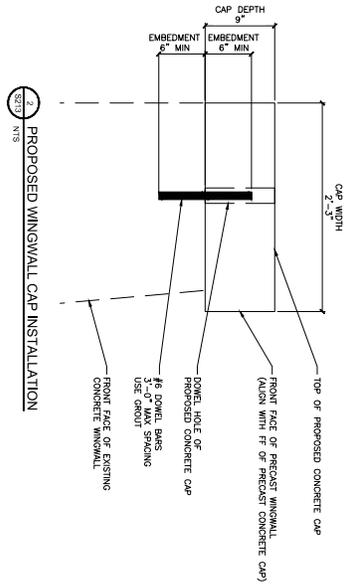
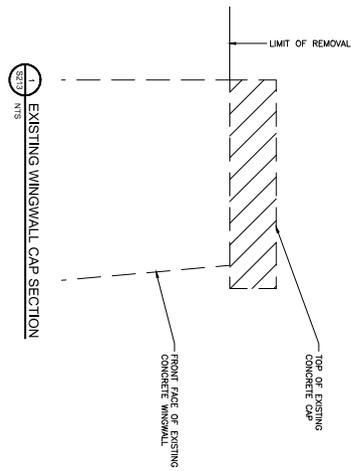


2 NEAR ABUTMENT ELEVATION
 0 10' 20'
 LOOKING STATIONS BACK

NOTE THAT SEGMENT D3 MAY BE DUE TO THE EXISTING MASONRY STRUCTURE. SEE DWG. S208 FOR REPAIR DETAILS.

- NOTES:
1. ALL DIMENSIONS, ELEVATIONS, AND PHYSICAL CONDITIONS SHOWN ON THE DRAWING FOR THE EXISTING STRUCTURES ARE BASED ON LIMITED SURVEY DATA AND FIELD VERIFICATION. THE EXISTING STRUCTURES ARE LIMITED TO THOSE AREAS FOR WHICH WORK IS REQUIRED, AND ARE PROVIDED ONLY FOR THE CONVENIENCE OF EXISTING CONDITIONS AT THE SITE APPLICABLE TO THE WORK.
 2. THE EXACT EXTENT OF CONSTRUCTION OR RESTORATION WORK CANNOT BE NECESSARILY OR ACCURATELY DETERMINED PRIOR TO COMMENCEMENT OF WORK. ACTUAL FIELD CONDITIONS MAY REQUIRE MODIFICATIONS TO THE CONSTRUCTION DETAILS, MATERIAL QUANTITIES, AND EXTENT OF THE MODIFICATION WORK SHOWN ON DRAWINGS. PERFORM THE WORK TO MEET FIELD CONDITIONS ENCOUNTERED.
 3. EXAMINE AND FIELD VERIFY ALL EXISTING AND GIVEN DIMENSIONS AND CONDITIONS PRIOR TO COMMENCEMENT OF THE WORK AND FABRICATION OF CONSTRUCTION MATERIALS. REPORT VARIANCES FROM THE DRAWINGS AND SPECIFICATIONS AND POTENTIAL INTERFERENCES PROMPTLY TO THE ARCHITECT AND ENGINEER. FIELD CONDITIONS AND DIMENSIONS IN THE SHOP AND DESIGN PLANS, INDICATE CHANGES AND ADJUSTMENTS ON DRAWINGS SUBMITTED.
 4. REFER TO SUBSTRUCTURE REHABILITATION AND PRECAST ABUTMENT CAP FOR ADDITIONAL INFORMATION.
 5. CONTRACTORS TO VERIFY ALL DIMENSIONS PRIOR TO SHOP DRAWING PREPARATION.

MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE RRD MAINLINE, GLENSIDE, PA		BRIDGE 11.62 REHABILITATION STRUCTURAL PRECAST WINGWALL CAP ELEVATIONS	
DATE: 10/25/2024 2:26:38 PM DRAWN BY: [Name] CHECKED BY: [Name]	01 10/25/24 100% RESUBMISSION 02 10/25/24 100% SUBMISSION 03 10/25/24 60% SUBMISSION	01 10/25/24 100% RESUBMISSION 02 10/25/24 100% SUBMISSION 03 10/25/24 60% SUBMISSION	PROJECT NO: 2024-01-001 SHEET NO: 11 TOTAL SHEETS: 11



- NOTES:
1. PROVIDE 6" MIN EMBEDMENT INTO SOUND CONCRETE OF EXISTING WINGWALL FOR THE PRECAST WINGWALL CAP- DOWEL BARS. USE HIGH STRENGTH GROUT FOR EXISTING SECTION.
 2. CONTRACTORS TO VERIFY ALL DIMENSIONS PRIOR TO SHOP DRAWING PREPARATION.

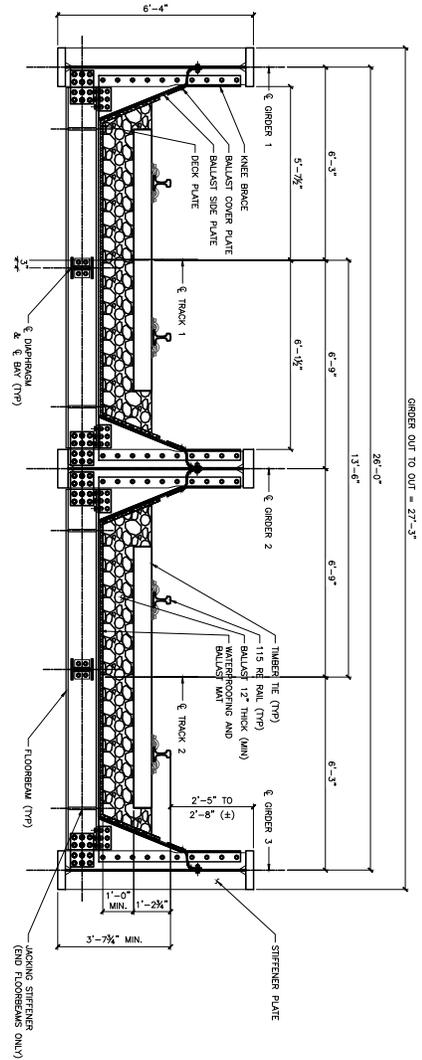


MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE
 RRD MAINLINE, GLENSIDE, PA
BRIDGE 11.62 REHABILITATION
STRUCTURAL
 PRECAST WINGWALL CAP DETAILS

REV	DATE	DESCRIPTION
01	7/24/24	60% SUBMISSION
02	9/22/24	100% SUBMISSION
03	10/25/24	100% RESUBMISSION

SHEET NO. **S213**
 OF 58
 DATE: OCT 2024
 DRAWN BY: 11
 CHECKED BY: 11
 PROJECT NO. 24-08-00000-01

100% RESUBMISSION
 DATE PRINTED: 10/25/2024 2:26:38 PM
 C:\P\WORKING\JACOBS_BAUJEG_SIMAZ\0560089\SEPTA_IL_BR_11.62_40_WINGWALL CAP.DWG

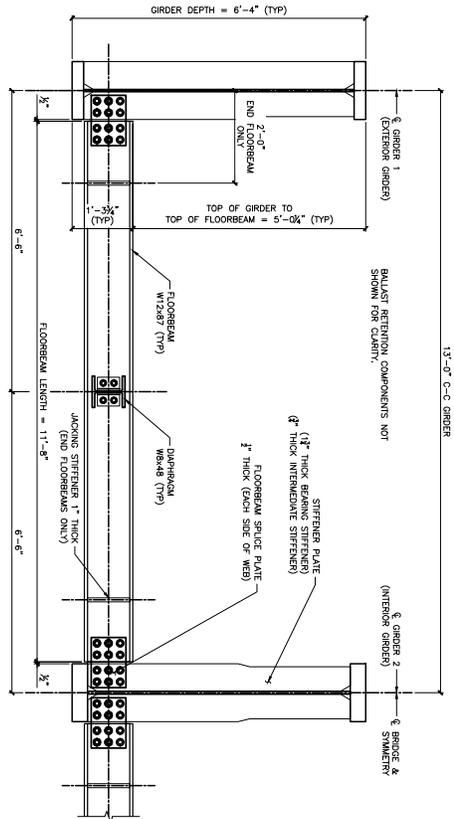


1. TYPICAL SECTION
LOOKING STATIONS AHEAD

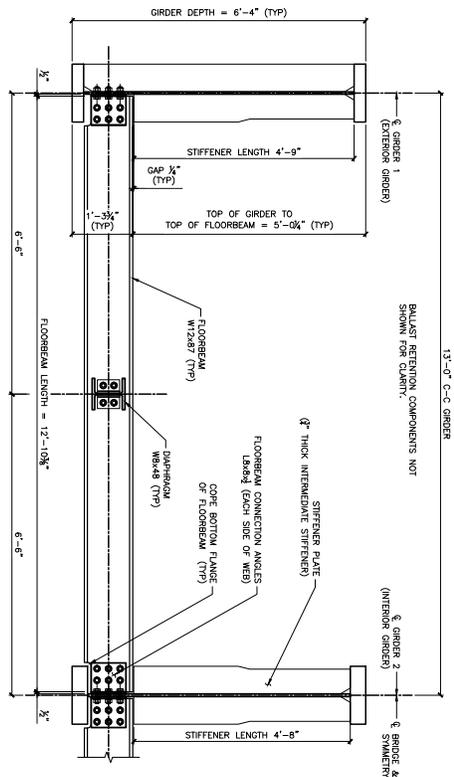
FRAMING SECTION TYPE A SHOW, TYPE B IS SIMILAR

GIRDER OUT TO OUT = 27'-3"

- NOTES:
1. TWO TYPES OF FLOORBEAM CONNECTION ARE REQUIRED, SEE FRAMING PLAN AND
 2. SEE DETAILS SHEETS FOR TYPE A AND TYPE B FLOORBEAM CONNECTIONS.



2. FRAMING STEEL SECTION - TYPE A



3. FRAMING STEEL SECTION - TYPE B

JACOBS

100% RESUBMISSION

DATE: 10/25/2024 2:26:42 PM

PROJECT: BRIDGE 11.62 REHABILITATION

DATE: 10/25/2024

NO. 11

REV. 03

REV.	DATE	DESCRIPTION
01	7/24/24	60% SUBMISSION
02	9/27/24	100% SUBMISSION
03	10/25/24	100% RESUBMISSION

JACOBS

100% RESUBMISSION

DATE: 10/25/2024 2:26:42 PM

PROJECT: BRIDGE 11.62 REHABILITATION

DATE: 10/25/2024

NO. 11

REV. 03



PA 11.62_50 STEEL SUPERSTRUCTURE.DWG
 DATE: 09/24/2024 11:00 AM
 PROJECT: BRIDGE 11.62 REHABILITATION
 SHEET: 11 OF 11

100% RESUBMISSION
 DATE: 10/25/2024 2:26:43 PM

Jacobs
 ENGINEERING GROUP, INC.
 1000 PENNSYLVANIA AVENUE
 SUITE 1000
 PHILADELPHIA, PA 19106

NO.	DATE	DESCRIPTION
01	7/26/24	60% SUBMISSION
02	9/27/24	100% SUBMISSION
03	10/25/24	100% RESUBMISSION

MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE
 RRD MAINLINE, GLENSIDE, PA

BRIDGE 11.62 REHABILITATION
 STRUCTURAL
 GIRDER DETAILS

DATE: 10/25/2024 2:26:43 PM

PROJECT: BRIDGE 11.62 REHABILITATION
 SHEET: 11 OF 11

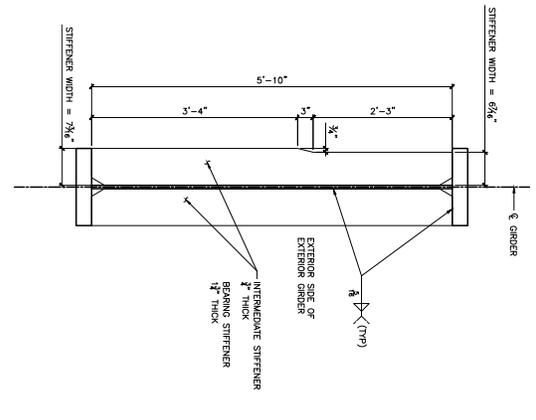
SCALE: AS NOTED

DATE: OCT 2024

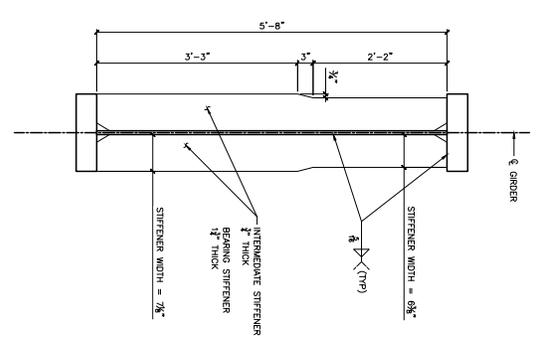
PROJECT NO: 24-00000000-00

DRAWING NO: **S217**

DESIGNED BY: [Blank]
 CHECKED BY: [Blank]
 APPROVED BY: [Blank]



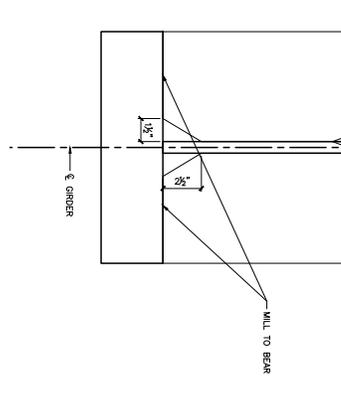
3 EXTERIOR GIRDER STIFFENER DETAIL - TYPE A



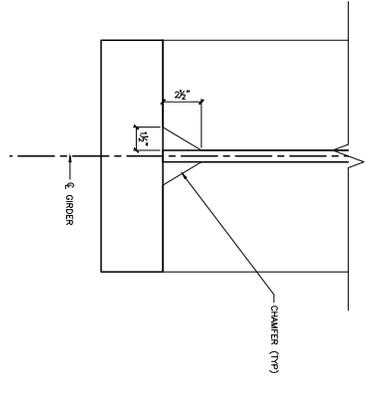
2 INTERIOR GIRDER STIFFENER DETAIL - TYPE A



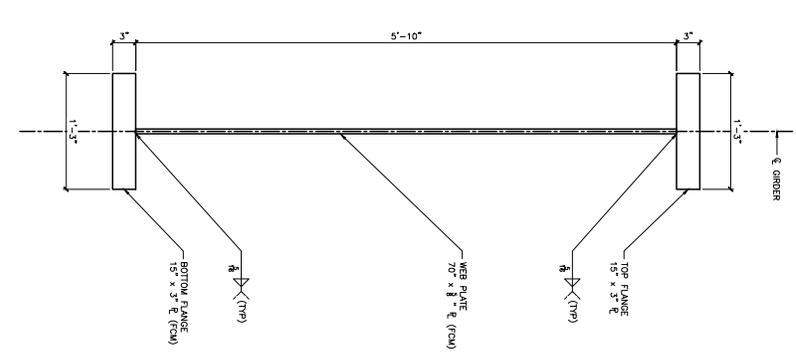
4 MILL TO BEAR



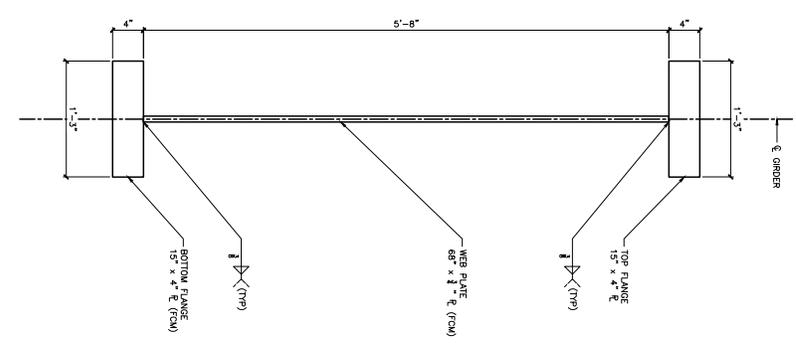
5 BEARING STIFFENER



6 STIFFENER CLIP DETAIL

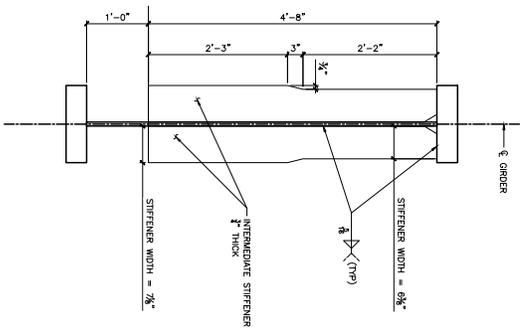


2 EXTERIOR GIRDER SECTION

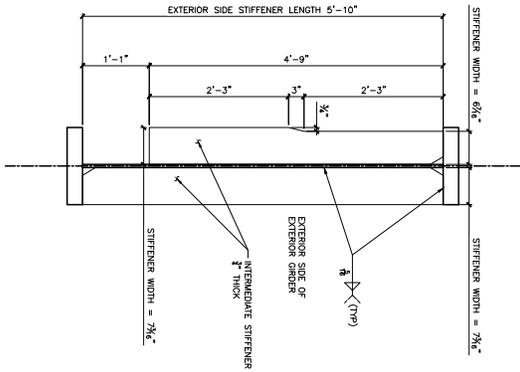


1 INTERIOR GIRDER SECTION

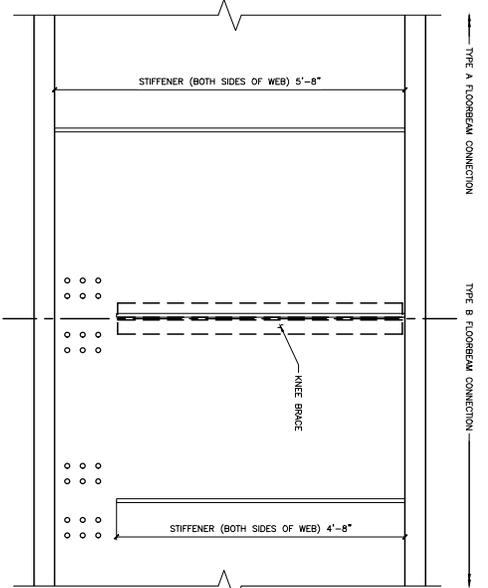
- NOTES:
1. TWO TYPES OF FLOORBEAM CONNECTIONS ARE REQUIRED. SEE FRAMING PLAN FOR DETAILS.
 2. GIRDER SECTIONS ARE APPLICABLE FOR BOTH TYPES OF FLOORBEAM CONNECTIONS.
 3. STIFFENER DETAILS SHOWN ARE APPLICABLE FOR TYPE A FLOORBEAM CONNECTIONS.
 4. SEE DETAILS SHEETS FOR TYPE A AND TYPE B FLOORBEAM CONNECTIONS.



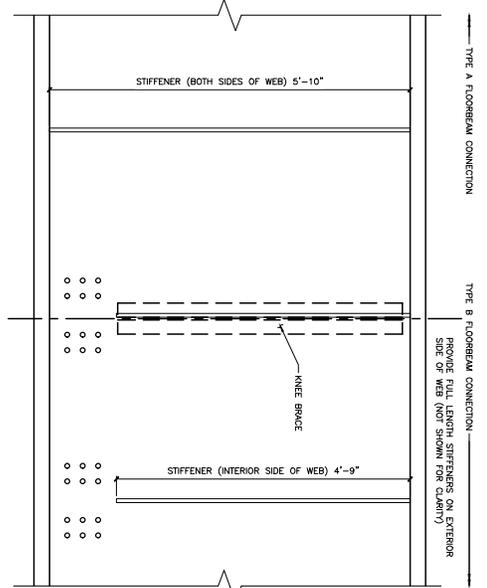
1 INTERIOR GIRDER STIFFENER DETAIL



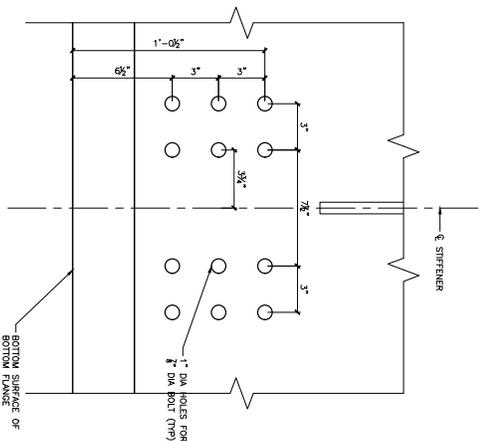
3 EXTERIOR GIRDER STIFFENER DETAIL



2 INTERIOR GIRDER STIFFENER DETAIL



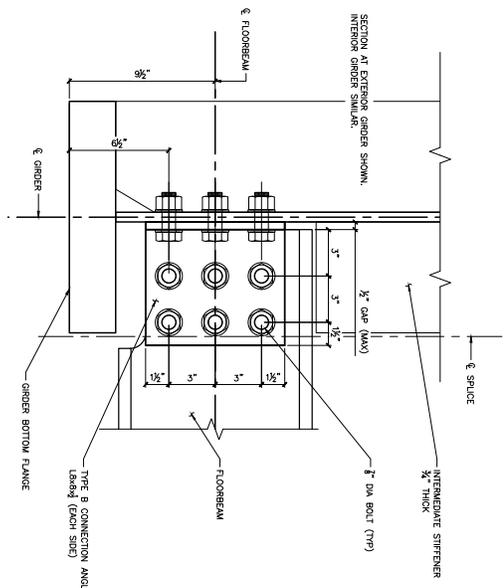
4 EXTERIOR GIRDER STIFFENER DETAIL



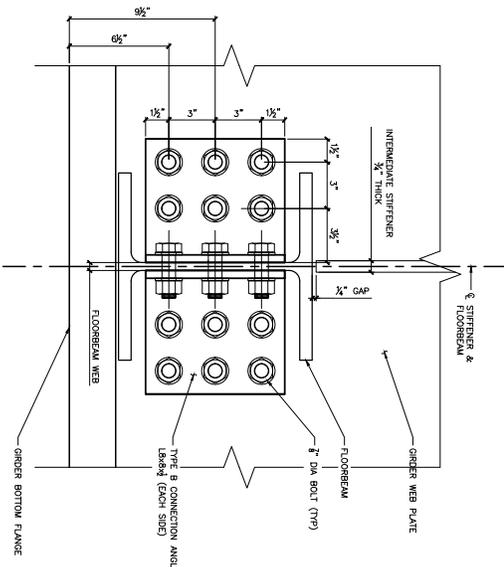
5 INTERIOR GIRDER STIFFENER DETAIL

- NOTES:
1. TWO TYPES OF FLOORBEAM CONNECTIONS ARE REQUIRED. SEE FRAMING PLAN AND GIRDER ELEVATION SHEETS FOR LIMITS.
 2. CONNECTIONS SHOWN ARE APPLICABLE FOR TYPE B FLOORBEAM CONNECTIONS.
 3. SEE DETAILS SHEET FOR TYPE B FLOORBEAM CONNECTIONS.
 4. PROVIDE CLASS B FINISH SURFACE FOR ALL FLOORBEAM CONNECTIONS.

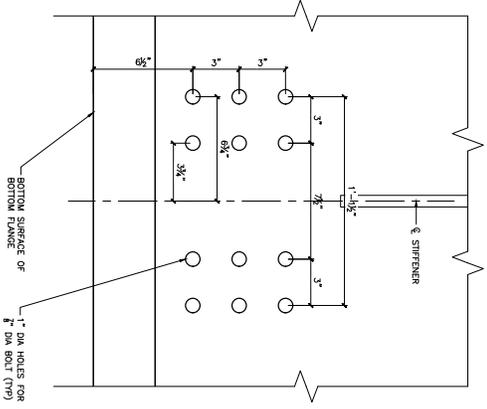
		BRIDGE 11.62 REHABILITATION STRUCTURAL GIRDER DETAILS - TYPE B	
PROJECT NO: \$218 DATE: OCT 2024 SHEET NO: 11 OF 11	MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE RRD MAINLINE, GLENSIDE, PA	03 10/25/24 100% RESUBMISSION 02 10/25/24 100% SUBMISSION 01 7/26/24 60% SUBMISSION	REVISIONS:
DATE: 10/25/2024 2:26:44 PM 100% RESUBMISSION	DATE PRINTED: 10/25/2024 2:26:44 PM	C:\PWORKING\GUACOBBS_B&J\EG_SIN\20250608\9K_SEPTA_ML_BR_11.62_50 STEEL SUPERSTRUCTURE.DWG	PROJECT NO: \$218 SHEET NO: 11 OF 11



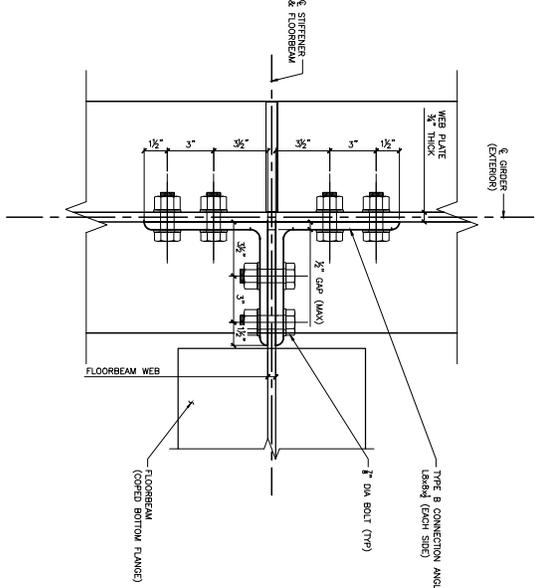
1 FLOORBEAM CONNECTION DETAIL - 1



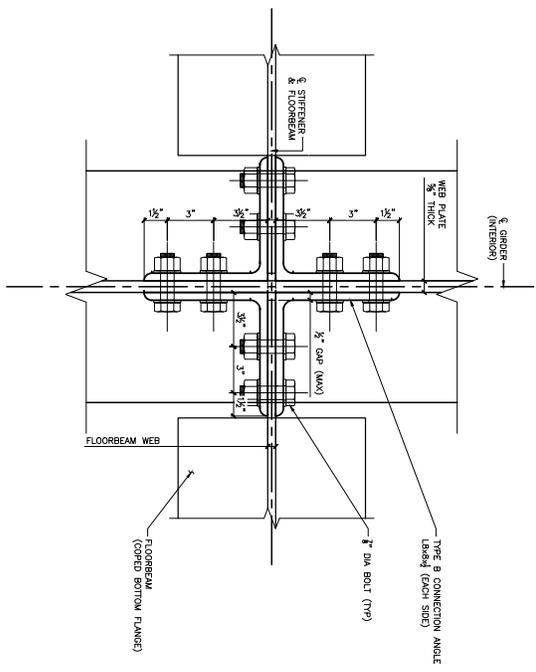
2 FLOORBEAM CONNECTION DETAIL - 2



5 FLOORBEAM CONNECTION DETAIL - 5



3 FLOORBEAM CONNECTION DETAIL - 3



4 FLOORBEAM CONNECTION DETAIL - 4

- NOTES:
1. ALL BOLTS ARE ASU AT 120 OR 125 GAP CRITICAL BOLTS.
 2. ALL ANGLES SHALL BE THERMALLY CUT AND PLANED.
 3. TWO TYPES OF FLOORBEAM CONNECTIONS ARE REQUIRED. SEE FRAMING PLAN AND GIRDER ELEVATION SHEETS FOR LIMITS.
 4. PROVIDE CLASS B PAVING SURFACE FOR ALL FLOORBEAM CONNECTIONS.

JACOBS

1000 CALIFORNIA STREET, SUITE 200
 SAN FRANCISCO, CA 94109
 TEL: 415.424.4000
 WWW.JACOBS.COM

PROFESSIONAL ENGINEER

DAVID M. HARRIS

1000 CALIFORNIA STREET, SUITE 200
 SAN FRANCISCO, CA 94109
 TEL: 415.424.4000
 WWW.JACOBS.COM

DATE: 10/25/2024 2:26:46 PM

PROJECT: MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE
 RRD MAINLINE, GLENSIDE, PA

BRIDGE 11.62 REHABILITATION
 STRUCTURAL

FLOORBEAM CONNECTION DETAILS - TYPE B

NO.	DATE	DESCRIPTION
1	10/25/2024	100% RESUBMISSION
2	10/25/2024	100% SUBMISSION
3	10/25/2024	60% SUBMISSION
4	10/25/2024	60% SUBMISSION
5	10/25/2024	60% SUBMISSION

AS NOTED

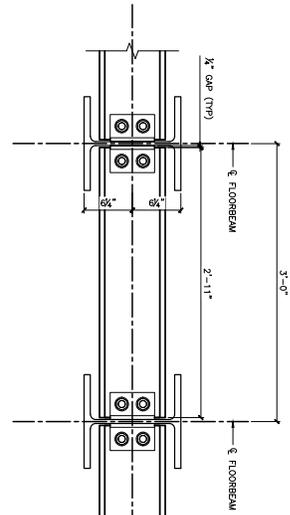
OCT 2024

11

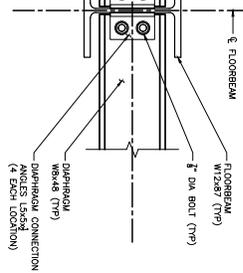
100% RESUBMISSION

100% SUBMISSION

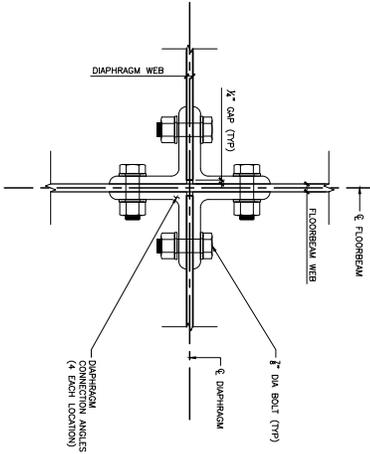
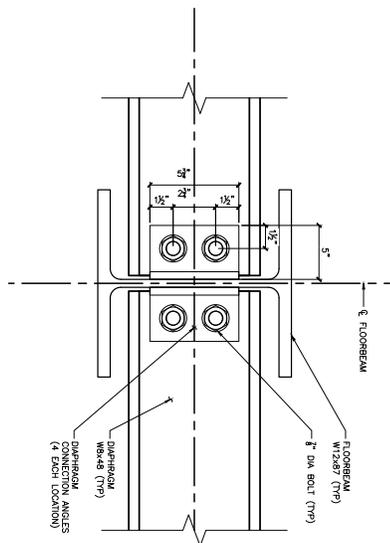
60% SUBMISSION



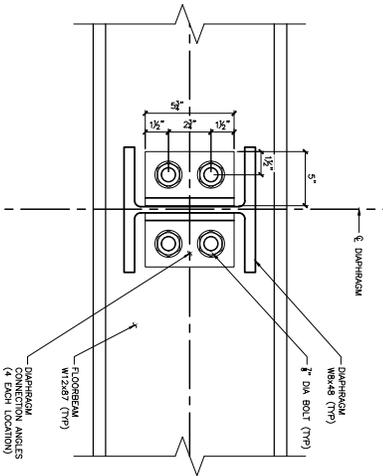
1 DIAPHRAGM PARTIAL ELEVATION



2 DIAPHRAGM CONNECTION DETAIL - 1



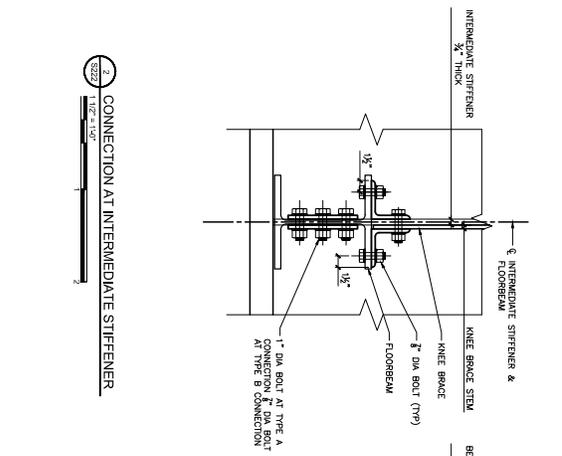
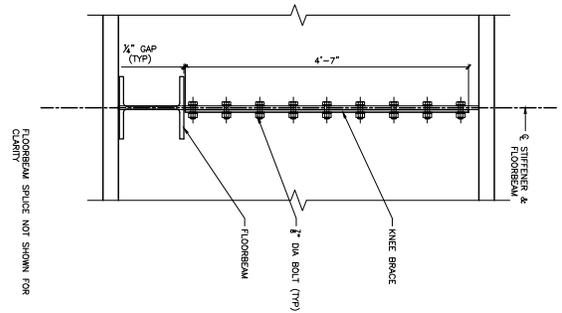
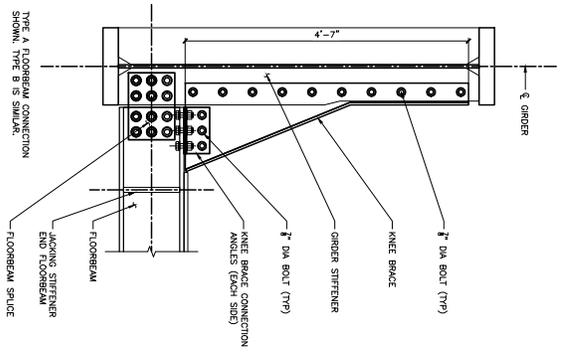
4 DIAPHRAGM CONNECTION DETAIL - 2



5 DIAPHRAGM CONNECTION DETAIL - 3

- NOTES:
1. ALL BOLTS ARE ASTM F1755 GRADE A325 SLIP CRITICAL BOLTS.

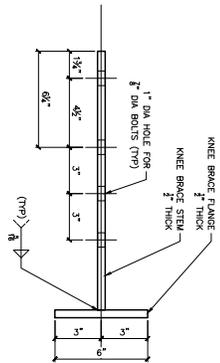
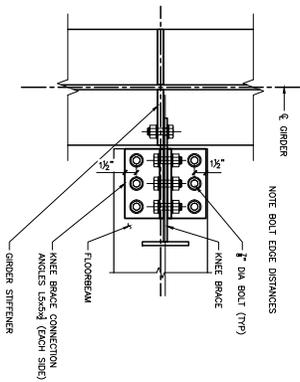
		PROFESSIONAL ENGINEERING ARCHITECTURE INTERIOR DESIGN PLANNING ENVIRONMENTAL SCIENCE CONSTRUCTION MANAGEMENT	
100% RESUBMISSION DATE: 10/25/2024 2:26:46 PM		PROJECT: MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE RRD MAINLINE, GLENSIDE, PA BRIDGE 11.62 REHABILITATION STRUCTURAL DIAPHRAGM CONNECTION DETAILS	
NO.	DATE	DESCRIPTION	BY
01	10/25/24	100% RESUBMISSION	SK
02	10/25/24	100% SUBMISSION	SK
03	10/25/24	60% SUBMISSION	SK
04			SK
05			SK
06			SK
07			SK
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100			SK



1 KNEE BRACE ASSEMBLY

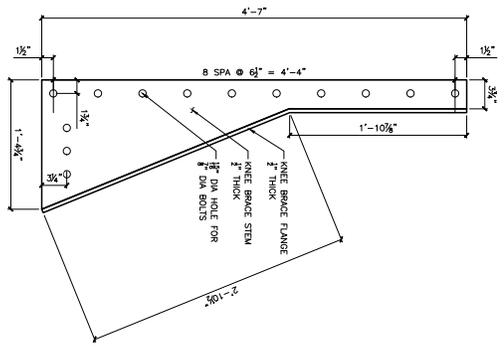
2 CONNECTION AT INTERMEDIATE STIFFENER

3 CONNECTION AT BEARING STIFFENER



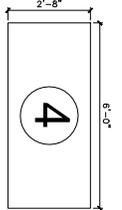
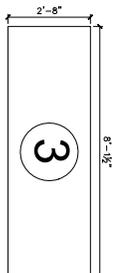
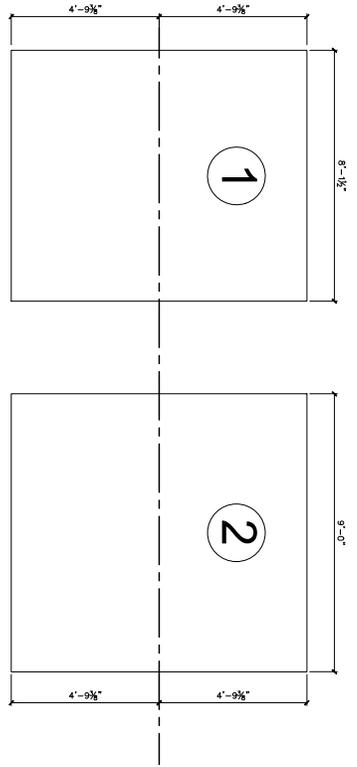
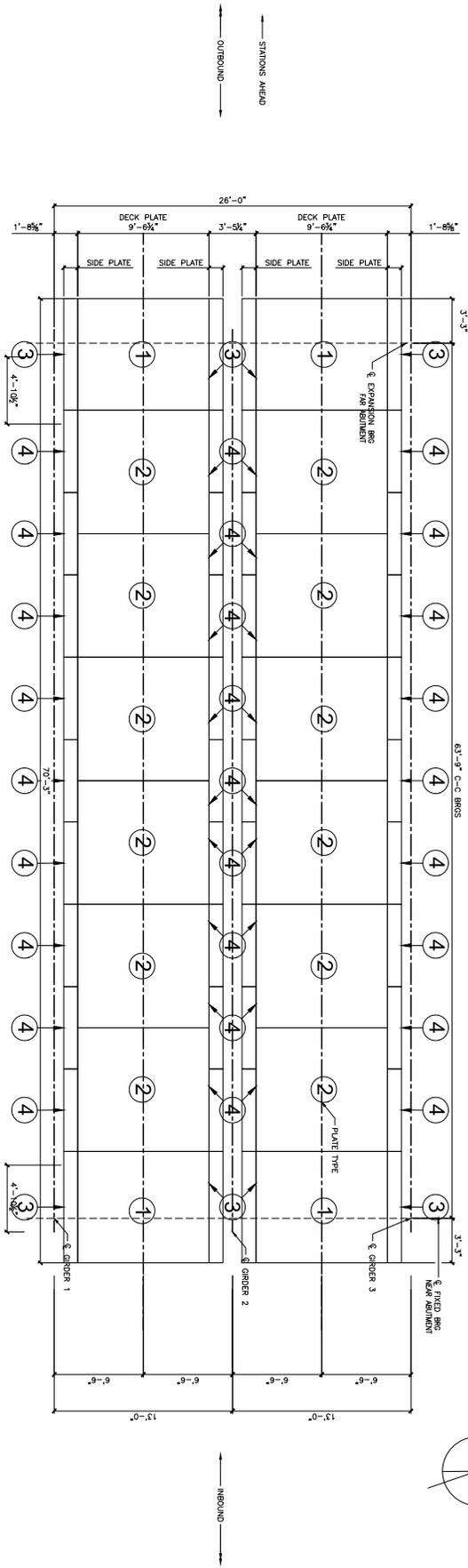
4 PARTIAL PLAN AT CONNECTION

5 KNEE BRACE DETAILS



NOTES
1. ALL BOLTS ARE ASTM F1554 GRADE A325 SUP CRITICAL BOLTS.

PROJECT NO: S222 DATE: OCT 2024		PROJECT NO: S222 DATE: OCT 2024	
MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE RRD MAINLINE, GLENSIDE, PA BRIDGE 11.62 REHABILITATION STRUCTURAL KNEE BRACE CONNECTION DETAILS			
REV	DATE	DESCRIPTION	
01	7/27/24	60% SUBMISSION	
02	9/27/24	100% SUBMISSION	
03	10/25/24	100% RESUBMISSION	
BY	CHKD	APPD	
SK	SK	SK	
SK	SK	SK	
SK	SK	SK	



- NOTES:**
- EXTERIOR AND INTERIOR GIRDERS ARE DIFFERENT.
 - DECK PLATES ARE 1/2" THICK AND SEGMENT ENDS ARE CONNECTED OVER FLANGE OF FLOORBEAMS.
 - SIDE PLATES ARE 1/2" THICK AND SEGMENT ENDS ARE CONNECTED OVER FLANGE OF FLOORBEAMS.
 - SIDE PLATES ARE 1/2" THICK AND SEGMENT ENDS ARE CONNECTED OVER FLANGE OF FLOORBEAMS.
 - PLATE DETAILS SHOWN APPROXIMATELY IN PLAN VIEW. SEE DWG S223 FOR SIDE PLATE DETAILS.
 - BENT BALLAST RETAINER PLATE IS NOT SHOWN FOR CLARITY. SEE DWG S223 FOR DETAILS.

JACOBS

100% RESUBMISSION

DATE: 10/25/2024 2:26:51 PM

MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE
RRD MAINLINE, GLENSIDE, PA

BRIDGE 11.62 REHABILITATION
STRUCTURAL
DECK PLATE PLAN

REV	DATE	DESCRIPTION
01	7/27/24	60% SUBMISSION
02	9/27/24	100% SUBMISSION
03	10/25/24	100% RESUBMISSION

DATE: 10/25/2024 2:26:51 PM

SCALE: AS NOTED

PROJECT: BRIDGE 11.62 REHABILITATION

DATE: OCT 2024

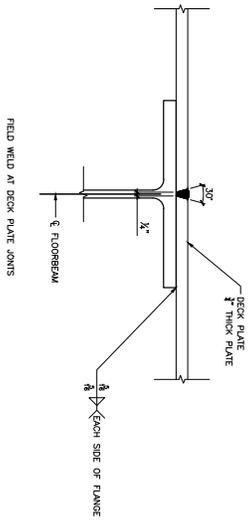
PROJECT NO: S223

REV: 03

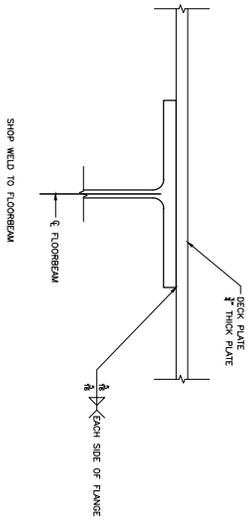
100% RESUBMISSION

DATE PRINTED: 10/25/2024 2:26:51 PM

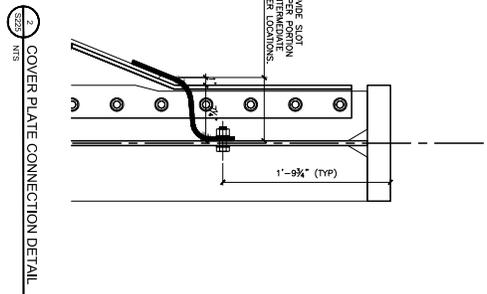
C:\PWORKING\JACOBS_B&I\EG_SIMU2\01650089\SEPTA_ML_BR_11.62_60 STEEL DECK PLATE.DWG



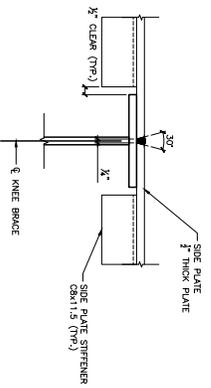
1 DECK PLATE WELD DETAIL
NTS



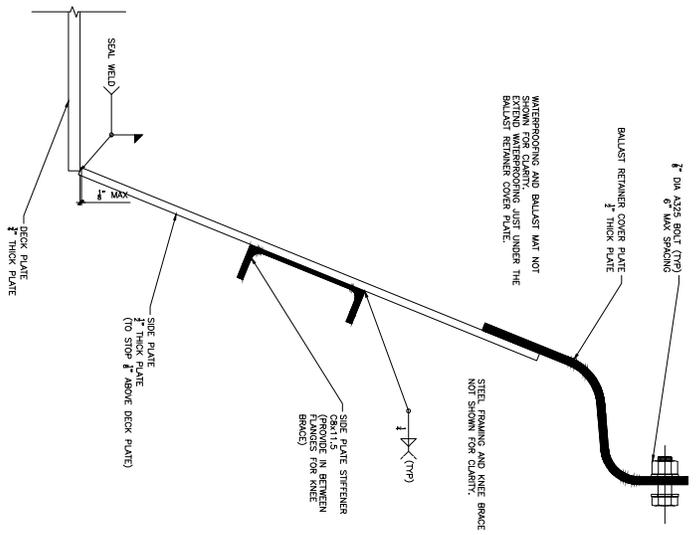
2 SIDE PLATE WELD DETAIL
NTS



3 COVER PLATE CONNECTION DETAIL
NTS



4 BALLAST RETAINER ASSEMBLY
NTS



5 BALLAST RETAINER ASSEMBLY
NTS

JACOBS
ENGINEERING
ARCHITECTURE
CONSULTANTS
INC.

1000 PENNSYLVANIA AVENUE
SUITE 1000
PHILADELPHIA, PA 19106

PROJECT: MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE
RRD MAINLINE, GLENSIDE, PA

BRIDGE 11.62 REHABILITATION
STRUCTURAL
DECK PLATE DETAILS

DATE: 10/25/2024 2:28:52 PM

NO.	DATE	DESCRIPTION
01	7/24/24	60% SUBMISSION
02	7/27/24	100% SUBMISSION
03	10/25/24	100% RESUBMISSION
04	10/25/24	100% RESUBMISSION
05	10/25/24	100% RESUBMISSION

DATE: 10/25/2024 2:28:52 PM

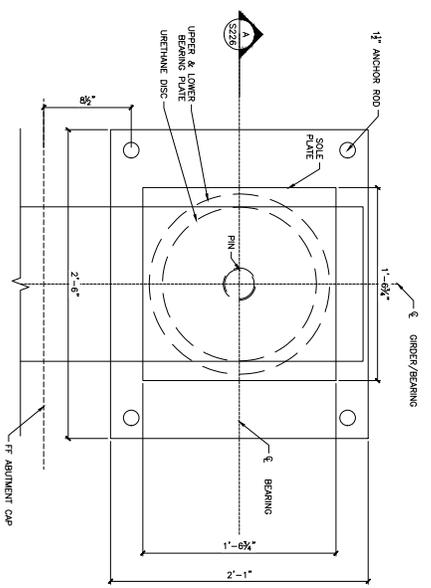
PROJECT: MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE
RRD MAINLINE, GLENSIDE, PA

BRIDGE 11.62 REHABILITATION
STRUCTURAL
DECK PLATE DETAILS

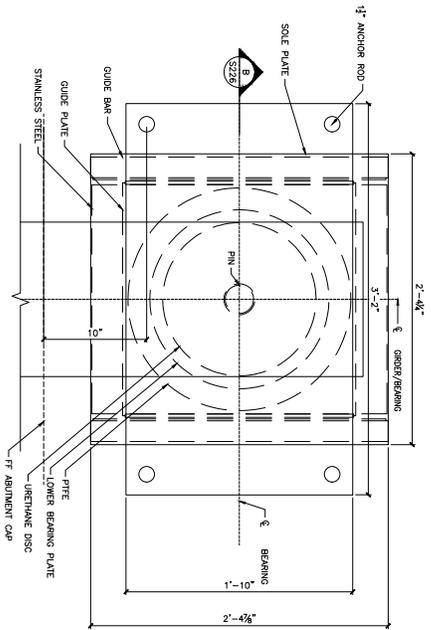
100% RESUBMISSION

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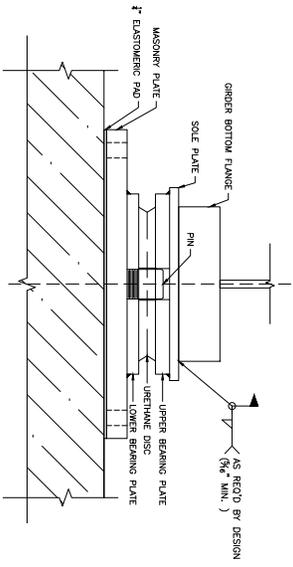
C:\PWORKING\JACOBS_B&I\JEG_S\14\2\0\650089\K_SEPTA_ML_BR_11.62_60 STEEL DECK PLATE.DWG



1 FIXED BEARING

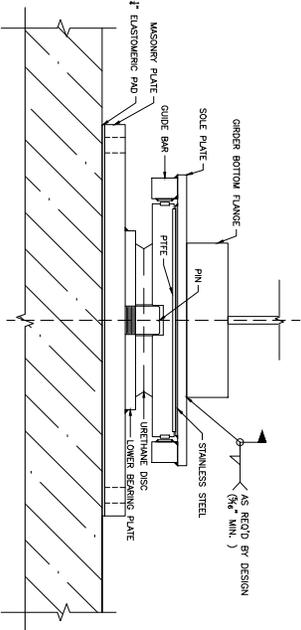


2 EXPANSION BEARING



SECTION AA

NEAR ABUTMENT INTERIOR BEAMS



SECTION BB

FAR ABUTMENT INTERIOR BEAMS

INTERIOR GIRDER - BEARING CAPACITY TABLE

SERVICE VERTICAL LOAD (KIPS)	576	720
SERVICE HORIZONTAL LOAD (KIPS)	12	60
LONGITUDINAL DISPLACEMENT (INCHES)	± 2.5	± 0.55
PIE LOAD ROTATION (RADIANS)	0.02	0.02
PIE LOAD TOLERANCE (RADIANS)	0.005	0.005
CONSTRUCTION TOLERANCE (RADIANS)	0.005	0.005

EXTERIOR GIRDER - BEARING CAPACITY TABLE

SERVICE VERTICAL LOAD (KIPS)	576	720
SERVICE HORIZONTAL LOAD (KIPS)	12	60
LONGITUDINAL DISPLACEMENT (INCHES)	± 2.5	± 0.55
PIE LOAD ROTATION (RADIANS)	0.02	0.02
PIE LOAD TOLERANCE (RADIANS)	0.005	0.005
CONSTRUCTION TOLERANCE (RADIANS)	0.005	0.005

- NOTES:
- SEE GENERAL NOTES FOR ADDITIONAL INFORMATION.
 - SCHEMATIC DRAWING FOR BID PURPOSES ONLY. SUPPLIER SHALL PROVIDE DESIGN CALCULATIONS FOR THE BEARING ASSEMBLY & ANCHORAGE IN ACCORDANCE WITH THE SPECIFICATIONS FOR BEARINGS IN THE GENERAL NOTES. SEE BEARING CAPACITY TABLE.
 - SOLE PLATE TO BE BEVELED IN LONGITUDINAL DIRECTION TO ACCOMMODATE SUPERSTRUCTURE SLOPE.
 - BEARING STIFFENERS AND END FLOORBEAM CONNECTION NOT SHOWN FOR CLARITY.
 - ANCHOR BOLTS SHALL BE ASTM F1554 GRADE 55 WITH MINIMUM DIAMETER OF 1 1/2".
 - ALL LOADS REPORTED FOR THE GIVEN RECEIVING ARE MAXIMUM RESULTANT LOADS. LOADS ARE REPORTED IN ASD ACCORDANCE WITH AASHTO SPECIFICATIONS.

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100% RESUBMISSION

DATE: 10/25/2024 2:26:55 PM

BRIDGE 11.62 REHABILITATION

STRUCTURAL

BEARING ASSEMBLY - INTERIOR GIRDER

NO.	DATE	BY	DESCRIPTION
1	10/25/24	SK	100% RESUBMISSION
2	10/25/24	SK	100% SUBMISSION
3	10/25/24	SK	60% SUBMISSION
4	10/25/24	SK	60% SUBMISSION

PROJECT INFORMATION

PROJECT NO: S226

DATE: 10/25/2024

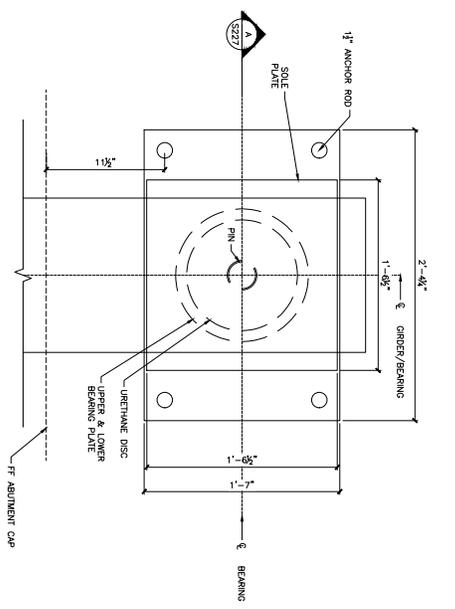
ISSUE: 11

REVISIONS

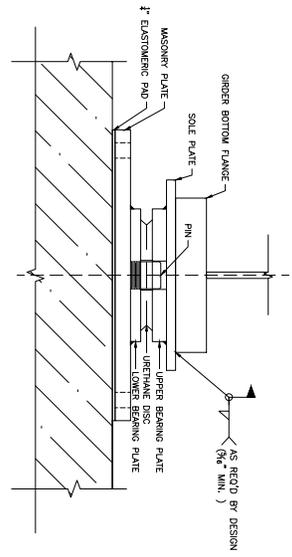
NO. 11

DATE: 10/25/2024

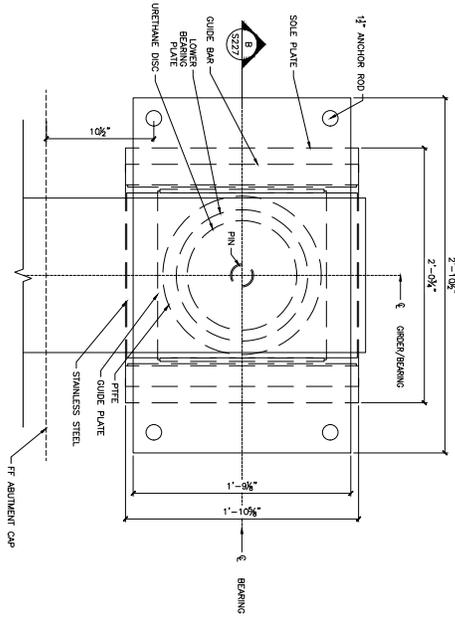
DESCRIPTION: 11



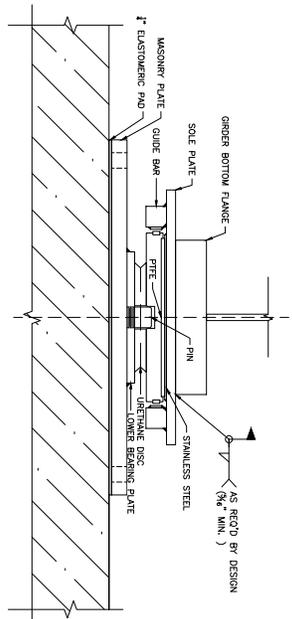
SECTION A-A
FIXED BEARING



SECTION AA
NEAR ABUTMENT EXTERIOR BEAMS



SECTION B-B
EXPANSION BEARING



SECTION B-B
FAR ABUTMENT EXTERIOR BEAMS

- NOTES:
1. SEE GENERAL NOTES FOR ADDITIONAL INFORMATION.
 2. SPRINGING BEARING FOR BRG PURPOSES ONLY. SUPPORTERS SHALL PROVIDE DESIGN THE CAPACITY OF THE BEARING AS SHOWN IN THE GENERAL NOTES. SEE BEARING CAPACITY TABLE.
 3. SOLE PLATE TO BE BENT IN LONGITUDINAL DIRECTION TO ACCOMMODATE SUPERSTRUCTURE SLOPE.
 4. BEARING STIFFENERS AND END FLOORBEAM CONNECTION NOT SHOWN FOR CLARITY.
 5. ANCHOR BOLTS SHALL BE ASTM F1554 GRADE 55 WITH MINIMUM DIAMETER OF 1/2".

Jacobs
CONSULTANTS
1000 PENNSYLVANIA AVENUE, N.W.
WASHINGTON, D.C. 20004
TEL: 202.638.2000
WWW.JACOBS.COM

PROJECT: BRIDGE 11.62 REHABILITATION
SHEET: BEARING ASSEMBLY - EXTERIOR GIRDERS
DATE: 10/25/2024 2:28:55 PM

REV	DATE	DESCRIPTION
01	10/25/24	60% SUBMISSION
02	10/25/24	100% SUBMISSION
03	10/25/24	100% RESUBMISSION

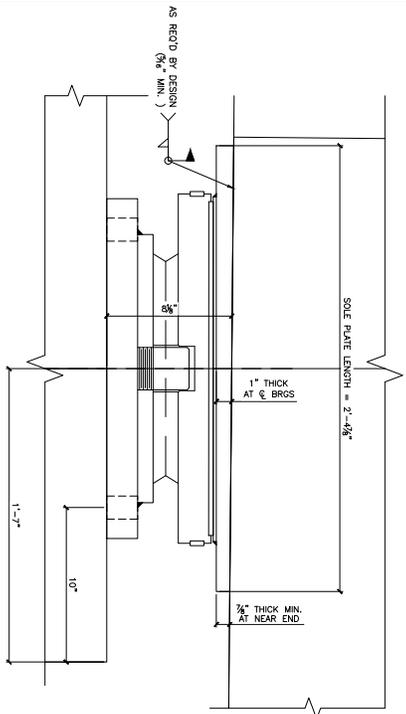
PROJECT NO: **S227**

DATE: **OCT 2024**

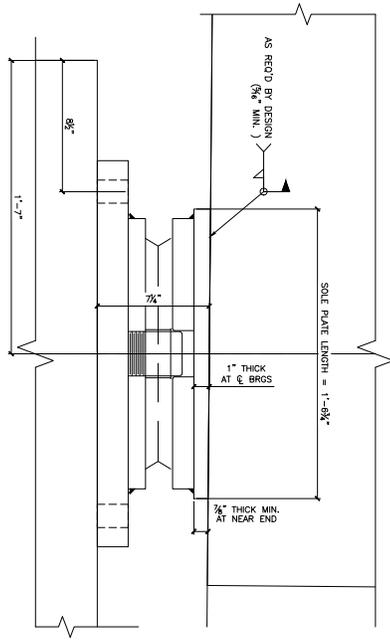
SCALE: **1:1**

PROJECT: **BRIDGE 11.62 REHABILITATION**

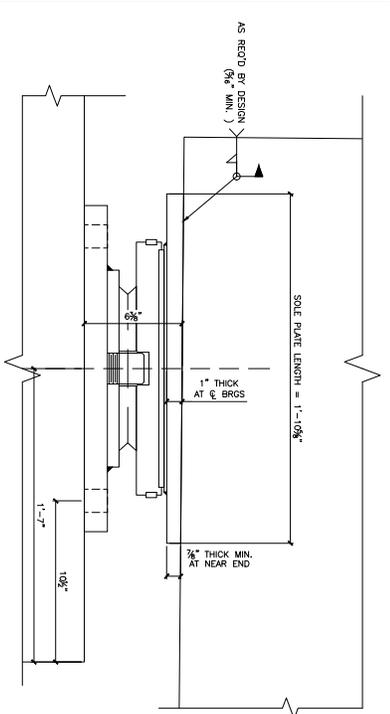
SHEET: **BEARING ASSEMBLY - EXTERIOR GIRDERS**



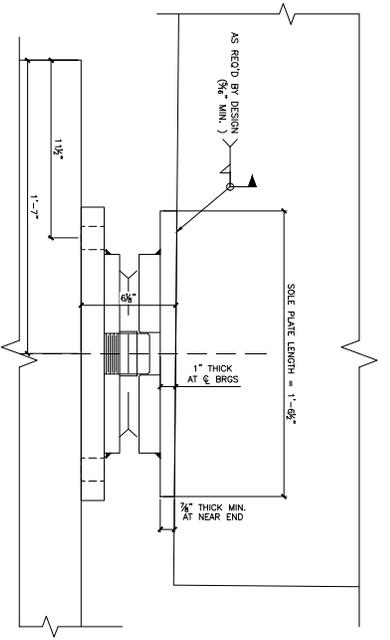
1 SOLE PLATE DETAIL - INTERIOR GIRDER EXPANSION END (FAR)



2 SOLE PLATE DETAIL - INTERIOR GIRDER FIXED END (NEAR)



3 SOLE PLATE DETAIL - EXTERIOR GIRDER EXPANSION END (FAR)



4 SOLE PLATE DETAIL - EXTERIOR GIRDER FIXED END (NEAR)

- NOTES:
- BEARER SOLE PLATES ARE TO BE PROVIDED AT EACH END OF THE GIRDER TO ACCOMMODATE LONGITUDINAL GRADE OF THE SUPERSTRUCTURE GIRDER END TO END. THE THICKNESS OF THE SOLE PLATE AT THE CENTER OF BEARING IS PROVIDED TO ACCOMMODATE THE BEARER REQUIRED TO MATCH THE GIRDER SLOPE.

Jacobs
 1000 MARKET STREET, SUITE 1000
 SAN FRANCISCO, CA 94102
 TEL: 415.436.3000
 WWW.JACOBS.COM

PROFESSIONAL SEAL
 CIVIL ENGINEER
 STATE OF CALIFORNIA
 NO. 10000
 DATE: 10/25/2024

PROJECT INFORMATION

PROJECT NO: **S228**

DATE: **OCT 2024**

SCALE: **AS NOTED**

DESIGNER: **11**

CHECKER: **11**

DATE: **10/25/2024**

MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE
 RRD MAINLINE, GLENSIDE, PA

BRIDGE 11.62 REHABILITATION
 STRUCTURAL

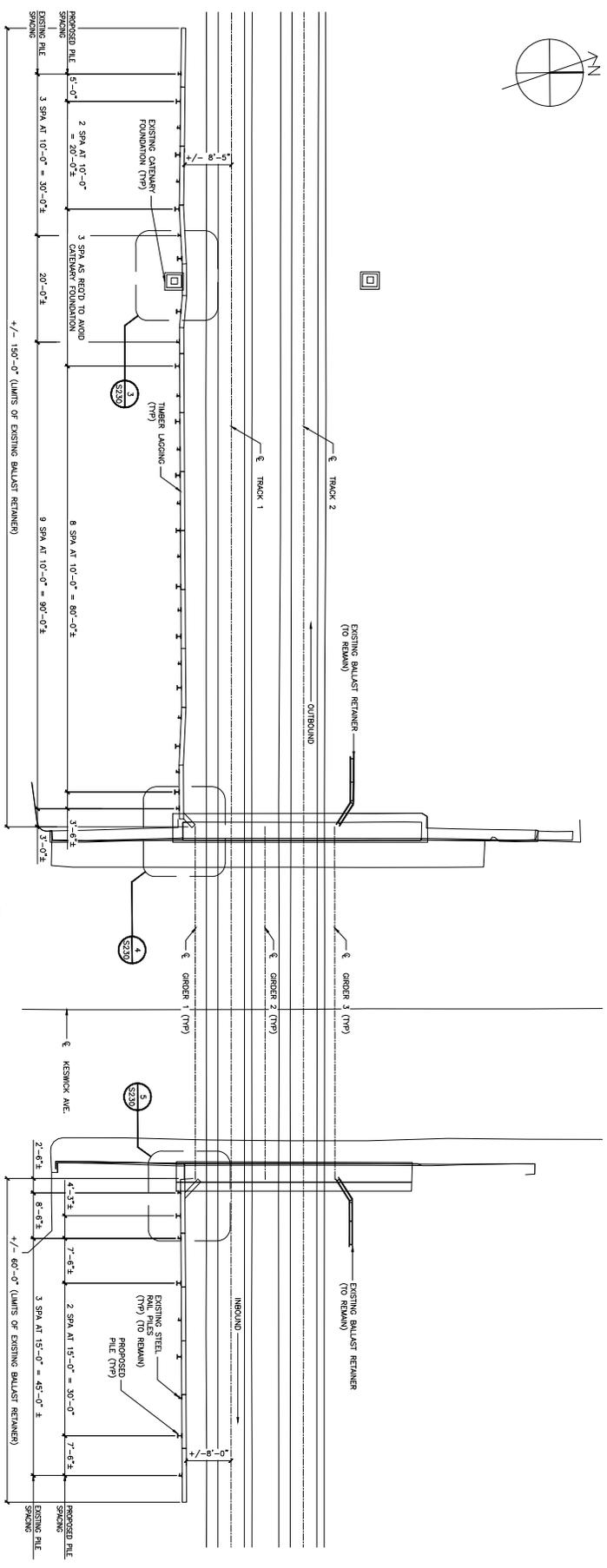
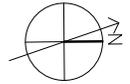
BEARING ASSEMBLY DETAILS

REV	DATE	DESCRIPTION
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02	9/27/24	100% SUBMISSION
03	10/25/24	100% RESUBMISSION

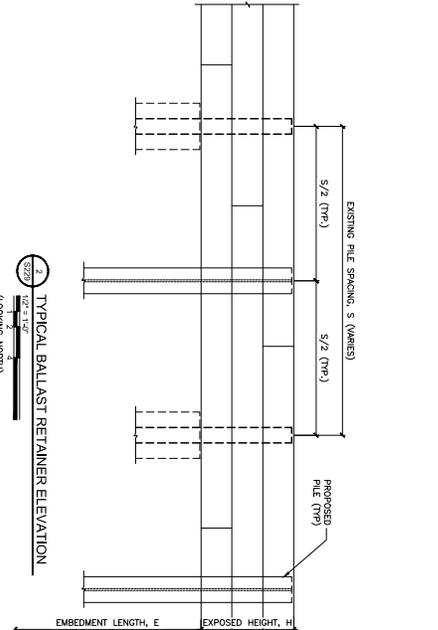
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1 BALLAST RETAINER PLAN
1" = 10'-0"



2 TYPICAL BALLAST RETAINER ELEVATION
1/2" = 1'-0"
(LOOKING NORTH)

PILE ALTERNATIVES TABLE

PILE SIZE	MINIMUM HEIGHT, H	MINIMUM EMBEDMENT LENGTH, E	MINIMUM TIEBARS PER PILE	MINIMUM DIMENSION, D
HERB39	3'-0"	8'-6"	12'-6"	N/A
HERB39	2'-0"	8'-0"	10'-0"	N/A

- NOTES
1. REPLACE ENTIRE TOP ROW OF TIEBARS USING, REPLACE ADDITIONAL DAMAGED DAMAGED TIEBARS USING.
 2. DO NOT BE TO BE RIGID TO THE ROW BEHIND WITH 10' ASYM AND DAMAGED DAMAGED TIEBARS.
 3. PROPOSED PILES TO BE INSTALLED AT THE ADJACENT BETWEEN EACH EXISTING PILE.
 4. THE PROPOSED STEEL HP PILES SHALL BE ASYM 457Z OR 50. DAMAGED PER ASYM A132
 5. HP PILE SECTIONS ARE TO BE DAMAGED TO THE MAXIMUM DAMAGED BEHIND AS SHOWN ON THIS SHEET
 6. EXPOSED WALL HEADS ENCOUNTERED IN THE FIELD, PROVIDE THE MAXIMUM DAMAGED BEHIND AS SHOWN.

JACOBS
INCORPORATED
1000 PENNSYLVANIA AVENUE, N.W.
WASHINGTON, DC 20004
TEL: 202.638.3000
WWW.JACOBS.COM

PROFESSIONAL SEAL
REGISTERED PROFESSIONAL ENGINEER
STATE OF MARYLAND
NO. 11111
DATE: 10/25/2024

MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE
RRD MAINLINE, GLENSIDE, PA
BRIDGE 11.62 REHABILITATION
STRUCTURAL
BALLAST RETAINER RECONSTRUCTION - 1

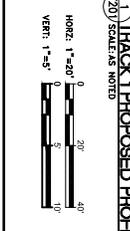
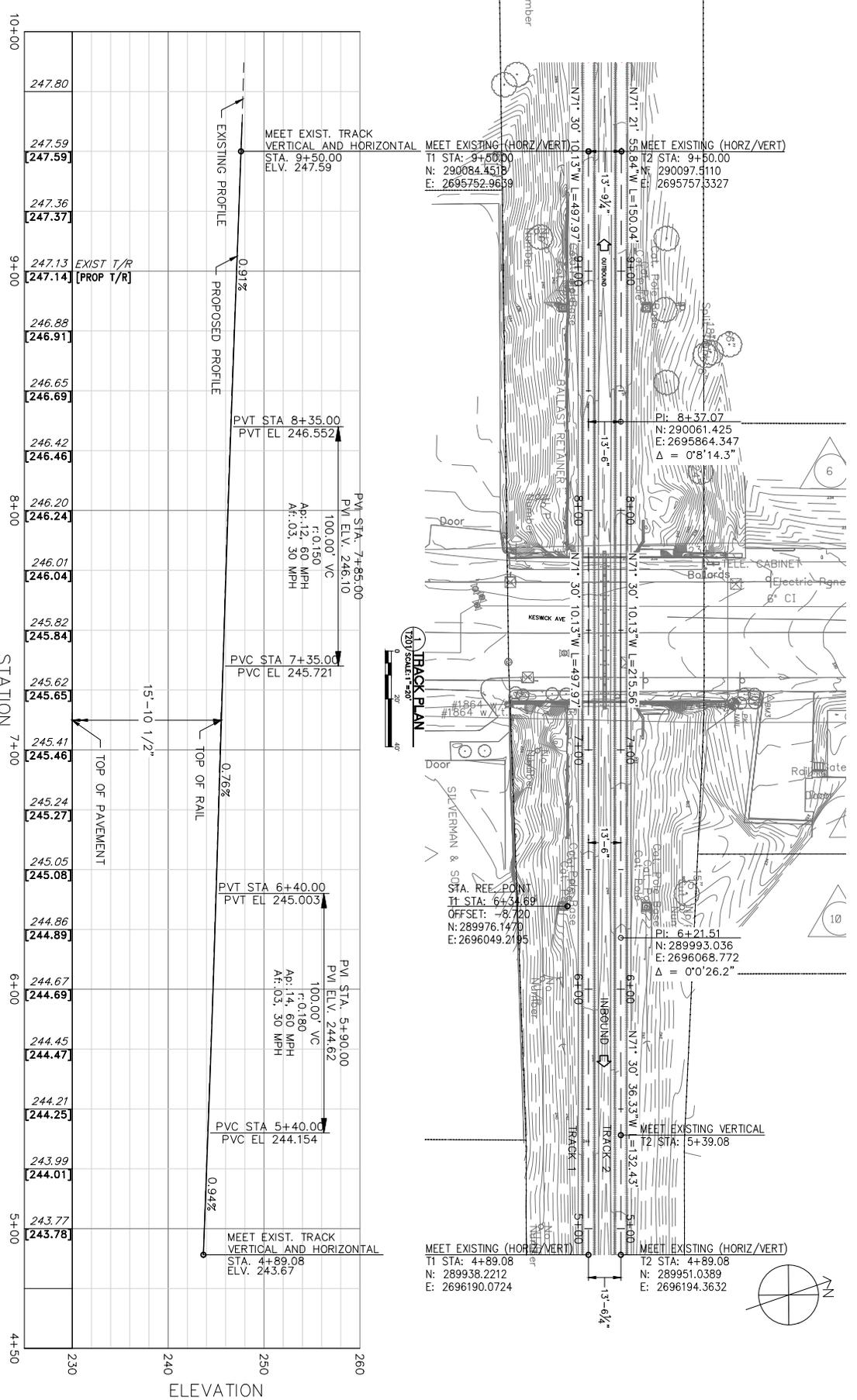
DATE: 10/25/2024
PROJECT NO: S229
SHEET NO: 03
SCALE: AS NOTED
DATE: 10/25/2024
PROJECT NO: S229
SHEET NO: 03
SCALE: AS NOTED

100% RESUBMISSION
100% SUBMISSION
60% SUBMISSION

DESCRIPTION

REV: 01
DATE: 10/25/24
BY: [Signature]

APP'D: [Signature]
CHK'D: [Signature]
BY: [Signature]



DATE	10/25/2024
AS NOTED	11
DATE	10/25/2024
PROJECT	BRIDGE 11.62 REHABILITATION
NO.	11
DATE	10/25/2024
PROJECT	BRIDGE 11.62 REHABILITATION
NO.	11
DATE	10/25/2024
PROJECT	BRIDGE 11.62 REHABILITATION
NO.	11

REV	DATE	DESCRIPTION	BY	CHKD	APPD
03	10/25/24	100% RESUBMISSION	MJK	DAB	DP
02	10/27/24	100% SUBMISSION	MJK	DAB	DP
01	07/24/24	60% SUBMISSION	MJK	DAB	DP

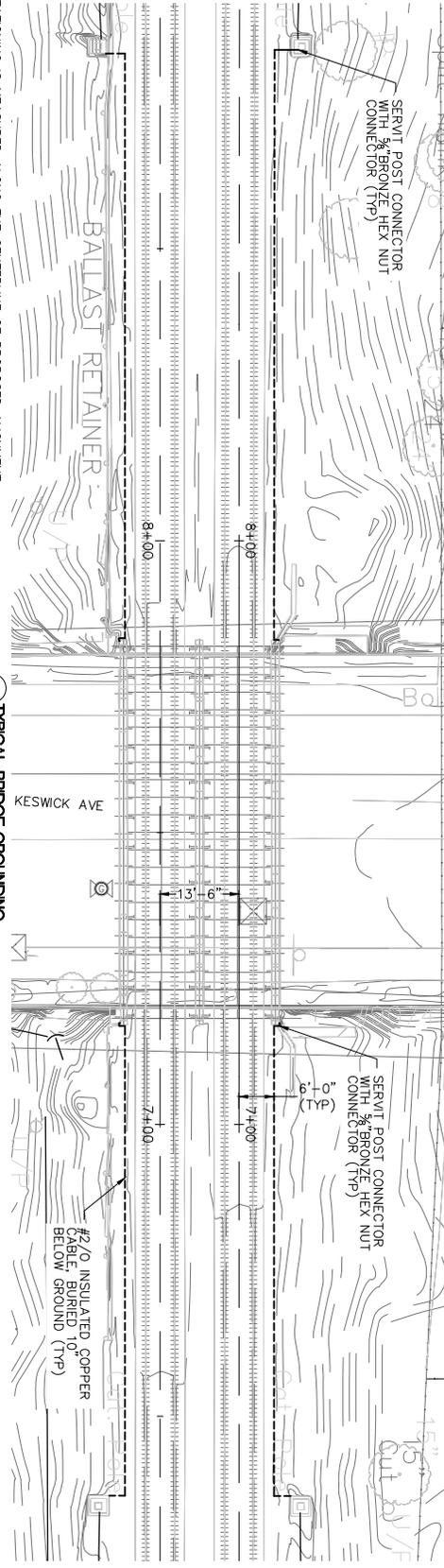
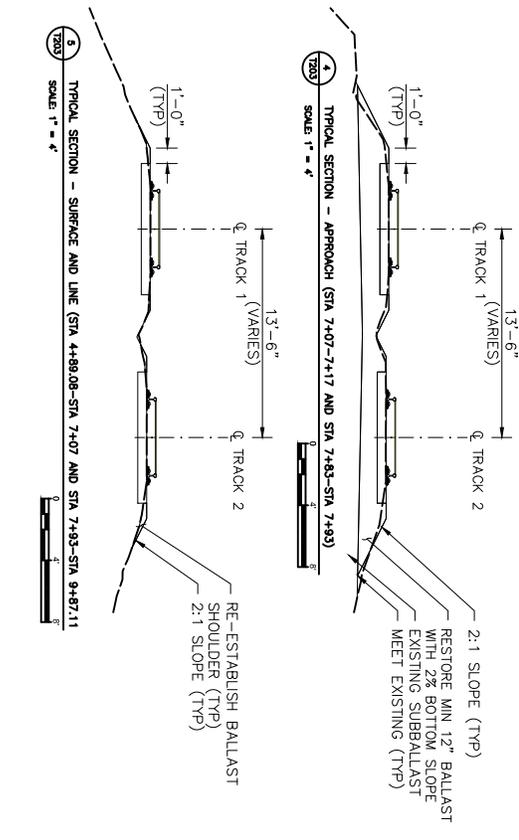
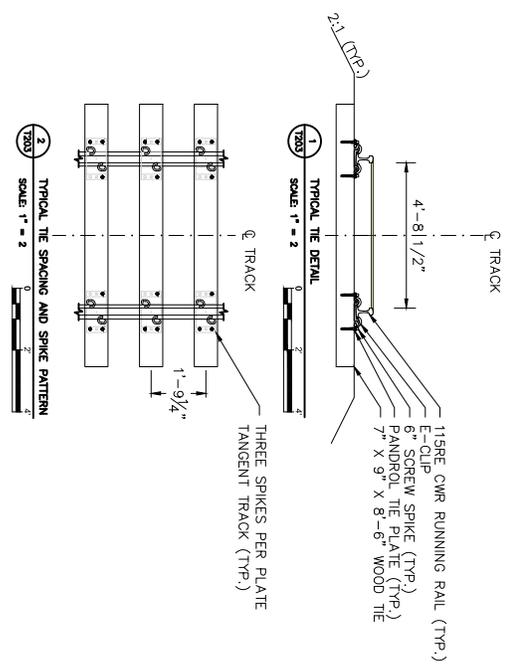
Jacobs

100% SUBMISSION

DATE: 10/25/2024

PROJECT: BRIDGE 11.62 REHABILITATION

DATE	10/25/2024
PROJECT	BRIDGE 11.62 REHABILITATION
NO.	11
DATE	10/25/2024
PROJECT	BRIDGE 11.62 REHABILITATION
NO.	11



- NOTES:
1. TRACK STATIONING IS MEASURED ALONG THE CENTERLINE OF PROPOSED ALIGNMENT.
 2. UTILIZE EXISTING TRACK WHERE PRACTICAL TO ACHIEVE THE FINAL ALIGNMENT BY LINING/SURFACING. INSTALL NEW TRACK ON BRIDGE AND WHERE NECESSARY ON THE APPROACHES TO ALLOW FOR CONSTRUCTION OF THE NEW BRIDGE.
 3. EXCAVATE EXISTING BALLAST ON EXISTING BRIDGE AND BETWEEN STATIONS 7+07 TO 7+93 (10' OFF BRIDGE ON EITHER SIDE). LIMITS CAN BE ADJUSTED IN ORDER TO ACCOMMODATE CONTRACTORS MEAN AND METHODS OF REPLACING BRIDGE.
 4. EXISTING HARDPAN SHALL REMAIN UNDISTURBED OUTSIDE BALLAST REMOVAL LIMITS.
- BENCHMARKS:
1. BM3: ELEVATION: 229.2123', 290083.810 N, 2695993.457 E, "X" CUT IN CONCRETE SIDEWALK
 2. BM4: ELEVATION: 239.4519', 290249.110 N, 2695950.291 E, "X" CUT IN CONCRETE SIDEWALK

- GROUNDING NOTES:
1. GROUND FASQA GIRDERS OF EACH BRIDGE TO NEAREST CATENARY STRUCTURE EAST AND WEST OF BRIDGE.
 2. PLAN SHOWN IS SCHEMATIC. ADAPT DETAILS TO SPECIFIC LOCATIONS.

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100% SUBMISSION

DATE: 10/25/2024 3:31:19 PM

MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE
RRD MAINLINE, GLENSIDE, PA

BRIDGE 11.62 REHABILITATION
TRACK

TRACK NOTES, DETAILS, AND TYPICAL SECTIONS

REV	DATE	DESCRIPTION
03	10/25/24	100% RESUBMISSION
02	09/27/24	100% SUBMISSION
01	07/04/24	60% SUBMISSION

PROJECT NO: 1203

DATE: OCT 2024

SCALE: 1" = 10'

DESIGNER: N/A

CHECKER: N/A

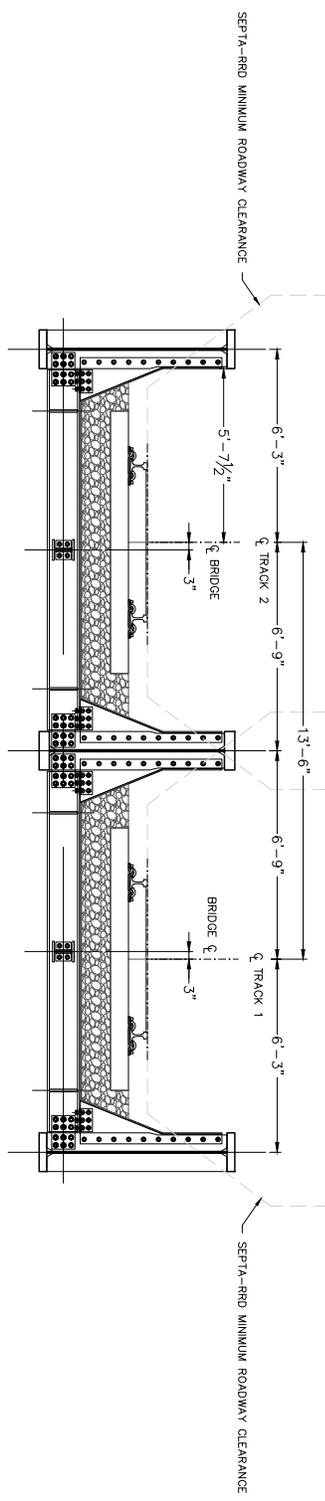
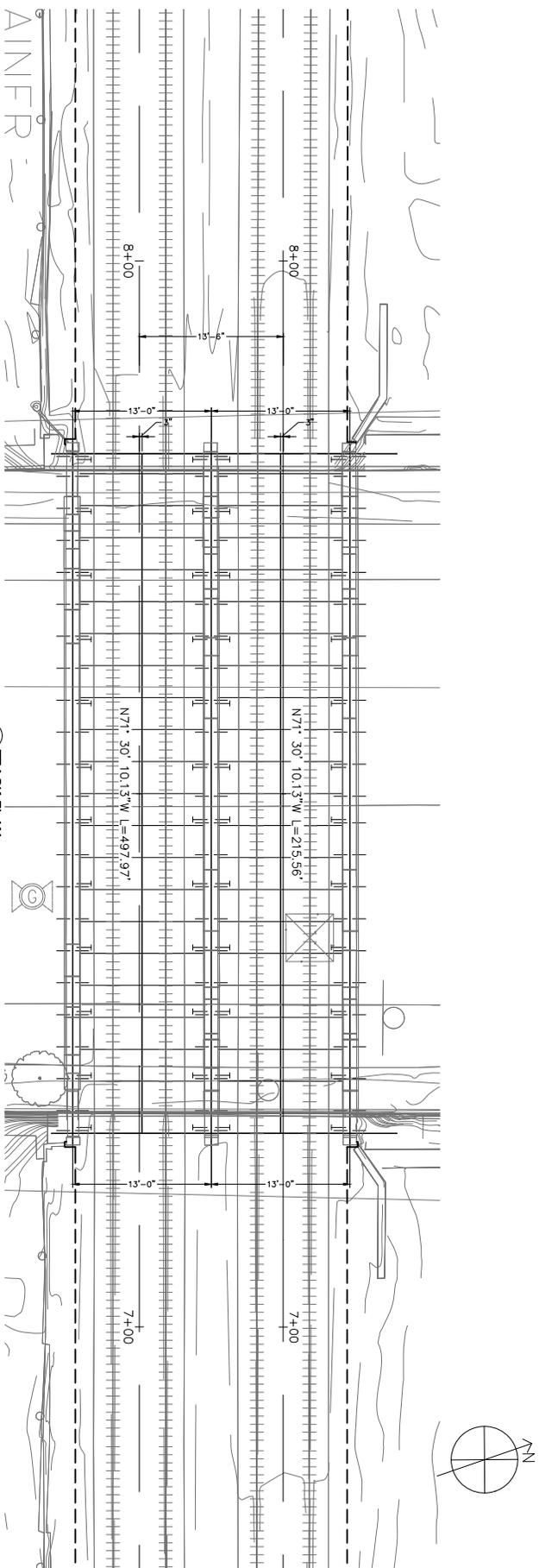
DATE: OCT 2024

SCALE: 1" = 10'

PROJECT NO: 1203

DATE: OCT 2024

SCALE: 1" = 10'



Jacobs
 300 N. ZEEB ROAD
 SUITE 200
 ANN ARBOR, MI 48106
 TEL: 734.769.7000
 WWW.JACOBS.COM

REV	DATE	DESCRIPTION
03	10/25/24	100% RESUBMISSION
02	09/27/24	100% SUBMISSION
01	07/26/24	60% SUBMISSION

BRIDGE 11.62 REHABILITATION TRACK
 TRACK CLEARANCES AND BRIDGE SECTION

MAINLINE BRIDGE MP 11.62 OVER KESWICK AVENUE
 RRD MANLINE, GLENSIDE, PA

BRIDGE 11.62 REHABILITATION TRACK
 TRACK CLEARANCES AND BRIDGE SECTION

DATE: 10/25/24
 DRAWN BY: [blank]
 CHECKED BY: [blank]
 PROJECT NO: T204
 SHEET NO: 03

AS NOTED
 OCT 2024
 11
 03

100% SUBMISSION
 DATE PRINTED: 10/25/2024 3:31:21 PM